

MAYOR
Barbara Jo Blain-Bellamy

MAYOR PRO TEM
Justin D. Jordan



COUNCIL MEMBERS
Amanda Butler
William M. Goldfinch IV
Beth Helms
Larry A. White
Autry Benton

**PLANNING DEPARTMENT
CITY OF CONWAY
COMMUNITY APPEARANCE BOARD MEETING
Wednesday, October 11, 2023 | 4:00 P.M.
Planning & Building Department – 196 Laurel Street**

- I. CALL TO ORDER**
- II. APPROVAL OF MINUTES (September 27, 2023)**
- III. CERTIFICATES OF APPROPRIATENESS**
 - A. **16 Elm St (Cypress Inn):** The applicant, Cypress Inn Property LLC, is requesting final approval on plans for a future building to be constructed beside 16 Elm St. (PIN 367-01-04-0042).
- IV. PUBLIC INPUT**
- V. BOARD INPUT**
- VI. STAFF INPUT**
- VII. UPCOMING MEETINGS**

<u>MEETING</u>	<u>DATE</u>	<u>TIME</u>	<u>LOCATION</u>	<u>ADDRESS</u>
City Council	October 16, 2023	4:00 p.m.	Council Chambers	229 Main St.
Community Appearance Board	October 25, 2023	4:00 p.m.	Planning & Building Dept. Conference Room	196 Laurel St.
Board of Zoning Appeals (BZA)	October 26, 2023	5:30 p.m.	Planning & Building Dept. Conference Room	196 Laurel St.
Planning Commission (PC)	November 2, 2023	5:30 p.m.	Planning & Building Dept. Conference Room	196 Laurel St.

VIII. ADJOURN

CITY OF CONWAY
COMMUNITY APPEARANCE BOARD MEETING
WEDNESDAY, September 27, 2023
Planning & Building Dept. Conference Room – 196 Laurel Street

Present: Duc Watts (arrived late), George Ulrich, Jacqueline Kurlowski, Jamie McLain, McKenzie Jordan, Troy Roehm, Gerry Wallace (arrived late)

Absent:

Staff: Jessica Hucks, Planning Director; Katie Dennis, Planning Concierge; Charlie Crosby, IT

Other: Eddie Moore, Hillary Howard

I. CALL TO ORDER

Chairperson Kurlowski called the meeting to order at approximately 4:00 p.m.

II. APPROVAL OF MINUTES

Jordan made a motion to accept the minutes as written and it was seconded by McLain to approve the September 13, 2023 minutes. The vote in favor was unanimous. The motion carried.

III. CERTIFICATES OF APPROPRIATENESS

IV. 315 Main Street Unit 2 (Sissy's): The applicant, A1 Signs and Graphics, requests approval for proposed signage for the business, Sissy's, located at 315 Main Street (367-01-01-0022).

A. 315 Main Street Unit 2 (Sissy's): The applicant, A1 Signs and Graphics, requests approval for proposed signage for the business, Sissy's, located at 315 Main Street (367-01-01-0022).

Hucks stated the applicant, A1 Signs and Graphics, requests approval of proposed window/door graphics for the business located at 315 Main St for Sissy's.

The applicant proposes to install **2-27" (w) X 15" (h)** "Sissy's" logo decals on the window panels that measure **29" (w) X 72" (h)**. The total coverage area of the decals will be **2.81 sq. ft.** for both windows and the maximum area allowed is **3.63 sq. ft.**

The applicant is proposing to install one **22" (w) X 12" (h)** logo decal on the door panel that measures **26.5" (w) X 72" (h)** with a max coverage of **6.63 Sq. Ft.**

The proposed size of the graphics are compliant with the standards of the UDO for window and door signage.

The applicant, staff and the board discussed at length.

Jordan made a motion to approve as presented with the condition that the applicant may in fill the hanger, the word "Sissy's", or use a bolder line choice to provide better distinction of the signage on the large window. Ulrich seconded the motion. The vote in favor was unanimous the motion carried.

V. QUATTLEBAUM NOMINEES

Hucks presented the 2022 Quattlebaum nominees to the board. The board considered and discussed the nominees.

Jordan made a motion to approve **Honey Hair Studio-315 Kingston St for Outstanding Restoration of a Non-Residential Building**, Roehm seconded the motion. The motion carried unanimously.

Wallace made a motion to approve **Bojangles-1617 Church St for Outstanding New Construction of a Non-Residential Building**, Jordan seconded the motion. The motion carried unanimously.

McLain made a motion to approve **601 Burroughs St. for Outstanding Restoration of a Home**, Jordan seconded the motion. The motion carried unanimously.

Wallace made a motion to approve **4301 Bradford Dr. for Outstanding New Construction of a Residential Building**, Jordan seconded the motion. The motion carried unanimously.

Wallace made a motion to approve **893 Kingston Lake Dr. for Outstanding Landscape Project**, Jordan seconded the motion. The motion carried unanimously.

Roehm made a motion to approve **B&B Theaters-220 Rivertown Dr. for Outstanding contribution to a quality development, restoration, landscape or design by an Organization or Individual**, Jordan seconded the motion. The motion carried unanimously.

VI. PUBLIC INPUT

None

VII. BOARD INPUT

VIII. STAFF INPUT

IX. ADJOURN

There being no further business to come before the board, a motion was made and seconded to adjourn the meeting at 4:29 pm. The vote in favor was unanimous and the motion carried.

Approved and signed this _____ day of _____, 2023.

Jacqueline Kurlowski, Chairperson

DATE: October 11, 2023
ITEM: III.A

ISSUE:

16 Elm St (Cypress Inn): The applicant, Cypress Inn Property LLC, is requesting final approval on plans for a future building to be constructed beside 16 Elm St. (PIN 367-01-04-0042).

BACKGROUND:

The applicant, Cypress Inn Property LLC, is requesting final approval on the future plans of expansion of the Cypress Inn.

The plans are currently awaiting comments to continue TRC review and the applicant has been made aware of some issues with the conceptual drawing by staff.

This property is located within a Special Flood Hazard Area (SFHA) and is required to meet standards of elevation, certain material types and flood openings.

At the June 28th CAB meeting the board asked the applicant to

- Provide a rendering showing the new building in relation to the existing structures
- Update renderings to show the wrap around balconies with larger column posts
- Provide a larger scale gazebo entrance for more proportion
- Explore the option of shutters
- Provide a gable bump out on the Laurel St side to provided dimension
- Provide an ADA ramp on the renderings and elevations

The applicant has received City Council approval for the Hotel/Short term rental incentive and is excited to move forward with construction.

At the July 26th CAB meeting the board was in favor of the updated plans and renderings but could not move forward with approval ahead of the approval of a height variance by the Board of Zoning appeals.

At the August 24th BZA meeting the board voted to approve the variance request of the applicant to allow for a building height of 46'. Since the BZA meeting City Council has approved an amendment to the Height limit in the WRD 1 and 2 District to be increased to 60'.

The applicant is now allowed to seek final approval of the provided design as the height is in compliance with the UDO.

Renderings are included in your packet.

APPLICABLE STANDARDS:

City of Conway Unified Development Ordinance

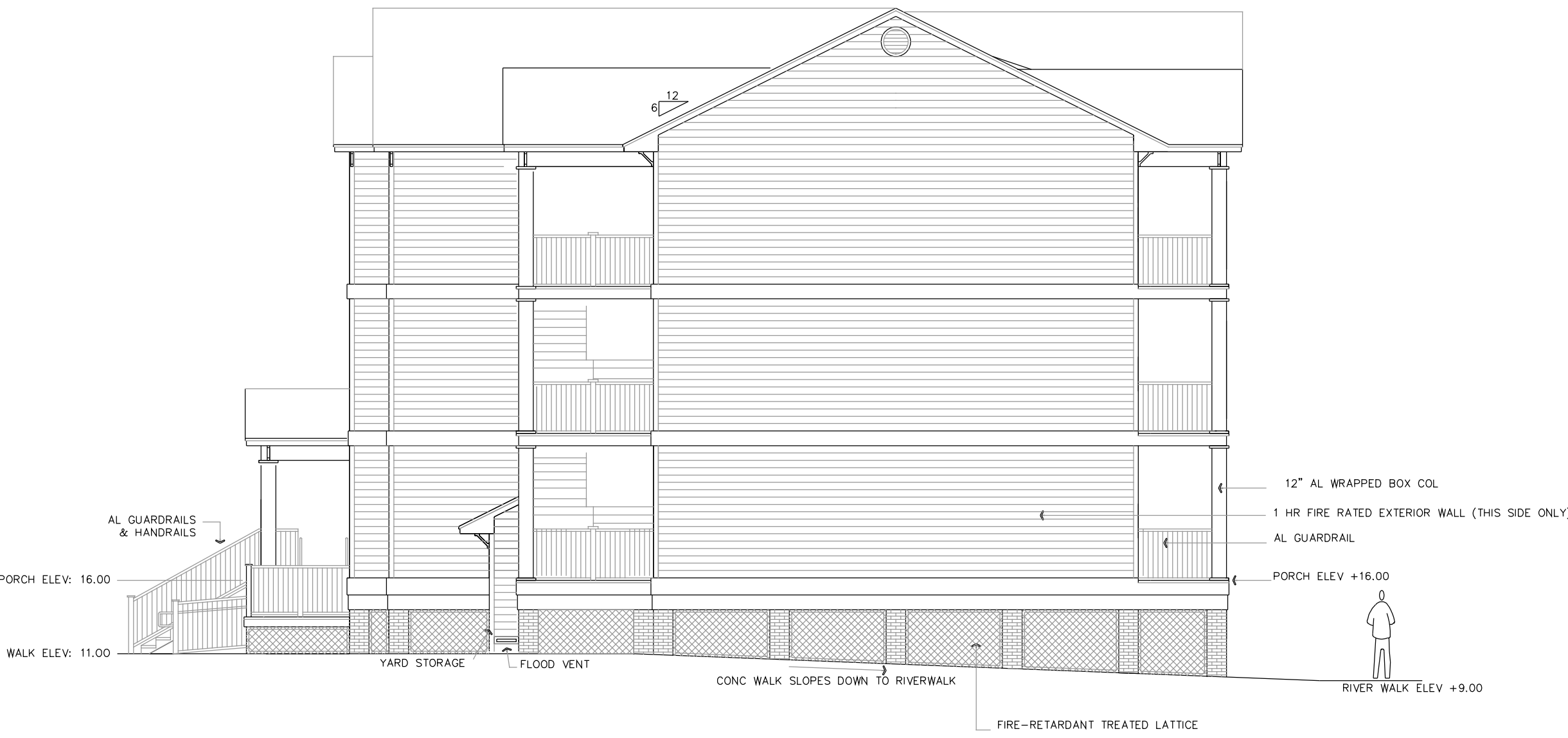
- Section 3.3.1-Special Use Districts
- Section 6.3.1-Non-Residential Architectural Design Standards
- Section 6.4.1- Special Use District Design Standards-WRD
- Section 6.6-General Design Standards
- Section 8.2-Parking Design Standards
- Section 9.3-Landscaping Design Standards

Historical Design Review District

- Section B, Ch 3: 3.4 The Downtown Environment
- Section B, Ch 4: 4.5 New Commercial Construction
- Section D, Ch 10: 10.5 WRD Roofs, Canopies and Roof Lines

STAFF RECOMMENDATION:

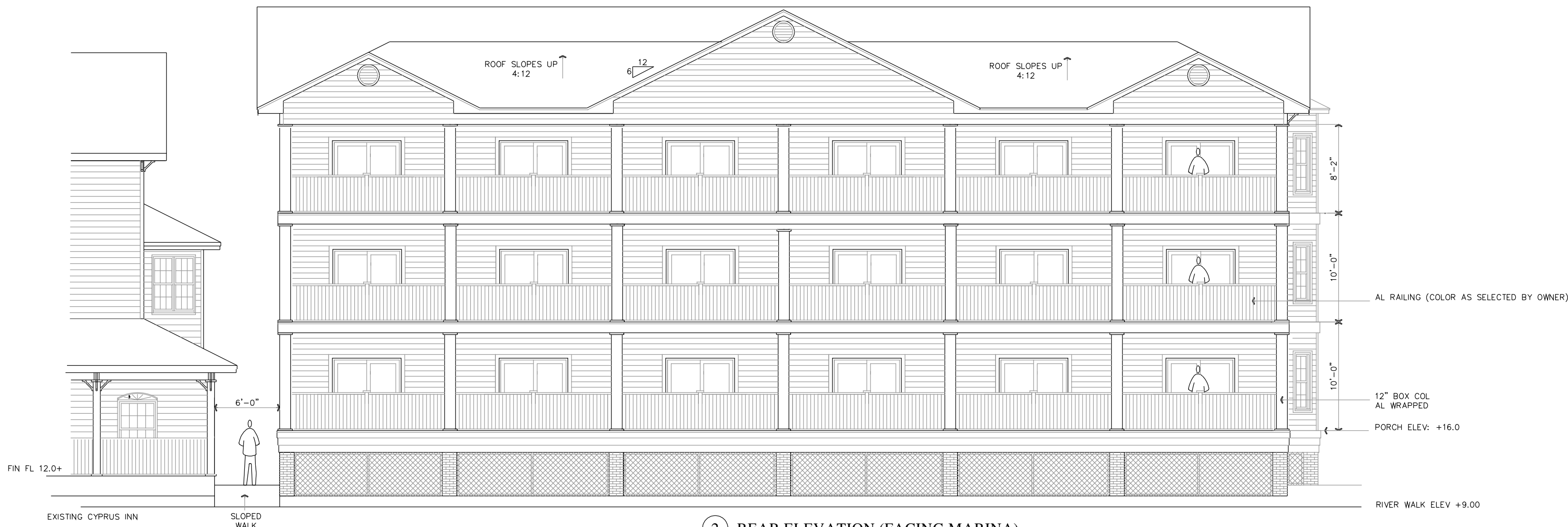
Staff recommends the board review the proposal carefully and give feedback.



