



# CONWAY PATHWAYS and TRAILS PLAN

SEPTEMBER 2022







# ACKNOWLEDGMENTS

This plan is made possible by the commitment to developing a detailed citywide pathways and trails system by the City of Conway.

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Welcome to  
**Conway**  
est. 1732





# Executive Summary

## VISION STATEMENT

The Conway Pathways and Trails Plan offers a vision for a trail system that connects Conway residents to each other and their natural environment. The plan is more than “lines on a map” – it demonstrates how a citywide system of pathways and trails can promote quality of life, economic development and resilience.

The Pathways and Trails Plan is a blueprint, informed by public priorities, that will guide the City and its partners as they design, fund, build and maintain a high-quality system of pathways and trails over time.

The City will use the Plan to guide the detail-oriented work – identifying specific land opportunities, designing the pathways and trails, leveraging funds and partnerships, applying for permits – of getting the trails built.

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## About the Plan

The Plan lays out the process for locating and prioritizing opportunities for pathways and trails. It proposes a citywide, and in some instances county-wide, trail network that will be built out over time.

The Plan identifies pathway and trail corridors within which the City and its partners – through future phases of work – will pursue detailed trail alignments, access, appropriate design and construction. The draft trail corridors in this plan currently identify 54.4 miles of pathway and trail opportunities within Conway and over 71 miles of county-wide connectivity opportunities from the city limits to other Horry County destinations.

The Plan establishes shared goals and principles that are informed by a public process. It acts as a reference to aid internal and external coordination of pathway and trail projects. And it helps the City allocate the resources needed to pursue future phases of analysis and implementation.

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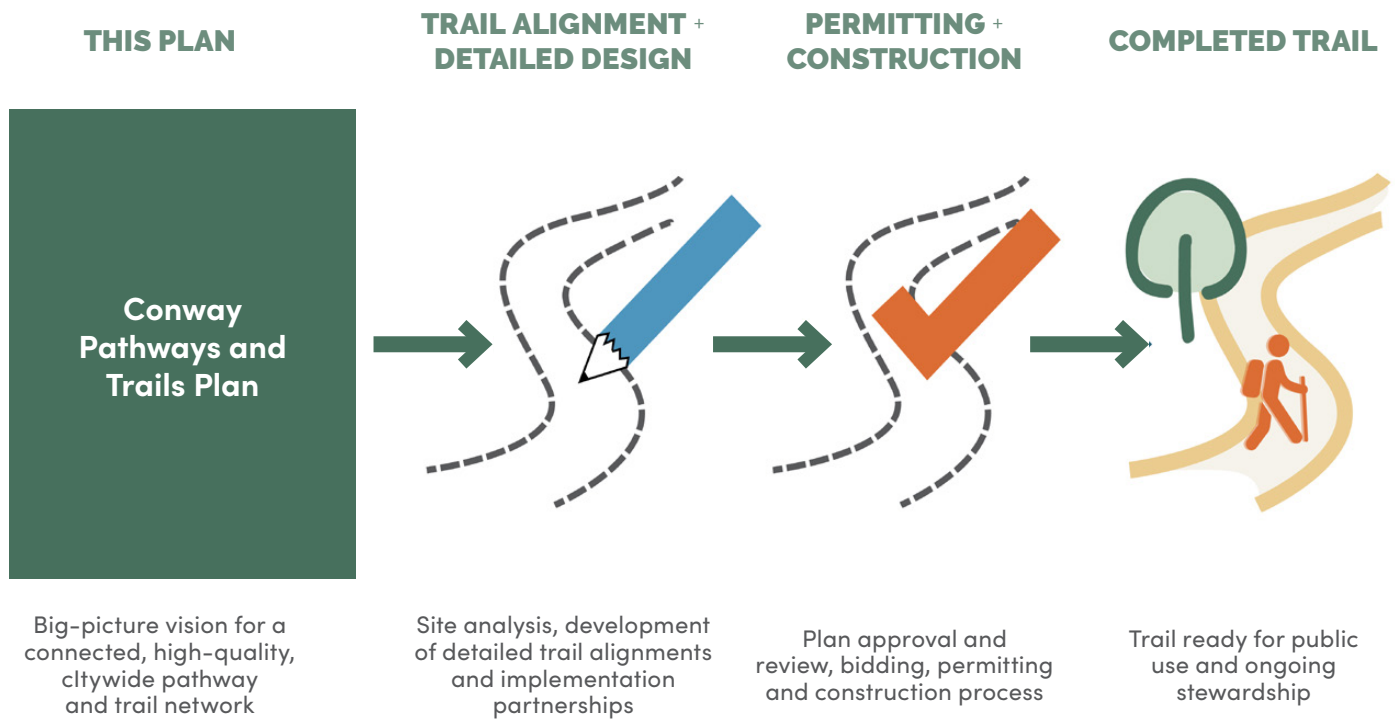
## Public Process and Key Takeaways