④ RIGHT SIDE ELEVATION (FACING ORIGINAL BUILDING)
1/8" = 1'-0"



③ LEFT SIDE ELEVATION
1/8" = 1'-0"

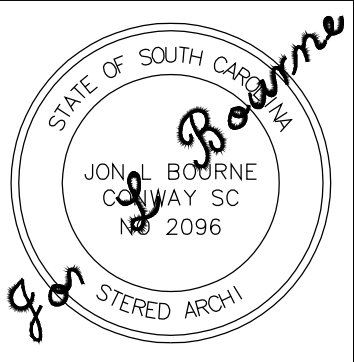


② REAR ELEVATION (FACING MARINA)
1/8" = 1'-0"
DROP PORCHED 4"



① FRONT ELEVATION (FACING PARKING)
1/8" = 1'-0"

DROP PORCHED 4"



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CYPRUS INN - BUILDING #2
RIVERFRONT DISTRICT
CONWAY SC

PRELIMINARY PLANS
NOT FOR CONSTRUCTION

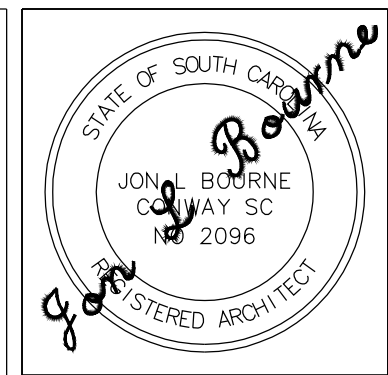
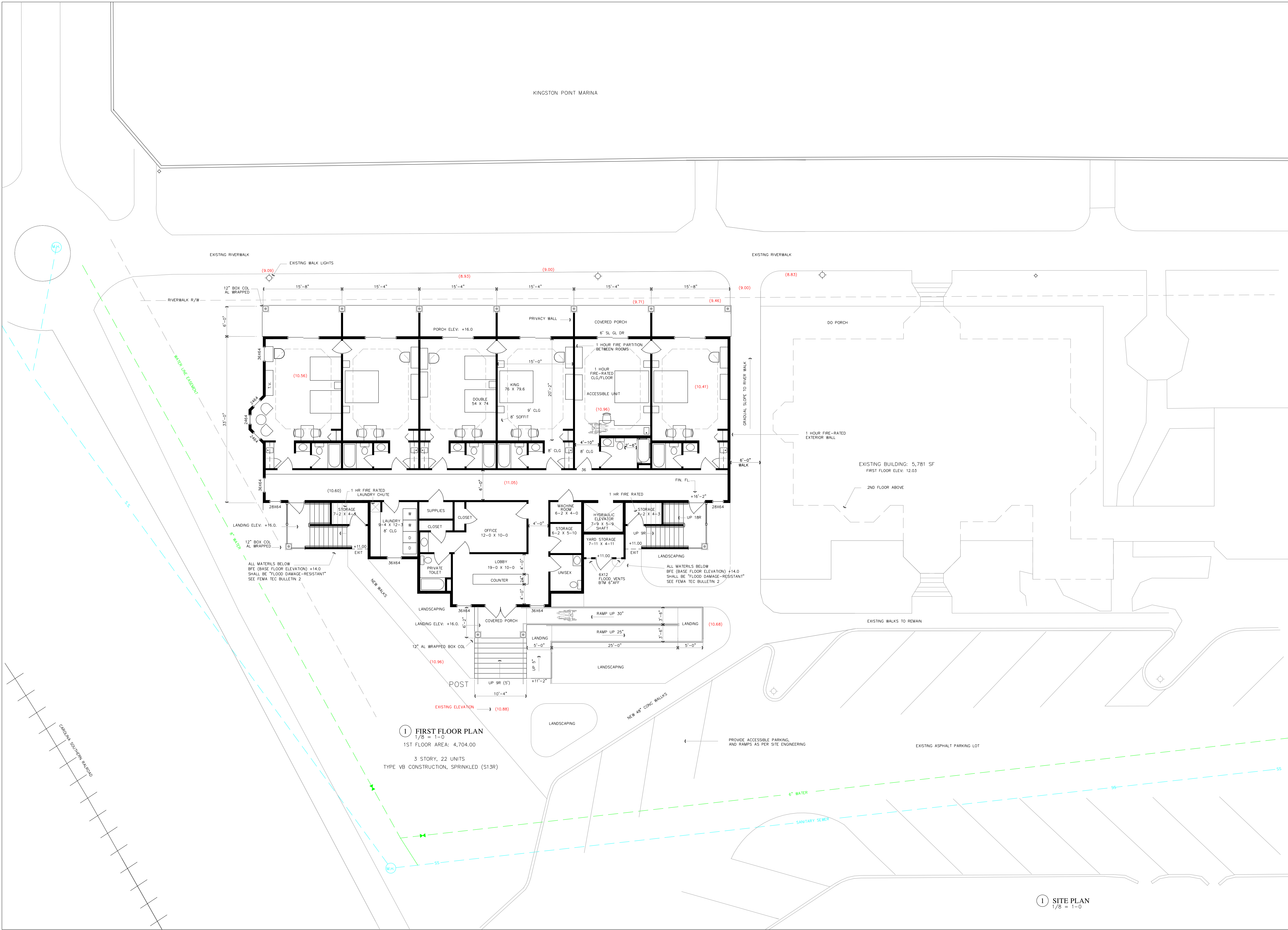
JON L. BOURNE - ARCHITECT
CONWAY, SC - 843.347.2902
Email: jlbarch@scstate.net

JLB

PROJECT NO: 22-1041
DATE: 7-10-2023
REVISED:
FILE:
SHEET

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OF 3

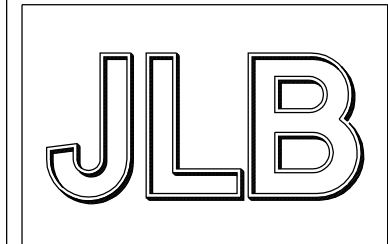


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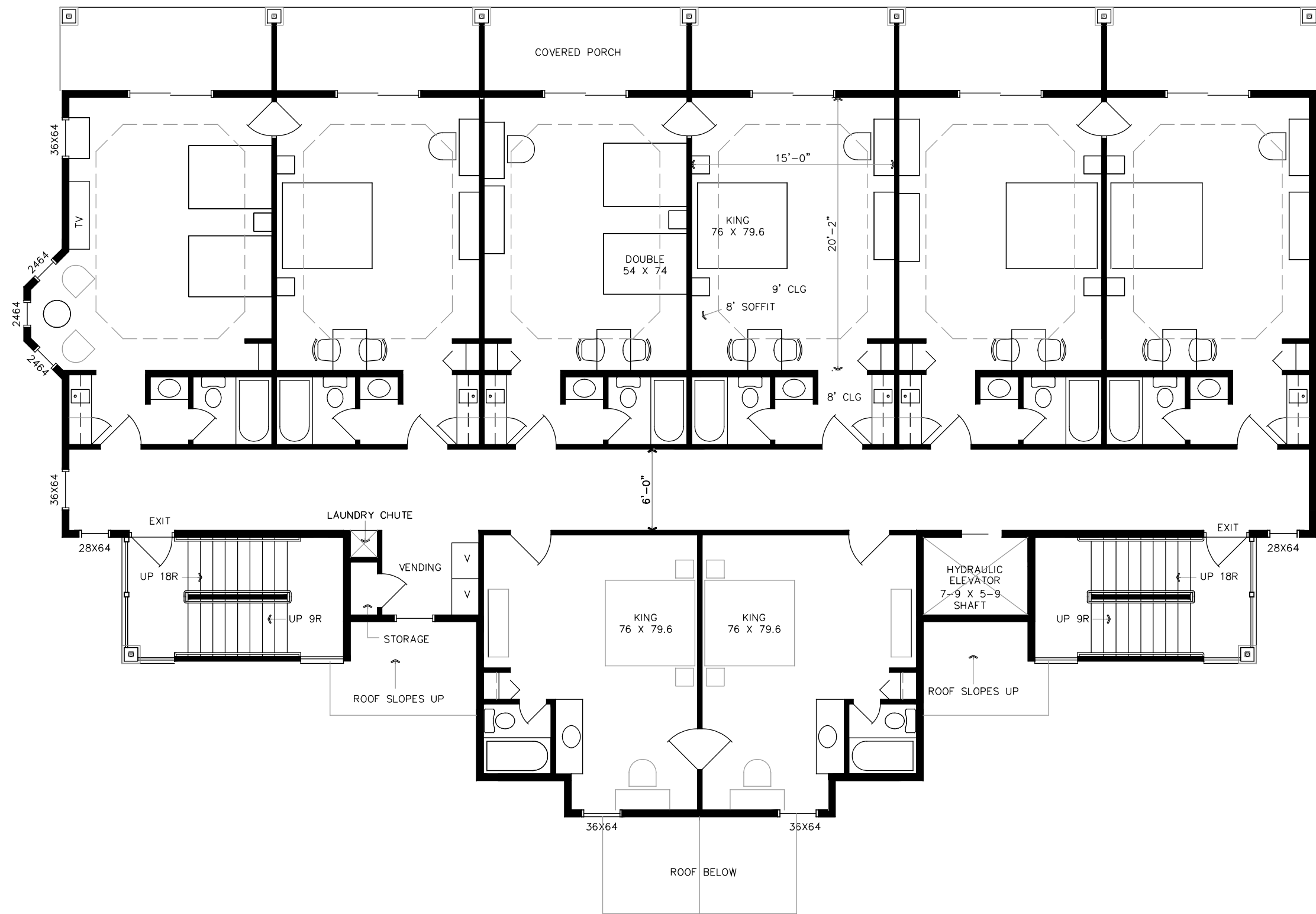
CYPRUS INN - BUILDING #2
RIVERFRONT DISTRICT
CONWAY SC

PRELIMINARY PLANS
NOT FOR CONSTRUCTION

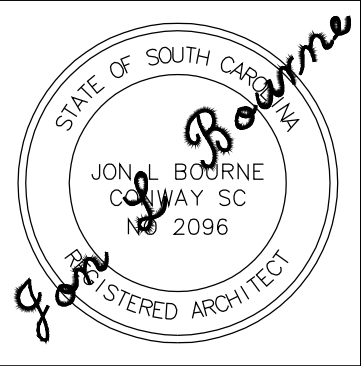
JON L. BOURNE - ARCHITECT
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Email: jlbarch@scconstr.net



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SHEET	1
OF 3	



① SECOND FLOOR PLAN (THIRD FLOOR SIMILAR)
1/8" = 1'-0"
2ND FLOOR AREA: 4,515 SF
INCLUDES PORCHES

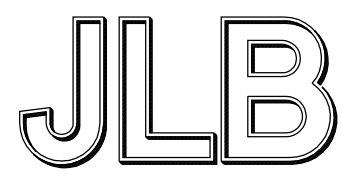


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CYPRUS INN - BUILDING #2
RIVERFRONT DISTRICT
CONWAY SC

PRELIMINARY PLANS
NOT FOR CONSTRUCTION

JON L. BOURNE - ARCHITECT
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PROJECT NO: 22-1041
DATE: 07-12-2023
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6.3.1 Non-Residential Architectural Design Standards

- A. **Intent:** Non-residential architectural design standards protect and enhance the aesthetic and visual character of various developments within the City of Conway. In particular, the purpose is to encourage and better articulate positive visual experiences throughout the City of Conway and to provide for economic growth and stability through the preservation of property values. The design review process is not intended to stifle innovative architecture but to assure respect for surrounding uses and reduce incompatible and adverse impacts on the visual experience.
- B. **Historic Design Review Districts (HDRDs):** Properties located within the HDRDs shall be reviewed and approved by the Community Appearance Board (CAB) and shall meet the "Historic Design Review Districts: Community Appearance Guidelines".
- C. **P, NC, HC, CC, CBD, GCO, and VCO:** Properties zoned P, NC, HC, CC, CBD, GCO, and VCO (exempting parcels zoned CP, FA, LI, and HI) that are located outside the HDRDs shall be reviewed and approved by the Planning Director and shall meet the following architectural design standards:
1. The architectural design, color, and material of a proposed structure, or structures, shall conform to community standards of good taste and design.
 2. Proposed structures will contribute to the image of the City of Conway as a unique place of visual character, integrity, and quality.
 3. All elevations of a structure shall be in harmony one with another in terms of scale, proportion, detail, material, color, and high design quality.
 4. The side and rear elevations of buildings shall be visually attractive, especially where those side or rear elevations are most often viewed by the public. Rooflines and architectural detailing shall present a consistency in quality design.
 5. All structures within a proposed development, including gasoline station canopies, shall utilize a uniform architectural theme and shall be designed to create a harmonious whole. It is not to be inferred that buildings must look alike to achieve a harmony of style. Harmony of style can be created through proper consideration of scale, proportion, detail, materials, color, site planning, and landscaping.
 6. The scale of buildings and accessory structures (including canopies) shall be appropriate to the scale of structures located in the surrounding area. Canopies designed as domineering or overpowering architectural features shall not be permitted.
 7. Long, monotonous facade design, including, but not limited to, those characterized by unrelieved repetition of shape or form, or by unbroken extension of line, shall not be permitted.
 8. The architectural design and material finish of buildings, signage, gasoline pump canopies, and other necessary structures shall be compatible with one another and surrounding structures.
 9. Color combinations of paints and stains shall be complimentary. In general, no more than three different colors per building shall be permitted.
 10. Materials shall express their function clearly and honestly and shall not appear as materials which are foreign to the character of the rest of the building.
 11. Any building exterior elevation shall consist of architectural materials which are equal in quality, appearance, and detail to all other exterior elevations of the same structure. Nothing in this section shall preclude the use of different materials

on different exterior elevations of the same structure so long as those materials maintain the architectural unity and integrity of the entire structure.

12. Stucco, tabby, wood siding, brick, stone, traditional metal components, textured concrete masonry units, glass, fiber cement siding, and other materials with similar textures are permitted. In addition to these materials, aluminum composite materials are permitted for HC Zoning Districts located within the GCO. Fiber cement, AZEK, and PVC are permitted as a synthetic trim.
13. No portion of a building constructed of unadorned concrete masonry units or corrugated metal, sheet metal, exposed metal, and / or manufactured panelized metal wall systems shall be visible in any manner from adjoining developed properties, from existing public rights-of-way, or from adjoining properties which are eligible for future development.
14. When unreasonable or impractical situations would result from the strict application of Section 6.3.1.C of the *UDO*, the owner or developer of property, zoned HC and located in the GCO, has the right to provide an alternative architectural design plan. Such situations may result from unique site conditions, innovative design applications, and / or unified development design. The Planning Director shall use the following criteria when determining whether an alternative architectural design plan can be accepted in lieu of meeting the requirements stated in Section 6.3.1.C:
 - a) The proposal includes a clear and concise explanation of the specific standards that are unreasonable or impracticable in that particular situation and how the alternative methods proposed will achieve the intent of Section 6.3.1.C.
 - b) The proposal represents the use of alternative designs and / or materials, which will result in an acceptable alternative to what is required in Section 6.3.1.C.
 - c) The proposal is compatible with and will enhance the use or value of surrounding properties.
 - d) The proposal is consistent with the intent of the *UDO*, the City of Conway Comprehensive Plan, and other current and future City of Conway adopted plans.
 - e) The Planning Director can require larger building setbacks, increased landscape buffers, and / or other screening methods as part of the approval for an alternative design plan.

Amended 12-3-12, #ZA2012-12-03 (C)

Section 6.4 – Special Use District Design Standards

6.4.1 Waccamaw Riverfront District (WRD)

The Waccamaw Riverfront District defines the area south of Second Avenue and east of Beaty Street. Historically a manufacturing and rail transportation corridor, it is important to honor the area's past while continuing to support new quality development. The purpose of this district is to help increase the economic vitality of the downtown, enhance the riverfront approach, provide opportunity for new mixed-use development, provide for the public's use and enjoyment, and to provide for the protection of the river bank.

New buildings and uses that locate in the Waccamaw Riverfront District should be of pedestrian scale and orientation; ensure the public's visual access and enjoyment of the river; evoke a feeling of the industrial and transportation history of the area; include design orientation both to the river and to the public way; and integrate public outdoor activity.

The area's configuration is linear and narrow and provides limited potential for on-site vehicular circulation or storage. It's an area primarily served and occupied by people, not vehicles, although some provision has been made for public and private parking. Uses in the Waccamaw Riverfront District are not to be automobile-oriented or dependent. *[Amended 2/18/19 #ZA2019-02-18 (C)]*

A. Sub-districts

The Waccamaw Riverfront District is divided into two sub-districts for zoning purposes: WRD-1 and WRD-2, as shown on the Official Zoning Map.

B. Dimensional Requirements

The following requirements shall apply to all new buildings, uses, or development in the Waccamaw Riverfront District.

1. Minimum lot area: Five-thousand (5,000) square feet.
2. Minimum front yard: Five (5) feet from edge of pavement or curb.
3. Minimum side yard. Zero (0) feet. Minimum of twenty feet between buildings.
4. Minimum rear yard:
 - a. Ten (10) feet measured from top of bank.
 - b. Fifteen (15) feet setback when the river easement does not apply.
 - c. Twenty-five (25) feet setback is required when a river easement is required (see Section E.2).
5. Flexibility in setbacks for properties adjacent to the Riverwalk may be needed to allow for creativity in site design and building placement, if approved the Planning Department. Property owners and/or developers shall be able to reduce their setbacks and distances between adjacent buildings, as to be determined on a case-by-case basis by the Planning Department.
6. No development shall be allowed in the existing rail road right-of-way.
7. Specific uses in water to be approved by the Conway City Council.

C. Waccamaw Riverfront HDRD Design Guidelines

All new or infill developments within the Waccamaw Riverfront District are required to follow the City of Conway's Community Appearance Guidelines, and must be reviewed by the City of Conway's Community Appearance Board (CAB).

D. Mixed Use Requirements for WRD-1

1. In order to stimulate pedestrian activity and public access within the WRD-1 District, (front or facade-facing ground level) of any new or redeveloped building or buildings must be devoted to one or more of the permitted uses listed in Article 4 for WRD. This front ground level space must maintain an open interior space in the front of the unit which is equal in depth to the interior width of the building. The depth shall be measured from the plane of the front façade, not the entrance door. In the event that multiple storefronts are used in combination for a development, this calculation shall be the greatest width of the individual storefronts used, not the total width. The rear of the first floor, as well as the second floor may include any permitted use of the district, provided that all other building and development requirements are met.
 - a. Multi-family residential shall be located on the second floor only.
 - b. Office-related uses are permitted as a secondary use in conjunction with another permitted use but not as a standalone use in WRD-1. *[Amended 6.21.21 ZA# 2021-06-21 (B)]*
2. Manufacturers of fine crafts, where the products are manufactured and sold onsite, are permitted as accessory uses in the Waccamaw Riverfront District, as specified in Article 4 – Use Tables. *[Amended 5/20/19 #ZA2019-05-20 (E)]*

E. Riverfront District Regulations

1. Standards for Development Site Review.
 - a. All new construction requiring a zoning permit shall have underground utility service from the nearest utility pole, including electric, cable television, and telephone service, unless this requirement is waived by the Technical Review Committee.
 - b. Structures, uses, and landscaping shall be arranged so not to interfere with the continued, active use of railroad facilities by present and future rail operators.
 - c. Sites shall be designed to accommodate persons with disabilities.
 - d. Landscaping shall be in compliance with Article 9.
 - e. With respect to vehicular, bicycle, and pedestrian circulation, special attention shall be given to the location and number of access points to the public street, sidewalk, or path, to the arrangement of parking areas, to service and loading areas, and to the location of accessible routes and ramps for the disabled. Common or shared driveways and walkways will be required.
 - f. Special attention shall be accorded to stormwater runoff so that the neighboring properties and/or the public stormwater drainage system are not adversely affected. Attention shall also be accorded to design features which address the affects of rain with particular attention to affects on the areas between buildings.
2. Riverfront Development Standards
 - a. Each approved application for development shall provide a Riverwalk area between the mean high-water mark of the river and all proposed structures and parking areas. The Riverwalk area shall be continuous, except as set forth in Section 6.4.1, and be no less than twenty-five feet (25') in depth across the river frontage of the property, such Riverwalk area being measured from, the mean high-water mark. Where lagoons and drainage swales occupy a substantial portion of the Riverwalk area because of natural land forms or drainage patterns additional width and/or vegetation may be required.

- b. If existing pilings can be established and recognized as usable for the boardwalk by the appropriate state and federal agencies, then the twenty-five feet (25') Riverwalk area can extend beyond the mean high-water mark. The Technical Review Committee will review the feasibility and desirability of these extensions on a case-by-case basis. *[Amended 2/18/19 #ZA2019-02-18 (C)]*
 3. Signage
 - a. Signage shall be in compliance with the requirements of Article 11 and the Waccamaw Riverfront HDRD Design Guidelines.

6.4.2 Planned Development District

- A. Minimum Development Standards
 1. A Planned Development project area shall contain a minimum of three (3) contiguous acres of land.
 2. The minimum lot size, maximum lot coverage, maximum height, maximum density, street width, and setbacks in a Planned Development shall meet general health, safety, and welfare requirements and be in harmony with good planning practices as determined by the Planning Commission.
- B. Common Open Space Requirements
 1. Developments which require the dedication of Open Space shall adhere to the standards in Section 10.3.9, Parks and Open Space Dedication.
- C. Utilities, Services, and Easements
 1. Structures within a Planned Development shall be connected to city water and sewer lines and all utility lines shall be placed underground. Adequate provisions to maintain on-site and off-site drainage shall be provided. Adequate provisions for utility and drainage easements shall be provided.
- D. Access and Circulation
 1. A circulation system shall be designed so as to provide for safe and convenient access to dwelling units, open space, community facilities, commercial uses, and industrial uses in the Planned Development.
 2. Principal vehicular access points shall be designed to permit smooth traffic flow and minimum hazards to vehicular, bicycle, or pedestrian traffic.
 3. Cul-de-sacs and dead-end streets shall be prohibited unless no other reasonable alternative is available (See Section 7.1.11). The use of cul-de-sacs solely for the purpose of increasing street frontage in order to provide additional lots shall be prohibited.
 4. Adequate access and circulation for emergency and service vehicles shall be provided, in accordance with applicable fire code(s).
 5. Sidewalks and bicycle lanes shall be designed to meet the requirements of Section 7.1.
- E. Application and Approval of Planned Developments
 1. Prior to a formal application being filed to rezone property to Planned Development, a sketch plan shall be submitted to the Planning Department. Such sketch plan shall illustrate the boundaries of the proposed area to be rezoned to Planned Development; the proposed land uses, a proposed lot layout and street configuration, estimated gross densities, and estimated useable open space.
 2. The proposed sketch plan shall be submitted to the Planning Commission with a staff recommendation as to whether the proposed design concept is consistent with the

Section 6.6 – General Design Standards**6.6.1 Outdoor Lighting**

A. Nonresidential and multifamily buildings and projects, including outparcels, shall be designed to provide safe, convenient, and efficient lighting for pedestrians and vehicles. Lighting shall be designed in a consistent and coordinated manner for the entire site. The lighting and lighting fixtures shall be integrated and designed so as to enhance the visual impact of the project on the community and/or should be designed to blend into the surrounding landscape. Lighting design and installation shall ensure that lighting accomplishes on-site lighting needs without intrusion on adjoining properties.