The City and planning team developed an engagement strategy that included:

- » **9** virtual focus group and stakeholder discussions
- » **1** [Storymap](#) introducing the Plan process
- » **2** online surveys
- » **2** public workshops – offered in the evenings, seeking community feedback on the process and the plan
- » **2** public "pop-up" events seeking community feedback on the plan

Additionally, the consultant team met with the City throughout the process for insight and guidance on the direction of the plan.





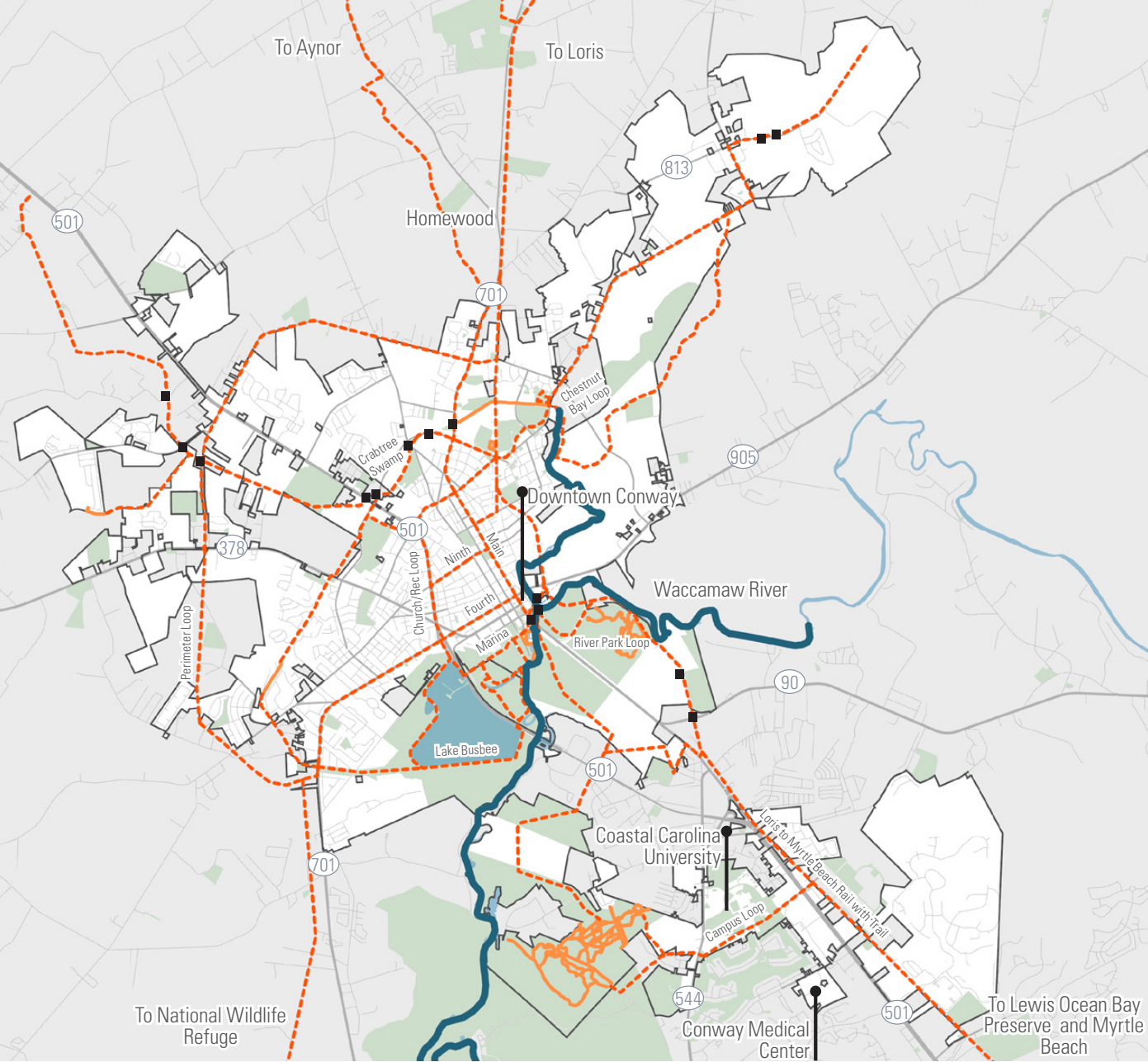
**Figure 1:** How this plan works: steps for Implementation of the Conway Pathways and Trails Plan