B. Applicability

The requirements of this section shall apply to all nonresidential or multifamily development, as well as all residential subdivision development.

C. Exempt

1. The following activities are exempt from the requirements of this Section.
 - a. Outdoor lights used for a temporary event are permitted through a Temporary Use Permit.
 - b. Outdoor lights used exclusively for recreational activities, concerts, plays or other outdoor events that are open to the public, provided that the event or function meets all other applicable requirements in this UDC. Such lighting shall be located at least 50 feet from any adjoining residential district or use.
2. Outdoor lighting exempt from the Section shall only be illuminated while the activity takes place and during high traffic periods immediately before and after the event.

D. Lighting Plan

A site lighting plan shall be required as part of the application review for all areas proposed for illumination that exceeds 40,000 square feet in area. Projects with multiple areas proposed to be illuminated (such as separate parking lots) shall submit a site lighting plan if the sum of the multiple areas exceeds 40,000 square feet.

E. Site Lighting Design Requirements

Lighting shall be used to provide safety while accenting key architectural elements and to emphasize landscape features. Light fixtures shall be designed as an integral design element that complements the design of the project. This can be accomplished through style, material or color. All lighting fixtures designed or placed so as to illuminate any portion of a site shall meet the following requirements:

1. Fixture (Luminaire)
 - a. The light source shall be completely concealed behind an opaque surface and recessed within an opaque housing and shall not be visible from any street right-of-way or adjoining properties. Overhead lighting fixtures shall be designed to prevent light from emitting upwards towards the sky.

- b. Under canopy lighting fixtures should be completely recessed within the canopy.
- 2. Fixture Height
 - a. Lighting fixtures shall be a maximum of forty (40') feet in height within the parking lot and shall be a maximum of fifteen (15') feet in height within non-vehicular pedestrian areas. All light fixtures located within fifty (50') feet of any residential use or residential property boundary shall not exceed fifteen (15') feet in height.
 - b. The Planning Director may allow fixtures above this height to provide internal lighting for stadiums, arenas, and similar facilities.
- 3. Light Source (Lamp)
 - a. Incandescent, florescent, metal halide, or color corrected high-pressure sodium are preferred. The Planning Director shall have the authority to approve other lamp types (including light emitting diodes (LEDs) and fiber optics) provided the color emitted is similar to the preferred types. Non color corrected high pressure sodium lamps are prohibited.
 - b. The same light source type must be used for the same or similar types of lighting on any one site throughout any development.
- 4. Mounting. Fixtures shall be mounted in such a manner that the cone of light is contained on-site and does not cross any property line of the site.
- 5. Limit Lighting to Periods of Activity. The use of sensor technologies, timers or other means to activate lighting during times when it will be needed may be required by the Planning Director to conserve energy, provide safety, and promote compatibility between different land uses.

F. Illumination Levels

- 1. All site lighting shall be designed so that the level of illumination as measured in foot-candles (fc) at any one point meets the standards in Table 6.4 with minimum and maximum levels measured on the pavement within the lighted area and average level (the overall generalized ambient light level), measured as a not-to-exceed value calculated using only the area of the site intended to receive illumination.
- 2. The maximum level of illumination at the outer perimeter of the site or project shall be 0.5 foot-candles when abutting a residential zoning district and 5.0 foot-candles when abutting all other districts and/ or streets.

Table 6.4: Light Levels for Different Types of Lighting

Light Levels (Foot Candles)			
Type of Lighting	Minimum	Average	Maximum
Architectural Lighting	0.0	1.0	5.0
Canopy Area Lighting	2.0	10.0	15.0
Multifamily Parking Lot	0.2	1.0	8.0
Nonresidential and Multifamily Entrances	1.0	5.0	15.0
Nonresidential Parking Lot	0.2	1.5	10.0
Storage Area (security lighting)	0.2	1.0	10.0
Vehicle Sales and Display	2.0	3.0	15.0
Walkways, Landscape or Decorative Lighting	0.2	0.8	5.0

G. Excessive Illumination

1. Lighting within any lot that unnecessarily illuminates and substantially interferes with the use or enjoyment of any other property is prohibited. Lighting unnecessarily illuminates another lot if it exceeds the requirements of this Section.
2. All outdoor lighting shall be designed and located such that the maximum illumination measured in footcandles at the property line does not exceed 0.2 on neighboring residential uses, and 0.5 on neighboring commercial sites and public rights-of-way.
3. Lighting shall not be oriented so as to direct glare or excessive illumination onto streets in a manner that may distract or interfere with the vision of drivers on such streets.
4. Fixtures used to accent architectural features, landscaping or art shall be located, aimed or shielded to minimize light spill into the night sky.

6.6.2 Mechanical Equipment, Dumpster, Recycling, and Trash Handling**A. Applicability**

This section shall apply to all residential and non-residential development, light industrial, and heavy industrial use. This includes any outdoor type of trash container or recycling container that is larger than the 95-gallon roll-out carts used by the City of Conway for weekly domestic pickup except in the case where a site uses more than one 95-gallon trash container.

B. Location

All mechanical equipment (including heating or air conditioning units and other mechanical equipment) and trash handling facilities shall be located on the same lot as the use served unless shared facilities are approved. The location of all utilities and trash handling facilities shall be in the rear or side yards. No such facilities shall be located in the required street yard.

C. Screening

1. All ground level mechanical equipment (including heating or air conditioning units and other mechanical equipment) and trash-handling facilities shall be completely screened from the public right-of-way and adjacent properties pursuant to Section 9.3. Mechanical equipment on rooftops shall be screened from the view of the public street.
2. A wall, solid wood fence, evergreen hedge, earth berm, or any combination thereof may be provided to obscure such facilities pursuant to Section 9.4.1; however, when the service side of the particular facility faces any property line, a minimum six (6) foot wall or solid wood fence with gates or doors shall be provided.
3. Landscaping of the entire service area shall be installed in accordance with the landscape buffer requirements listed in Section 9.3.

D. Access

1. All required trash handling facilities shall be designed with appropriate means of access to a street or alley in a manner that will least interfere with traffic movement, and will most facilitate the service of the facilities.

E. Utilization

1. Space allocated to any trash handling facilities shall not be used to satisfy the space requirements for off-street parking or loading facilities, nor shall any parking or loading spaces be used to satisfy the space requirements for any trash handling facility.

F. Performance

1. All trash handling facilities shall be designed to prevent wind-blown debris from leaving the site.
2. All food-related businesses shall provide water quality treatment in conformance with applicable standards and design guidelines for runoff from trash handling facilities.

G. Additional Requirements

1. The Technical Review Committee may impose additional requirements as necessary to protect public health and safety.

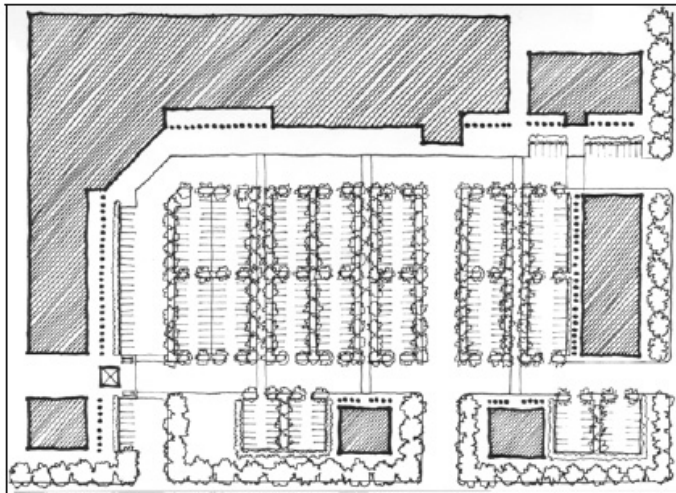
Section 8.2 – Design Standards

8.2.1 General Design Standards

- A. Unless no other practicable alternative is available, any off-street parking area shall be designed so that, without resorting to extraordinary movements, vehicles may exit such areas without backing onto a public street.
- B. No parking is allowed in yards or landscaping between the hours of 11:00 p.m. and 8:00 a.m.
- C. Every off-street parking space shall be arranged so that any vehicle may be moved into and out of such space without moving another vehicle.
- D. Circulation areas shall be designed so that vehicles can proceed safely without posing a danger to pedestrians and without interfering with parking areas.
- E. All open off-street parking areas, except for single family, two-family and townhouse dwellings, shall provide concrete curb and gutter per approval of the Technical Review Committee to ensure that no portion of a vehicle encroaches into the required setback. Plans for surfacing and drainage of driveways and stalls for five (5) or more vehicles shall be submitted to the Technical Review Committee for review. All plans shall be subject to the Technical Review Committee's written approval.
- F. Any lighting used to illuminate an off-street parking area shall be hooded and so arranged as to reflect the light away from adjoining property, abutting residential uses and public rights-of-way.
- G. All open, non-residential off-street parking areas of five (5) or more spaces shall be screened and landscaped from abutting or surrounding residential districts.
- H. All commercial driveway and parking areas shall be paved with asphalt, concrete, brick pavers, or alternative paving surfaces except for areas used for overflow, special events, and peak parking.

8.2.2 Parking Lot Design

Parking lots (over 100 parking spaces) shall have designated landscaped pedestrian/ bike pathways to improve pedestrian and bicycle connections and safety.



8.2.3 Handicapped Accessible Parking Requirements

A. Regulations and dimensions for handicapped parking spaces shall be per requirements of the Americans with Disabilities Act (ADA) (Public Law 101-136) and Building Code adopted by the City of Conway or the State of South Carolina.

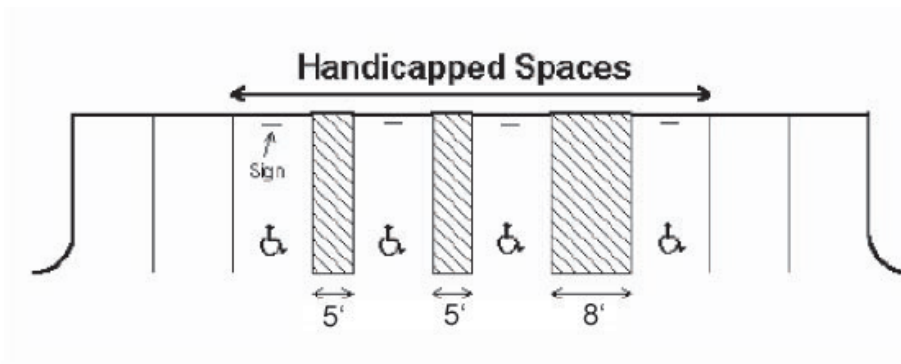
1. Required Number. The required number of handicapped accessible spaces, which must be provided on-site, shall be as provided in Table 8.1. In some instances, ANSI requirements may be used to calculate required spaces, and whichever is more restrictive shall be used to calculate required spaces.

ADA spaces shall count toward the requirements for off-street parking as specified in Table 8.3. In addition, handicapped van spaces are required at a rate of one van space for each eight (8) handicapped spaces required, with a minimum of one.

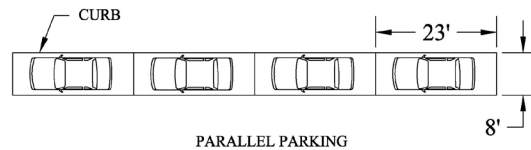
2. Locations. Accessible parking spaces serving a particular building shall be located on the shortest accessible route of travel from adjacent parking to an accessible entrance. In parking facilities that do not serve a particular building, or buildings with multiple entrances, accessible parking shall be located on the shortest accessible route of travel to an accessible pedestrian entrance of the parking facility.
3. Dimensions. Accessible parking spaces shall be at least nine (9) feet wide by nineteen (19) feet in depth. Accessible parking spaces must have a minimum five (5) foot-wide access aisle. For van spaces, the width of the parking space shall be at least eleven (11) feet wide with a minimum five (5) foot wide access aisle. Parking access aisles shall be part of an accessible route to the building or facility entrance; two accessible parking spaces may share a common access aisle.
4. Signs. Accessible parking spaces shall be designated as reserved by a sign showing the symbol of accessibility, per applicable state law requirements. Such signs shall be located so that they cannot be obscured by a vehicle parked in that space.