Discussions and feedback guided this plan's development and helped to ensure that all elements — from the understanding of system gaps to the plan's recommended strategies and design guidelines — reflect the experiences and vision of the public.

#### KEY TAKEAWAYS WHICH GUIDE THE PLAN'S FOCUS:

- » Conway residents said **protecting and building new connections to Conway's natural resources** should be the greatest focus of the Plan.
- » Top obstacles that prevent community members from using Conway's trails are an **overall lack of trails, lack of connections to desired destinations, and lack of available information on existing trails.**
- » Pathways and trails should be **equitable and far-reaching** into the community.
- » Conway residents would most like to use future pathways and trails for **walking or hiking for exercise, biking for exercise, and being in nature.**
- » Stakeholders and community members expressed an **important need for security, safety, resilience and sustainability, accessibility and comfort** to be embedded in the trail design process.
- » **Quality-of-life benefits** of a trail system that are **most important** to community members are: **recreation and health, improving Conway's competitive edge to attract employers and retailers, and conservation of and access to nature.**
- » Conway residents **prioritize** pathways and trails that provide citywide connectivity with an emphasis on **connection of institutional, historic and natural places.**

# CONWAY PATHWAYS & TRAILS PLAN



## Pathway and Trail Corridors

The Conway Pathways and Trails system will offer a variety of experiences, with new pathways and trails designed to suit their specific environmental, cultural and development contexts. The strategies, design guidelines, and prioritization in this Plan document should be referenced to ensure consistent and sustainable trails and a high-quality system that serves multiple users.

LEGEND

CITY OF CONWAY BOUNDARY

MAJOR ROADS

ROADS

EXISTING TRAIL

WATER BODY

PUBLIC LANDS AND UTILITY CORRIDORS (LIGHT GREEN)

DRAFT TRAIL CORRIDORS

PATHWAYS AND TRAILS

BLUEWAYS/PADDLE TRAILS

PEDESTRIAN/BIKE WATER CROSSINGS

4



COMPARISON OF LEVELS OF SERVICE	Existing Mileage	Proposed Mileage	Existing + Proposed	Mi/1,000 Residents from Existing	Mi/1,000 Residents from Proposed
Trails and Multi-Use Paths	16	54.4	70.4	0.7	2.9
Blueways	4	15	19	0.2	0.8
TOTALS	20	69.4	93.4	0.9	3.7

**Figure 2:** This table compares metrics for Conway's existing trails and Level of Service (miles per 1,000 people) with the new trail mileages proposed in the Conway Pathways and Trails Plan. In addition, the Plan also identifies 71 miles of county-wide trail opportunities not factored into the Level of Service calculations.