Table 8.1: ADA Parking Requirements

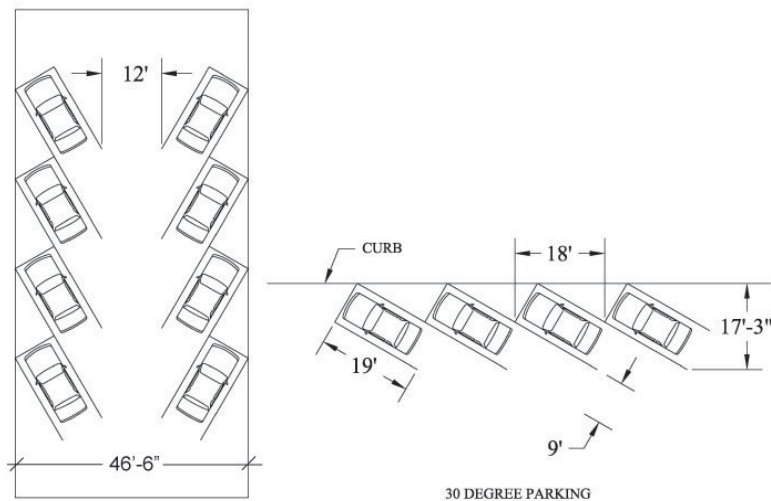
Total Parking in Lot	Required Minimum Number of Accessible Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
100 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2% of total
1,001 and over	20, plus 1 for every 100 spaces over 1,000

ADA Spaces Detail**8.2.4 Minimum Parking Space and Aisle Dimensions****A. One-way traffic flow:**

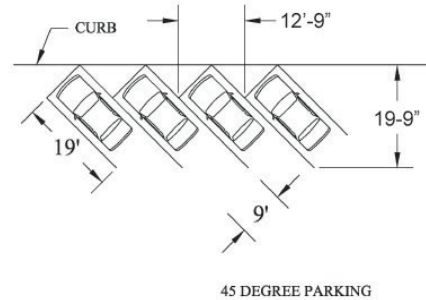
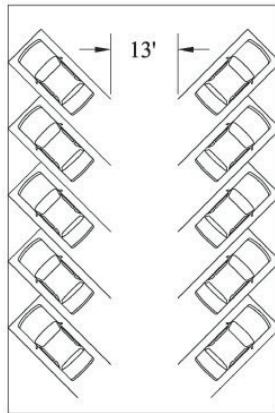
1. Parallel: Aisle Width: Twelve (12') feet
Stall Dimensions: Eight (8') feet by twenty-three (23') feet



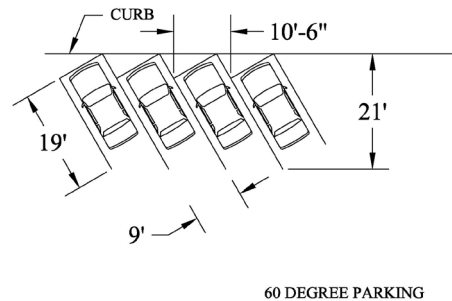
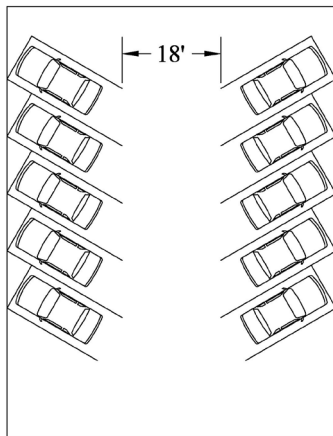
2. Thirty (30) Degree: Aisle Width: Twelve (12') feet
Stall Dimensions: Nine (9') feet by nineteen (19') feet
Angular Parking Dimension: Seventeen (17') feet, three (3") inches



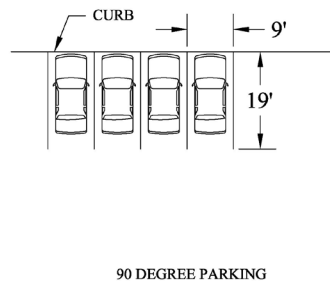
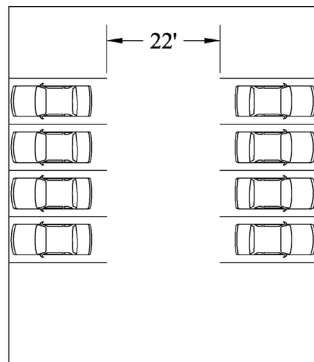
3. Forty-Five (45) Degree: Aisle Width: Thirteen (12'-9") feet
 Stall Dimensions: Nine (9') feet by nineteen (19') feet
 Angular Parking Dimensions: Nineteen (19') feet, eight (9")



4. Sixty (60) Degree: Aisle Width: Eighteen (18') feet
 Stall dimension: Nine (9') feet by nineteen (19') feet
 Angular Parking Dimension: Twenty-one (21') feet

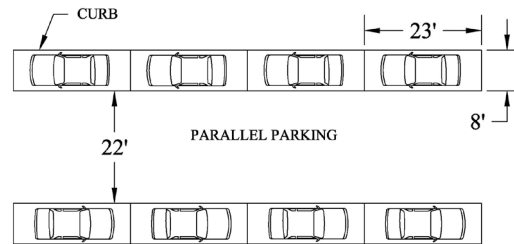


5. Ninety (90) Degree: Aisle Width: Twenty-two (22') feet
 Stall Dimensions: Nine (9') feet by nineteen (19') feet
 Angular Parking Dimension: Nineteen (19') feet

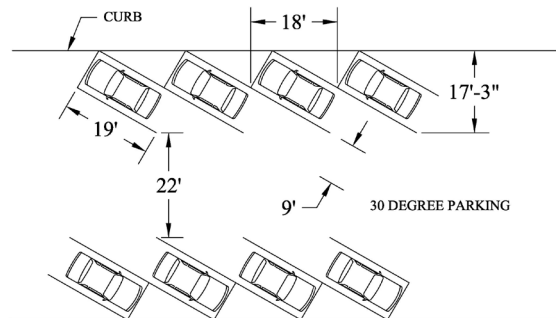


B. Two-way traffic flow:

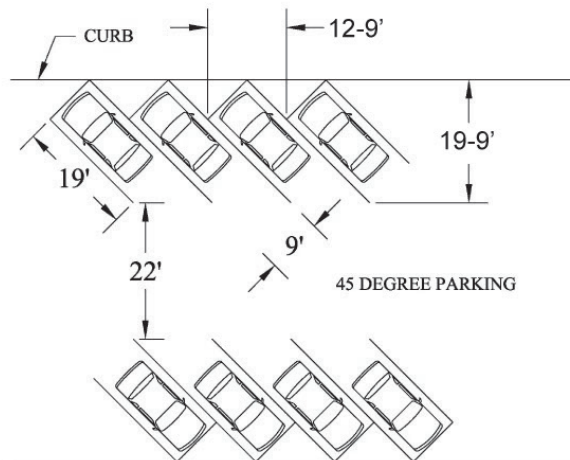
1. Parallel: Aisle Width: Twenty-two (22') feet
 Stall dimensions: Eight (8') feet by twenty-three (23') feet
 Parallel Parking Dimension: Eight (8') feet



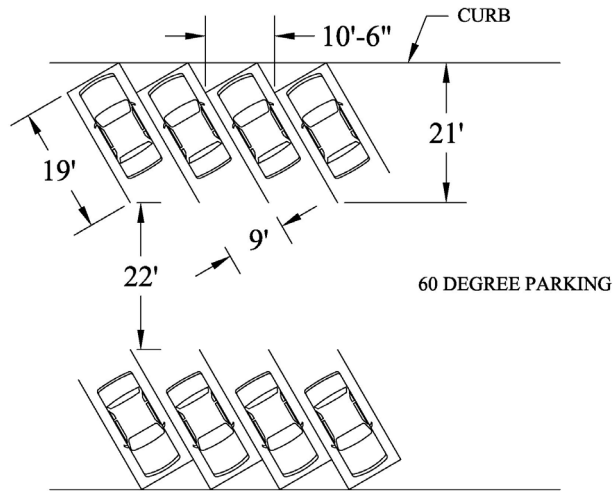
2. Thirty (30) Degree: Aisle Width: Twenty-two (22') feet
 Stall Dimensions: Nine (9') feet by nineteen (19') feet
 Angular Parking Dimension [two (2) spaces]: Thirty-four (34') feet, six (6") inches



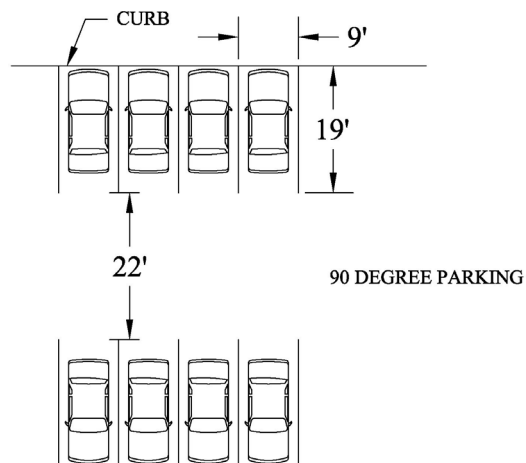
3. Forty-five (45) Degree: Aisle Width: Twenty-two (22') feet
 Stall Dimensions: Nine (9') feet by nineteen (19') feet
 Angular Parking Dimension [two (2) spaces]: Thirty-nine (39') feet, four (4") inches



4. Sixty (60) Degree: Aisle Width: Twenty-two (22') feet
 Stall Dimensions: Nine (9') feet by nineteen (19') feet
 Angular Parking Dimension [two (2) spaces]: Forty-two (42') feet



5. Ninety (90) Degree:
 Aisle Width: Twenty-two (22) feet
 Stall Dimensions: Nine (9') feet by nineteen (19') feet
 Angular Parking Dimension [two (2) spaces]: Thirty-eight (38') feet

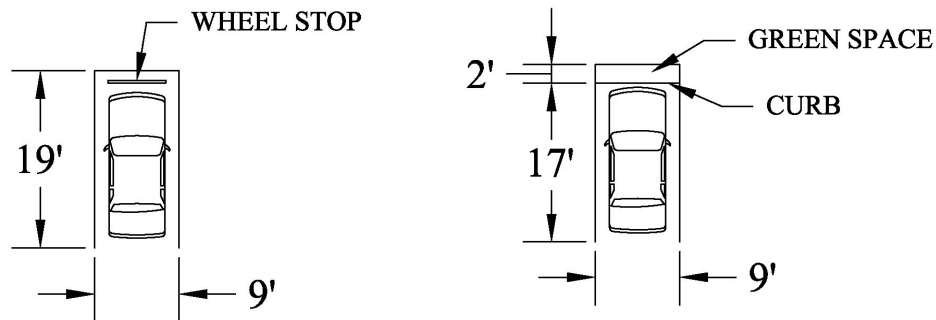


8.2.5 Paving Surfaces

- A. **Typical Paving Surfaces.** The paving surface of all on-site and off-site parking areas shall be a dust free, all weather material (i.e. asphalt, concrete, brick pavers). The paving surface shall have the parking stalls, loading and unloading zones, fire lanes and any other applicable designations delineated in white or yellow paint.
- B. **Alternative Paving Surfaces.** Alternative paving surfaces include pervious paving, concrete pavers, and similar materials shall be acceptable with the approval of the Technical Review Committee.
1. Any non-paved surface used for overflow, special events, and peak parking that cannot be maintained with healthy, living turf grass or similar ground cover shall be paved with pervious pavement or brick pavers.
 2. Pervious pavers or concrete, stabilized grass lawn, or other pervious parking surfaces may be permitted for specific uses as set forth below.
 3. All driveways, access aisles and parking spaces (excluding handicapped) may be surfaced with grass lawn or other pervious parking surface for the following:
 - a. Uses within or near environmentally sensitive areas.
 - b. Uses which require parking on an average of less than five days per week during a month;
 - c. Parks, playgrounds, ballfields, football and baseball stadiums, fairgrounds, and other similar outdoor recreation areas; and
 - d. Surplus parking areas above the required number of parking spaces (see Section 8.2.10, Parking Requirements for Specific Uses).

8.2.6 Wheelstop Requirements

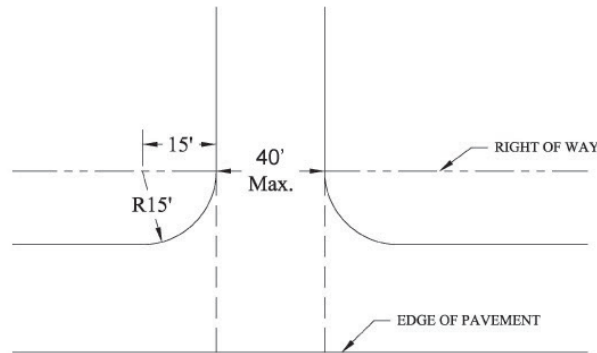
- A. Wheelstops shall be required in all handicapped parking spaces.
- B. Wheelstops shall be required in all parking spaces that abut sidewalks and parking spaces that abut landscape areas where the parking area does not contain curbing.
- C. Wheelstops shall be located thirty (30) inches on center from the front of the parking space. The stop shall be a minimum of four (4") inches in height and shall have the capacity to stop both wheels of a vehicle. Parking Spaces abutting sidewalk can eliminate wheelstops only by providing a two (2) foot green space between the curb and required landscape areas. Green space may be counted in required buffer; however, required plants cannot be placed within the two (2) foot green space; in the case where the two (2) foot buffer is used parking spaces can be 9x17 in size. (See Figure 1 below).
- D. Wheelstop requirements can be waived if the sidewalk in front of the parking space is at least six (6) feet in width and is raised at least four (4) inches.

**Figure 1**

8.2.7 Entrance and Exit Curb Cuts

All curb cuts on public streets shall meet the following requirements:

- A. **Maximum.** A maximum of one (1) driveway per lot shall be permitted. Additional driveways may be permitted with the approval of the TRC and/or SCDOT. Curb cuts shall not exceed forty (40) feet in width.
- B. **Application.** Parcels zoned HC, LI, or HI, which share frontage with residential neighborhoods on a residential local street, residential cul-de-sac, or residential alley shall be permitted a curb cut onto such street classification upon review and approval of the Technical Review Committee, and approval by Planning Commission and City Council if it involves a major subdivision or extension of a roadway. If a parcel fronts on a major arterial, minor arterial, collector, sub-collector, non-residential local, non-residential cul-de-sac, and non-residential alley, curb cuts which meet all other applicable requirements set forth herein shall be permitted. Street classifications are defined in Section 7.1.3.
- C. Curb cuts shall not intersect a sight triangle and shall not be located within twenty-five (25') feet of another curb cut.
- D. The required minimum radius shall be fifteen (15') feet as illustrated in the figure below.



8.2.8 Off-Premises Parking

Required off-street parking spaces may be located off-premises provide the following requirements are met:

- A. A minimum of seventy-five (75%) percent of all required parking spaces shall be located on the premises for which the use is located, and a maximum of twenty-five (25%) percent of such required parking spaces may be located on one (1) off-premise parking area. Off-premise parking areas and the premises on which the use is located shall be separated by a maximum of two hundred (200) feet. Such distance shall be measured between the closest point of such off-premise parking area and the most direct public access walking route.
- B. The property owner of off-premises parking areas shall restrict the use of land for such off-premise parking by means of a properly recorded deed restriction, which shall require the written consent of the City of Conway to release such property from the restriction. Such restriction shall be in effect for as long as the use which requires such off-premises parking exists, or other parking arrangements are made.

- C. Off-premises parking areas shall be permitted in any zoning district where the use served by such off-premise parking is permitted.
- D. Paved sidewalks from the off-premises parking area to the use such off-premise parking serves shall be required at the owner's expense. Such sidewalks shall be constructed to SCDOT Standards.
- E. Off-premise parking areas shall be clearly designated by appropriate signage indicating the use served by such off-premise parking. Such signage shall adhere to requirements set forth in Article 11.
- F. Off-premise parking areas shall meet all requirements for on-premise parking including but not limited to parking design standards, landscaping requirements, signage, and design review requirements.

8.2.9 Joint-Use Parking

- A. Certain adjacent developments/businesses may be permitted to make joint utilization of a maximum of fifty (50%) percent of the required parking spaces, as required in Section 8.2.10, provided the peak hours of attendance of one use is during the time the adjacent use is not in operation. Such situations shall include, but are not limited to, religious institutions, theaters, or assembly halls where the peak hours of attendance are at night or on weekends, and the adjacent use is closed at night or on weekends. The shared parking areas shall be interconnected by vehicular access ways and/or sidewalks. If adjacent uses utilizing shared parking change to where the hours of peak operation are the same, the total required number of parking spaces for each use shall be installed to meet the standards set forth herein. Joint -parking agreements between the entities utilizing joint-use parking shall be submitted to the City of Conway for review and approval.
- B. A joint-use parking plan shall be enforced through written agreement among the owners of record. An attested copy of the agreement shall be submitted to the Horry County Register of Deeds for recordation. Proof of recordation of the agreement shall be presented to the Conway Planning Department prior to issuance of a Building Permit. A parking agreement may be revoked by the parties to the agreement only if off-street parking is provided pursuant to this Section or if an alternative parking plan is approved by the Planning Commission and Planning Department Staff.

8.2.10 Master Parking Plan for Institutional Uses

The Conway Planning Commission may authorize a reduction in the number of required off-street parking spaces for campuses that have different peak parking demands or different operating hours. Parking plans shall be subject to the following standards:

- A. **Location.** Off-street parking spaces shall be located no farther than 200 feet from the closest point of such parking area and the most direct public access walking route. The Planning Commission may waive this distance limitation, if written assurances are made that van, shuttle or other acceptable methods or means shall be operated between the lot and the principal use.

- B. **Zoning Classification.** Parking areas shall be considered accessory uses of principal uses that the parking spaces are intended to serve. Parking areas must be the same or a more intensive zoning classification than the zoning classification of the building or grounds area that is to be served.
- C. **Required Study and Analysis.** The applicant shall submit a shared parking analysis to the Planning Commission that clearly demonstrates the feasibility of the proposed parking arrangement. The study shall address, at a minimum, the size and type of the proposed development, the composition of tenants, the anticipated rate of parking turnover, and the anticipated peak parking and traffic loads for all uses that shall be sharing off-street parking spaces. The Planning Commission shall have the authority to require a revised study and analysis should conditions change that may result in a change in site parking conditions.
- D. **Revocation.** Failure to comply with the parking provisions of this Section shall constitute a violation of the Conway UDO and shall specifically be cause for revocation of a Certificate of Occupancy.
- E. **Periodic Review.** The Planning Department may require review of parking plans and amendments as needed.

8.2.11 Queuing Lanes

In addition to the number of spaces required in Section 8.2.12, uses which provide drive-thru services shall provide queuing lanes to accommodate the stacking of vehicles. A nine (9') feet by nineteen (19') feet area shall be provided for each car length required in Table 8.2.

Table 8.2: Queuing Lane Requirements

TYPE OF USE	NUMBER OF CAR LENGTHS
Restaurants	8 car lengths per window
Financial Institutions	4 car lengths per window or automated teller machine
Dry Cleaners	4 car lengths per window
Car Washes	5 car lengths per automatic wash bay; 3 car lengths per manual wash bay
Retail Establishments	4 car lengths per window

8.2.12 Parking Requirements for Specific Uses

Off-street vehicle parking areas shall be provided for every use here after established. The following table sets forth the use classifications and the minimum number of spaces required for each classification.

Table 8.3: Parking Requirements

Type of Use	Minimum Parking Required	Maximum Parking Allowed
Residential Uses		
Single-family attached	2 spaces per unit.	N/A
Two family dwelling (duplex)	2 spaces per unit.	Three spaces per unit.
Residence within building containing a non-residential use	One space per unit.	1.5 spaces per unit.
Apartment, One Bedroom	1.5 spaces per unit plus 0.1 per unit for guest space.	2 spaces per unit plus 0.2 per unit for guest space.
Apartment, Two Bedroom	1.5 spaces per unit plus 0.1 per unit for guest space.	2 spaces per unit plus 0.2 per unit for guest space.
Apartment, Three Bedroom	2 spaces per unit plus 0.2 per unit for guest space.	3 spaces per unit plus 0.2 per unit for guest space.
Apartment, Four Bedroom	3 spaces per unit plus 0.2 per unit for guest space.	4 spaces per unit plus 0.2 per unit for guest space.
Rooming House	1 space per 2 sleeping rooms.	1 space per sleeping room.
Public-Institutional Uses		
Institutions of higher education	1 space per two (2) students; 1 space per faculty and staff at capacity class attend.	1 space per student; 1.5 spaces per faculty and staff at capacity class attend.
High schools	10 per classroom, or 1 per 3 seats in auditorium or principal place of assembly, whichever is greater.	15 per classroom, or 1 per 3 seats in auditorium or principal place of assembly, whichever is greater.
All other educational facilities	1.5 spaces per classroom/ administrative office	2.0 spaces per classroom/ administrative office
Hospital	2 spaces per patient bed	3 spaces per bed.
Nursing homes; Rest homes; Homes for the aged	1 space per 4 patient beds	1 space per 3 patient beds.
Government Institutions	1 space per 300 sq. ft of gross floor area	1 space per 200 sq. ft. of gross floor area
Theaters; Auditoriums; Public Assembly	1 space per 4 seats	1 space per 3 seats
Religious Institutions	1 space per 6 seats in main assembly area	1 space per 3 seats in main assembly area.

[Amended #ZA2017-01-03 (C) and (D)]

Type of Use	Minimum Parking Required	Maximum Parking Allowed
Recreational Facilities		
Athletic Field	20 spaces per field	N/A
Billiard hall/amusement arcade	1 per 200 square feet	1 per 112.5 square feet
Bowling alley	1 per each bowling lane (add parking for billiard hall/ amusement arcade, if provided)	3 per each bowling lane (add parking for billiard hall/ amusement arcade, if provided)
Community center	1 per 300 sq. ft.	1 per 250 sq. ft.
Ice or roller skating rink	1 per 200 sq. ft.	1 per 150 sq. ft.
Miniature golf	2 per hole	3 per hole
Golf driving range, principal use	0.75 space per tee	1 space per tee.
Golf Course	2.5 spaces per hole	3 spaces per hole
Swimming pool – subdivision community	1 per 150 sq. ft. of surface water area.	1 per 100 sq. ft. of surface water area.
Health or fitness club	1 per 200 sq. ft.	1 per 150 sq. ft.
Swimming pool – public	1 per 125 sq. ft. of surface water area.	1 per 75 sq. ft. of surface water area.
Tennis or racquet ball court	2 per court	4 per court
Theater, cinema	1 per 4 fixed seats	1 per 3 fixed seats
Commercial Uses		
Retail establishments such as clothing, notions, music, arts, gifts, sporting goods, hobbies, etc.	1 space per 200 sq. ft. of gross floor area.	1 space per 150 sq. ft. of gross floor area.
Retail Establishments such as furniture, hardware, appliances, etc.	1 space per 400 sq. ft. of gross floor area.	1 space per 225 sq. ft. of gross floor area.
Grocery Stores & Specialty Food Stores	1 space per 400 sq. ft. of gross floor area.	1 space per 225 sq. ft. of gross floor area.
Commercial strip center	1 space per 275 sq. ft.	1 space per 168.5 sq. ft.
Restaurants; Nightclubs; Bars; Taverns	One space per 125 sq. ft.	1 space per 56.25 sq. ft.
Service Establishments (not set forth elsewhere herein)	1 space per 300 sq. ft. of gross floor area.	1 space per 187.5 sq. ft. of gross floor area.
Day Care Center	1 space per 500 sq. ft. of gross floor area	1 space per 281.5 sq. ft. of gross floor area
Professional & Business Offices	1 space per 300 sq. ft. of gross floor area	1 space per 187.5 sq. ft. of gross floor area
Doctor & Dentist Offices	1 space per 250 sq. ft. of gross floor area	1 space per 150 sq. ft. of gross floor area
Financial Institutions	1 space per 300 sq. ft. of gross floor area	1 space per 187.5 sq. ft. of gross floor area
Hotel, Motel, and Bed & Breakfast Inns	1 space per room or suite available for rent	1.5 per room or suite available for rent
Funeral Homes; Mortuaries	1 space per 4 seats in chapel or parlor	1 space per 5 seats in chapel or parlor
Car sales; Manufactured housing sales; Outdoor Equipment sales	1 space per 2000 sq. ft. of gross sales lot area.	1 space per 2500 sq. ft. of gross sales lot area.
Service Stations	2 spaces per service bay	3 spaces per service bay
Marinas	0.25 space per boat slip	3 spaces per boat slip
Doggie Daycare/Spa	1 space per 500 sq. ft. of gross sales lot area.	1 space per 300 sq. ft. of gross sales lot area

Type of Use	Minimum Parking Required	Maximum Parking Allowed
Industrial Uses		
Manufacturing; Assembly	1 space per 1,000 sq. ft. of gross floor area.	1 space per 600 sq. ft. of gross floor area.
Warehouse	1 space per 1,500 sq. ft. of gross floor area.	1 space per 1,000 sq. ft. of gross floor area.
Wholesale	1 space per 1,000 sq. ft. of gross floor area.	1 space per 600 sq. ft. of gross floor area.