## Plan Principles

1. **Enhance quality of life in Conway by connecting communities and destinations across the city.**
2. **Provide a high-quality pathway and trail experience.**
3. **Connect Conway to other communities and destinations in Horry County.**
4. **Protect natural resources and make trail corridors resilient.**
5. **Promote economic development and tourism.**
6. **Provide safe, equitable and inclusive access to pathways and trails.**
7. **Ensure the plan is implemented.**

the City will establish Level of Service (LOS) standards, starting with a **goal of maximizing public access within a 10 minute walk of pathways and trails.**

In coordination with Horry County and SCDOT, neighboring municipalities, and landholders, implementation of additional county-wide trail opportunities would significantly increase LOS along major commuter corridors such as Highways 701 and 501. By including connections to county-wide trail service, Conway's population of 24,000 and neighboring county residents will have expansive access to employment centers, commercial destinations, natural open space, residential land use areas, historic places, and Horry County's waterways.

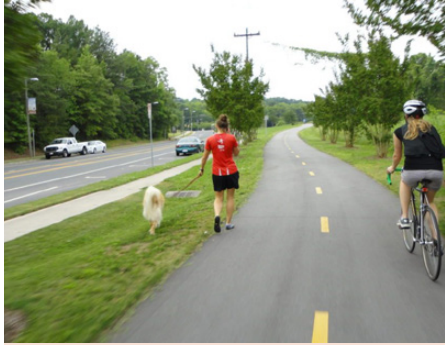
## Trail Typologies, Design and Maintenance

The approach to planning Conway's pathway and trail corridors and typologies is found in the Pathways and Trails Plan and Design and Maintenance Guidelines chapters of this document. The following trail typologies were determined with respect to Conway's community and environmental context. They inform experiential qualities and detailed design of trails, as well as elements of project prioritization in the Implementation Plan.

## Level of Service

Implementation of the Pathways and Trails Plan will more than double the size of Conway's trail system, setting an ambitious target of around **2.9 miles of trails and multi-use paths and 0.8 miles of blueway trail per 1,000 residents.** To track the impact of pathway and trail improvements on measures such as overall system access and connectivity,