8.2.13 Compact Car Spaces

- A. Design requirements for compact car spaces:
 - 1. Compact car spaces shall have a required stall dimension of 8 feet by 17 feet.
 - 2. Compact car parking spaces shall be allowed only as 90-degree angle parking.
 - 3. Compact car parking spaces shall be clearly identified by signing or other marking as approved by the Planning Director.
 - 4. No more than four (4) compact car parking stalls shall be placed side-by-side, or eight (8) head-to-head.
- B. Compact car spaces shall be permitted in districts as followed:
 - 1. Professional (P), Neighborhood Commercial (NC), Core Commercial (CC), and Institutional Campus (IC) shall have a maximum of 20% of required parking as compact car spaces.
 - 2. Highway Commercial (HC), Institutional (IN), Light Industrial (LI), and Heavy Industrial (HI) shall have a maximum of 10% of required parking as compact car spaces.

Section 9.3 – Landscaping Design Standards

9.3.1 Landscaping Design Standards

- A. Calculation of Street Landscaping: Street landscaping rate and width calculations shall exclude access drives.
- B. All plant materials used to satisfy requirements set forth herein shall be suitable for the climatic characteristics of Conway (USDA Climate Zone 8). The recommended plant list in Section 9.5.1 lists species of plants that are suitable for the climate of Conway.
- C. Equal spacing of plant material installed to satisfy requirements set forth herein shall not be required. Plant materials shall be grouped and clustered in order to present a more natural appearance. However, not more than 50% of each required plant material shall be grouped or clustered. The remainder of the materials shall be distributed throughout the landscaping.
- D. Existing trees may be counted as canopy or understory as set forth herein provided such trees are a minimum of five (5) inches in caliper, in good health and located in the approximate area as required herein. Invasive species shall not be counted toward existing trees.

A twenty-five (25') foot strip of undisturbed woodlands preserved between the parking area and right-of-way shall be permitted in lieu of the requirements set forth in Section 9.2.3

- E. **Canopy trees.** Any tree that reaches a mature height in excess of forty (40) feet. Canopy trees shall be a minimum of three (3) inches in caliper and twelve (12) to fourteen (14) feet in height at the time of installation.
- F. **Understory trees.** Understory trees shall be a minimum of six (6) feet high and one inch in caliper, measured six inches above grade, when planted. When mature, an understory tree should be between fifteen (15) and forty (40) feet high.
- G. **Palm trees.** Palm species trees shall be used as an ornamental or decorative tree only. Palm trees shall not be permitted to meet the minimum landscaping requirements (i.e. buffers, perimeter parking requirements, mitigation requirements, etc.).
- H. **Shrubs.** Shrubs shall be a minimum of three (3) gallons in size and one and one-half (1.5) feet in height at the time of installation. Where this ordinance specifically requires "tall shrubs", such shrubs shall be a minimum of three (3) gallons in size and three (3) feet in height at the time of installation and shall reach a minimum mature height of five (5) feet.
- I. **Groundcover.** Groundcover shall be grass, turf, sod, ivy, bulbs, potted flowers, and bedding plants. Pebbles, wood chips, bark, mulch, straw, and similar materials may be used in conjunction with groundcover to delineate planting beds, but in no instance shall such materials be used for the purpose of sidewalks, parking areas, or driveways. Areas dedicated for lawns shall be cleared of debris, graded level, and covered with sod, turf, or grass seed.

- J. **Berms.** Berms constructed to satisfy buffer requirements stated herein shall be physical barriers which screen incompatible land uses. If included in the landscape design, berms shall be:

1. Constructed with a maximum slope of one foot of rise to three feet of run (3:1).
2. Have a minimum crown of six (6) feet in width.
3. The width of any required buffer for residential uses abutting industrial property, or for industrial uses abutting residential properties shall be a minimum height of 4 feet with a maximum slope of 3:1.
4. Have a maximum slope of 4:1 when greater than six feet in height, as measured from the exterior property line.
5. Designed and constructed with an undulating appearance which mimics as much as is practicable a natural topographical feature of the site.
6. Substantially planted and covered with live vegetation. No berm shall consist entirely of turf grass, ground cover, mulch or similar material. If a berm is greater than 2 feet in height all trees shall be arranged so that they are planted within 2 vertical feet of the natural grade.
7. Fully installed, planted and stabilized prior to issuance of certificate of occupancy.
8. Designed to prevent standing water or to impede the flow of storm water from adjacent properties.
9. Free of structures, including fences, unless approved by the City of Conway as part of the landscaping requirements for a development site.

- K. **Walls and Fences.** Walls and fences constructed to satisfy buffer requirements stated herein shall be placed in a linear, serpentine, or stepped alignment and shall be a minimum of five (5) feet in height, but shall not exceed ten (10) feet in height. Such wall and fences shall be solid, continuous structures that screen incompatible lands uses. Walls shall be constructed of brick, stone, stucco, or concrete. Fences shall be constructed of wood or synthetic wood material. Other materials and colors may be approved either by the TRC or CAB.

As an alternative screening requirement for parking lot edge(s) which abut public street rights-of-way in the Central Business District or Core Commercial District, a three (3) feet masonry wall to provide casual seating may be installed in place of the continuous row of shrubs.

In landscape buffers that require wooden fences or masonry walls, the required plant materials shall be installed on the opposite side of the fence from the subject parcel. The finished side of the fence to be required on the outside of the property. In order to provide the finished side of the fence on the inside of the property, written documentation that is notarized and witnessed shall be provided by the adjacent property owner(s) to the Planning Director.

- L. Subject parcel boundaries adjacent to delineated wetlands (as determined by U.S. Army Corp of Engineers) which would normally require landscape buffers as determined in this Article shall be exempt from such required buffer provided the width of the delineated wetlands is a minimum of thirty (30) feet. If the adjacent wetlands should be mitigated and approved for development at any time in the future, the required buffer shall be installed on the subject parcel.

- M. For the purposes of determining landscape buffer requirements, rights-of-way shall not impede the contiguity of parcels. However, to prevent land uses from being completely buffered from a public right-of-way and the view of passing traffic, the buffer requirement set forth in Table 9.1 may be reduced by one letter for parcel boundaries adjacent to arterial, collector, and sub-collector streets. The requirement for tall shrubs in such landscape buffers may also be reduced to small shrubs.
- N. No structures or portions of structures (except structures required in conjunction with public utility services) shall be permitted in a required landscape buffer. Ingress and egress shall be permitted through required landscape buffers in accordance with Section 9.2.
- O. Adjacent commercial parcels shall provide pedestrian and bicycle connectivity when a buffer separates parcels. A minimum six (6) feet walkway shall be provided to allow pedestrian and bicycle access between parcels. The material used for walkways located within natural areas shall allow for the percolation of water into the ground. Suitable materials include wooden decking, crushed gravel, and pervious pavement as approved by the Technical Review Committee.

9.3.2 Minimum Landscaped Area Required

Each property shall devote a minimum of fifteen (15%) percent of its total area to landscaping which may include existing or transplanted trees, shrubs, hedges, and lawns. Paved areas, gravel areas, and retention/detention ponds shall not be calculated as part of the minimum fifteen (15%) percent.

9.3.3 Preservation of Existing Trees and Vegetation

Existing trees and vegetation shall be incorporated into the landscape plans for all proposed developments and may be used to satisfy requirements stated herein. The Conway Tree Preservation Ordinance shall be consulted and used for specific guidance on tree preservation. Site plans shall be designed to preserve existing vegetation, wherever practical.

1. Existing trees may be counted in meeting the requirement for trees along rights-of-ways. To be counted, a tree must have a tree diameter at breast height (DBH) of two inches. The type and condition of such trees are subject to approval by the Planning Director for that purpose and must be depicted on the landscape plan. If existing trees do not satisfy the requirement, a sufficient number must be planted to comply with the requirements of this Article.
2. Random placement of landscape islands and irregular shaped parking lots are not encouraged and shall be only required in locations where such random placement and irregularities will preserve natural vegetation, landmark, or protected trees.
3. The Planning Director shall have the authority to require additional landscape islands of any shape or size that are necessary for the preservation of natural vegetation. Such additional landscaped islands may be exempt from the minimum requirements set forth in Section 9.3.4D.

B COMMERCIAL HDRD HISTORIC DESIGN GUIDELINES

Chapter 3 BASICS OF TRADITIONAL COMMERCIAL BUILDINGS

3.4. The Downtown Environment

Downtown is a highly structured architectural environment where it is important to understand the concepts and traditional application of density, set back, building heights, horizontal continuity of building elements and reserving the sidewalk as the "pedestrian hallway."

Density

The downtown environment is dense, regardless of overall community size or how large the central business district is in proportion. Density lends close proximity for the uses, structures, and lifestyle choices of residents and business persons who frequent their downtown. Density helps businesses succeed because it provides continuous and contiguous points of interest.

As a downtown grows and becomes more dense the blocks of buildings can have a layered effect on the perception of the patron or visitor with more interesting buildings continuing around a corner, and larger buildings being in the blocks further from the perceived center of the area. This progression in density is reflected in scale and/or height.

Setback

Traditionally, downtown buildings are built right to the edge of the sidewalk ("zero-lot-line construction") and to the edges of their property boundaries to which commercial structures share adjoining, or "party," walls. New buildings set back varying distances from the front or side property lot lines can offset the rhythm of the "wall" of businesses along the street. If there are existing gaps caused by a variation on building setback these can be filled with landscaping, outdoor seating, or other visually interesting and functional amenities to continue perceived building edge (see below).

APPROPRIATE:



Conway, 2009

INAPPROPRIATE:

(Note: distance to building edge is filled with landscaping feature.)



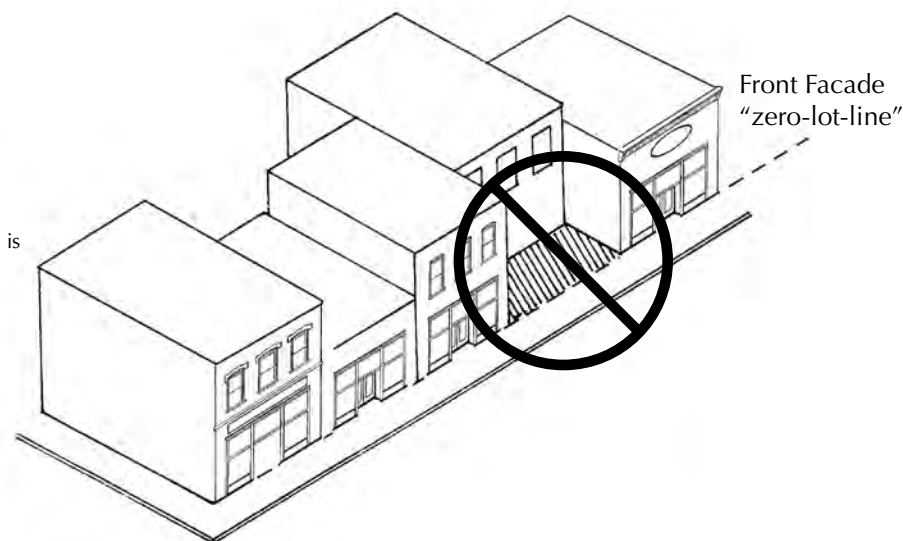
JB-a Photo Archives



Conway, 2008

The traditional commercial architectural environment in the heart of Conway's central business district (Downtown National Register Historic District - shown above) along with the blocks of 3rd and 4th Aves. from Kingston to Elm Streets, allow for high density. Buildings physically share "party" side walls and are built to the edge of the sidewalk. This creates a context that defines the downtown environment.

Fig. 2.6: Example of Improper Setback in Downtown



3.4. Downtown Environment (continued)

Building Height

Building height is most important when dealing with infill construction (if for an unfortunate reason a building is lost or there is a vacant lot) and potential building additions (see Section B, 4.6 "Rooftop Additions"). Generally, building height in a traditional downtown, or in individual districts within an area, reflects structures built about the same time in block groupings. Corner buildings are often considered anchors and may have a bit more mass and therefore height. Therefore, the downtown environment has block faces that are generally harmonious in building height and floor alignment. Heights out of scale with the average height originally intended can become inappropriate.