# TRAIL TYPOLOGIES



**Multi-Use Trail  
(Urban/Suburban)**



**Historic, Boulevard &  
Main St. Trails**



**Multi-Use Trail  
(Rural)**



**Swamp, Levee &  
Canal Trails**



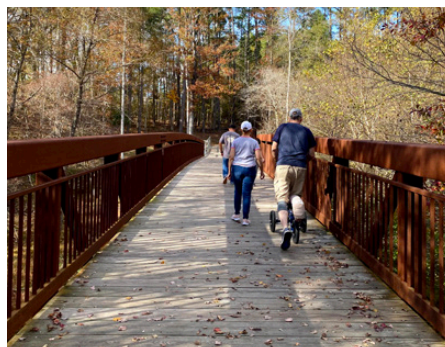
**Wetland or Sensitive  
Lands Trails**



**Loris to Myrtle Beach  
Rail with Trail**



**Blueway/ Paddle  
Trails**



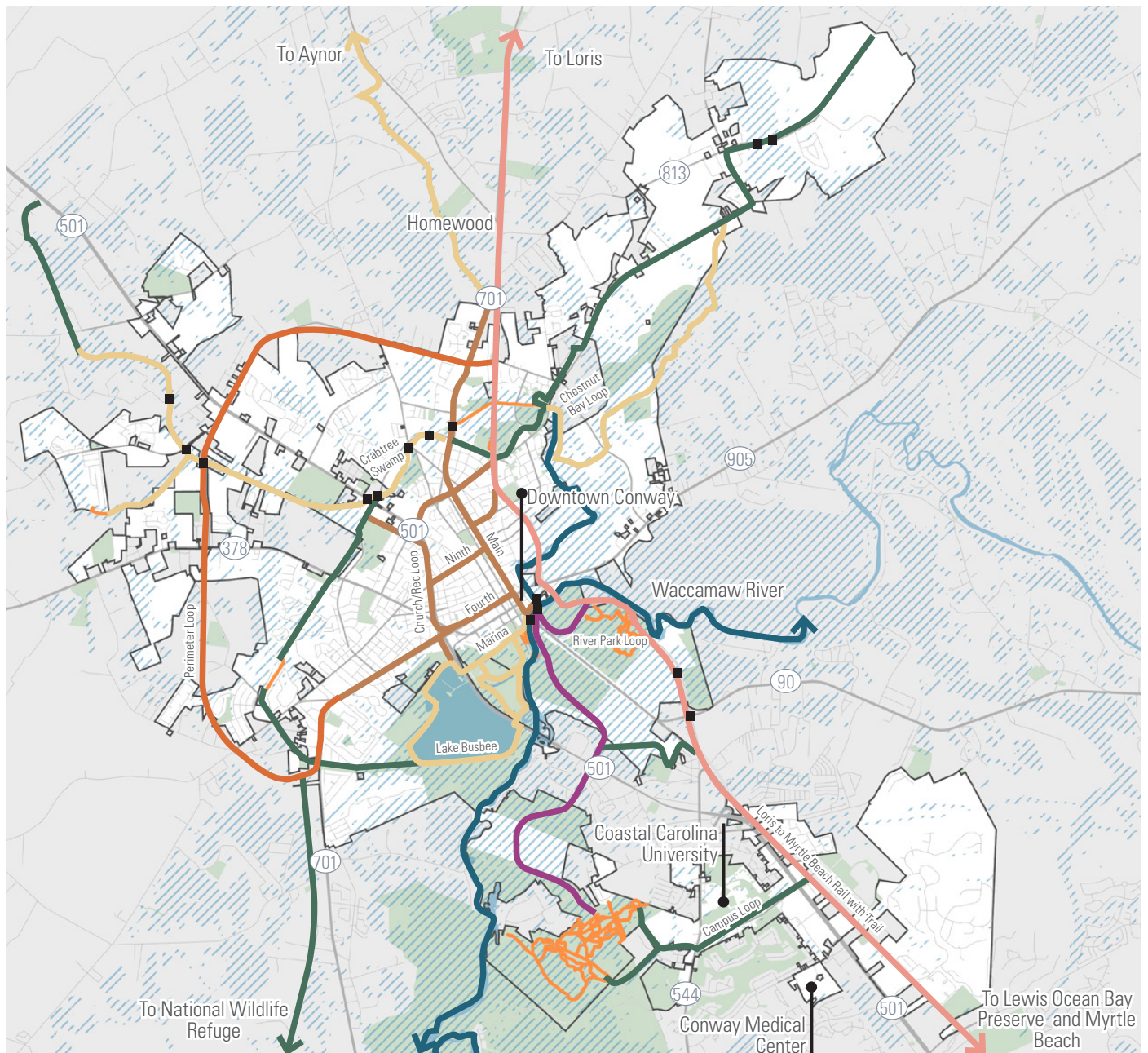
**Pedestrian and  
Bicycle Water  
Crossings**

## Public Input

Over 60% of Survey 2 respondents chose Multi-Use Trail (Urban/Suburban), Typical Multi-Use Trail (Rural), Swamp, Levee & Canal Trails as their highest priority trail types, and over 50% said Loris to Myrtle Beach Rail with Trail, and Blueways/Paddle Trails were second-highest priority trail types.

Trail typology precedent photo credit: Top Row: Alta, DowntownPhoenix.org, NCDOT; Middle Row: East Coast Greenway, Mecklenburg County, Mountain Division Alliance; Bottom Row: Design Workshop





## Trail Typologies

By stitching together a citywide system of pathways and trails that follow canals, the railroad, stream corridors and road corridors – and further imagining spurs and loops through a diversity of natural and cultural landscapes – the plan can satisfy residents' vision for an interconnected system of natural, urban and suburban trails with distinct experiential qualities.