Controlling building height is not meant to prevent new development of greater density or limit building height in downtown. The concept of "height progression" contributes to the downtown's sense of place and wayfinding for the user. It is important to be able to stand in a central place within a downtown (perhaps from a landmark such as the front of City Hall), look out and see a general progression of building heights from this vantage point. The progression of larger buildings behind the earlier, smaller buildings, or built further down auto corridors will give a sense of order. Keeping in mind progression in scale will allow Conway's built environment to be experienced from the heart of the district outward.

Significant smaller, historic buildings should not be visually blocked or overwhelmed by buildings or additions. National Register buildings should especially weigh the importance of height and scale to historic significance.

Infill opportunities on vacant lots are available in many areas throughout downtown Conway. Current zoning allows new / infill construction in the CBD up to 60-feet (average height, measured from finished grade), however existing historic construction (see Fig 2.8) establishes a precedent to which new building height may be considered. Higher, dense new construction might be appropriate as infill in the "civic" blocks around the courthouse (or along the outlying Fourth Ave auto corridor, Core Commercial zoning district) with views to the river and into downtown as well as expanding the commercial district. The Community Appearance Board will review infill development in the CBD and consider whether the proposed development is consistent with the height limitation for the zoning district, as well as the surrounding area and buildings. Amended 9-20-2021 [ZA2021-09-20(E)]

With historic precedent (surrounding context) and engineering, one-story buildings may be structurally feasible to add second floors (see Section B, Chapter 4.6 "Additions").

Fig. 2.8: Examples of Traditional Building Height in Conway



Existing buildings establish a precedent for building heights in downtown Conway (3rd Ave. above left). A precedent for three-story infill on one downtown block (perhaps four-stories would work visually) can be in scale if constructed near to, or neighboring taller structures (illustrated example between the two buildings at the left), while most blocks of Conway's Commercial HDRD buildings remain one- or two-story (above right, also see Fig 2.6). High parapets on upper facades can bring the height of a one-story building to almost two stories. CAB will review building height of new infill development for consistency with the height limitation of the zoning district, as well as the surrounding area and buildings.

B COMMERCIAL HDRD HISTORIC DESIGN GUIDELINES

Chapter 4 COMMERCIAL REHAB & ARCHITECTURAL DESIGN GUIDELINES

4.5. New Commercial Construction

New, in-fill development or new construction to replace a structure that has been lost should continue the dense, pedestrian oriented, urban environment described in Section B, Chapter 3.4 "The Downtown Environment." **To ensure compatible building design in the commercial areas of the Conway Historic Design Review Districts, all new construction must follow all of Section B, Chapter 4 "Commercial Architectural Guidelines" as well.**

Placement and Orientation

- 4.5.1 Align new construction with the setback and spacing of existing structures in the adjacent downtown area, which generally have "zero-lot-line" front or side setbacks.
- 4.5.2 Locate parking to the rear of the building or utilize available on-street spaces.

Scale

- 4.5.4 Design the new construction to be of similar height, width and proportions of existing structures in the adjacent downtown area (see Figure 2.27 right). The CBD has a "party-wall" precedent.
- 4.5.5 Limit the number of stories of new construction scaled to adjacent structures (see also Section B, Chapter 3.4, Fig. 2.7). Generally, no greater than one story higher than the tallest adjacent building. **The CAB has the right to allow variances or deny additional stories if the building appears out of scale with building forms in the surrounding block.**

Style

- 4.5.6 New buildings should be contemporary. It is appropriate to display the style and construction methods of the period in which it is constructed and not become a "faux" reproduction or create "false history."
- 4.5.7 Customize the elements of new construction (material choices, banding, cornices, door types, reveal of materials) in context with those features of existing structures in the adjacent downtown area.

In this commercial historic district a new structure (left side of courtyard) was appropriately designed with facade, storefront, form, orientation, scale and contextual style to the surrounding buildings. Historic one-part commercial in the area establish a building form. The new structure was built with all contemporary materials.

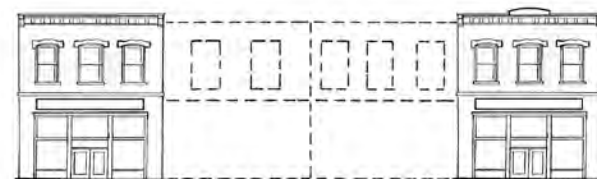


Dalton, Georgia, 2008

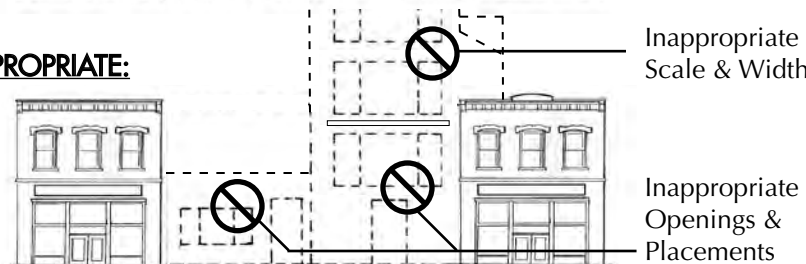
- 4.5.3 Window size, placement, as well as storefront opening and height should be consistent with the rhythm of those in existing building forms in the adjacent downtown area (see Figure 2.28 below).

Fig. 2.27: Examples of New Construction and Rhythm

APPROPRIATE:



INAPPROPRIATE:

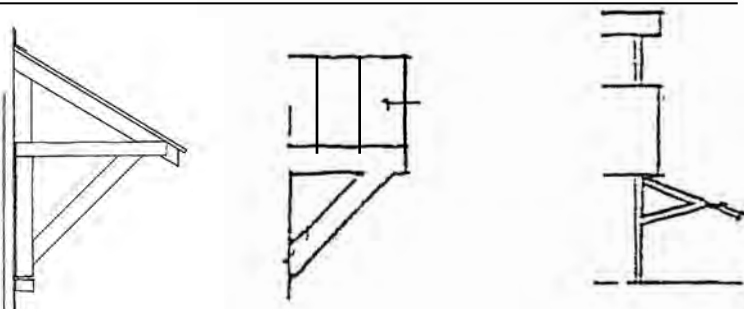


- 4.5.8 Design the roof form to be consistent with those of existing structures in the adjacent downtown area.
- 4.5.9 Design composition and fenestration should be compatible (shapes, sizes, placement of windows and doors, vertical or horizontal emphasis).
(For more information see Section A, 1.7. "Sense of Place & Context" and Section B, Chapter 3 "Basics of Traditional Commercial Buildings.")

10.5. WRD Roofs, Canopies and Roof Lines

Columns, Brackets, & Eaves (incl. Sheds)

Fig. 5.13: Illustrated Traditional Industrial Canopy & Bracket Types



Not all bracketed canopy types are shown above (nor in scale to each other). (Left to right) Side profiles of open-end wood bracket shed canopy & vertical attached wood posts, simple gabled wood bracket porch cover with standing seam roof and large projecting metal braced truck/cart loading dock cover (10 - 16 foot projection).

Appropriate

- 10.5.1 Preserve (retain, restore and maintain) any original industrial railing, eave, bracket or column material.
- 10.5.2 Preserve (maintain or restore, do not remove, cover, or alter) the eaves and simple nature of exposed brackets, rafter tails, flashing and trim.
- 10.5.3 Retain (and repair) rather than replace deteriorated rigid canopy parts.
- 10.5.4 If replacement of parts is necessary due to severe deterioration, replace with features to match (accurately duplicate profiles, massing, scale) in design, construction, projection and materials.
- 10.5.5 If original canopies cannot be determined using photographic or physical evidence, then provide a design that is compatible with the architecture of the building and/or in keeping with similar structures in the adjacent WRD area ("river-side" or "town-side" for character, See Pg. E.2 "Character Areas of Conway's Riverfront"). Generally, replacement trim, decking, brackets (columns if existing) and railings should be proportionate to the original construction. **Non-decorative wood framing is preferred for structures on the "river-side" of the railroad** with use of an industrial palette of materials (such as steel, tin, sheet metal, etc.)
- 10.5.6 Retain later-period porches that match modern changes, additions or upgrades with significant architectural history.
- 10.5.7 If no porch railings exist and are needed for safety, install lightweight wood or wrought iron units painted neutral hue to visually recede.

Fig. 5.14: Features of Porches and Canopies in the Riverfront Area

All images, Conway, 2009-10



(Left) Use of different types of rolled, corrugated, "v" or "w" lapped or standing seam metal panels are appropriate for the WRD industrial character of canopies and porch coverings. Utilitarian, contemporary buildings such as the marina public restrooms also use this material. Avoid use on residential structures. Note simple eave detail.



(Above, left "town-side") Square columned porches and deep eaves are a Cypress Inn feature.



(Above, right "river-side") Simple, bracketed canopies on contemporary construction of the Marina clubhouse.



(Right) Long porches with lightweight steel bracing allowed goods to be moved from warehouses to rail and trucks under shaded and dry conditions, free of columns.

Inappropriate

- 10.5.8 Do not remove, replace, reduce, cover, or alter original porch material.
- 10.5.9 Do not sandblast or use any abrasive method to clean or strip, including high-pressure water. Use only gentle, restoration-sensitive chemical cleaners and strippers or mild detergents and natural bristle brushes on wood or brick.
- 10.5.10 Do not enclose industrial porches or loading docks or install permanent glass enclosures in replacement of or in front of existing porch elements. This would not have been part of the industrial riverfront environment. Accurate historic character creates a tourism destination district. Full screens, retractable blinds or plastic curtains may be used for porch dining options if mounted to inside of columns or set back from porch edge under canopy overhang.

10.5. WRD Roofs & Roof Lines (continued)

- 10.5.11 See Section B, Chapt. 4.2, "Commercial Roofs," and Section D, Chapt. 8.5, "Roofs and Rooflines," Item #s 8.5.1-6 for review criteria and information on treatment and general maintenance also applied to WRD roofing.

Fig. 5.15: Features of Warehouse & Industrial Roofs in the Riverfront Area



Roofs and rooftop monitors are defining features of historic warehouses. Large expanses of pitched surface is appropriate for metal roofs.



Old and new seam metal roofing. Originally overlapped "V" or "W" joints were used, modern versions are rolled & crimped.



Roofing and eave details of the contemporary-compatible construction on the marina clubhouse (implied roof monitor optional).



The "fireproof" masonry and barrel vaulted roofs of the warehouses along 2nd Ave, lower, denser "town-side of railroad" forms.

Industrial Roofing & Covering

Appropriate

- 10.5.13 Maintain the longevity of the original material if it is of a quality such as slate or metal where individual sections can be repaired. Generally this will be standing seam for the warehouse buildings and applied or rolled roofing on the flat or barrel roof structures. Finish residential forms and building types as residential roofs stated in Item # 10.5.11 above.
- 10.5.14 If replacement is necessary and roof covering is proven to not be made any longer, substitute an approved "architectural" compatible roofing material upon the age and style of the building. Composite shingles may only be required on a few of the residential and public buildings in the sub-areas of the WRD closer to the Central Business District.

Inappropriate

- 10.5.15 Do not use roofing material of different color or composition from what has a visual appearance of what would have originally covered the building type.
- 10.5.16 Generally avoid composite "tabbed" residential shingle roofing, inappropriate for the WRD industrial area unless the building is of a residential nature.

Roof Pitch & Curved Roofs

- 10.5.17 Retain intended roof pitch. The most character-defining element of large buildings.
- 10.5.18 See Section B, Chapter 4.2, "Commercial Roofs," Item #s 4.2.21-30 for review criteria and information on understanding roof forms.

Roof Monitors, Vents, Skylights & Chimneys

- 10.5.19 Preserve (maintain or restore, do not remove) original form of roof monitors, vents & skylights. The shape of these historic features define the building forms of the WRD HDRD warehouses. They bring in natural light to reduce electricity and can be retrofitted to vent large interior spaces.
- 10.5.20 Repair or replace missing clerestory windows based on accurate duplication of openings and close visual approximations of the original. Historic photographs are a primary reference source. Temperatures in monitors get extremely high, avoid PVC, vinyl or plastic that can warp.
- 10.5.21 Chimneys may denote office location in the warehouse and are a valuable historic feature. Preserve, retain and repair existing chimneys.