### LEGEND

- |                                    |                                       |
|------------------------------------|---------------------------------------|
| CITY OF CONWAY BOUNDARY            | MULTI-USE TRAIL (URBAN/SUBURBAN)      |
| MAJOR ROADS                        | MULTI-USE TRAIL (RURAL)               |
| ROADS                              | SWAMP, LEVEE & CANAL TRAILS           |
| EXISTING TRAIL                     | HISTORIC, BOULEVARD & MAIN ST. TRAILS |
| WATER BODY                         | LORIS TO MYRTLE BEACH RAIL WITH TRAIL |
| PUBLIC LANDS AND UTILITY CORRIDORS | WETLAND OR SENSITIVE LANDS TRAIL      |
| WETLANDS                           | BLUEWAYS/PADDLE TRAILS                |
| PEDESTRIAN/BIKE WATER CROSSINGS    |                                       |

# IMPLEMENTATION PLAN

## Prioritizing Pathway and Trail Projects

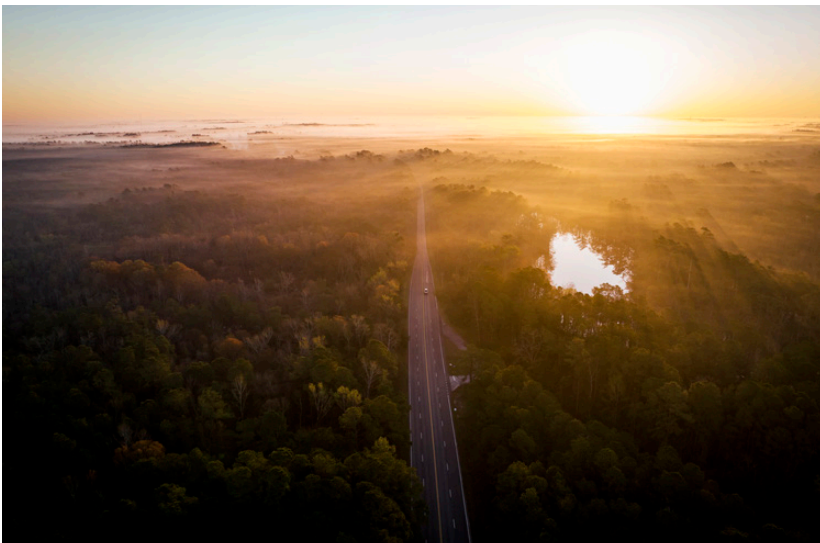
The Pathways and Trails prioritization model offers clear immediate action items and a long-term blueprint for the City, its partners and developers to reference and coordinate efforts. The model also identifies short- and long-term funding and partnership recommendations. Cost estimates and design and maintenance guidelines will help the City and its partners establish clear expectations for initial and ongoing costs and maintenance needs. Project prioritization and phasing recommendations are driven by community priorities and grounded in physical and economic realities.

PROJECT TYPE	ESTIMATED PLANNING AND CONSTRUCTION COST
Tier 1	\$19.3 m - \$72.8 m
Tier 2	\$9.5 m - \$36 m
County-wide	varies
Blueways	\$1.3 m - \$7 m
Existing	-

### Key Takeaways

- » The Principles, Objectives and Strategies of the Plan informed the distribution of pathway and trail projects into tiers.
- » Tier 1 is the highest priority for short-term projects that promote equity in the system while building important connections.
- » The prioritization model (Table 1 on page 11) lists the projects by tier. The higher the project's place in the table, the higher it scored according to the equity-oriented system goals of connectivity and public interest. Each project is shown with its estimated range of planning and construction costs.
- » The plan recommends a staggered budgeting approach whereby higher-tier projects are budgeted at a higher per-mile cost and lower-tier projects at a lower per-mile cost.
- » The result will be an emphasis on funding high-quality pathways, trails and blueways within those corridors that are most important for building a system that serves residents equitably, while ensuring progress is made on the trail projects that are lower-priority but still critical for realizing city and county-wide connectivity.

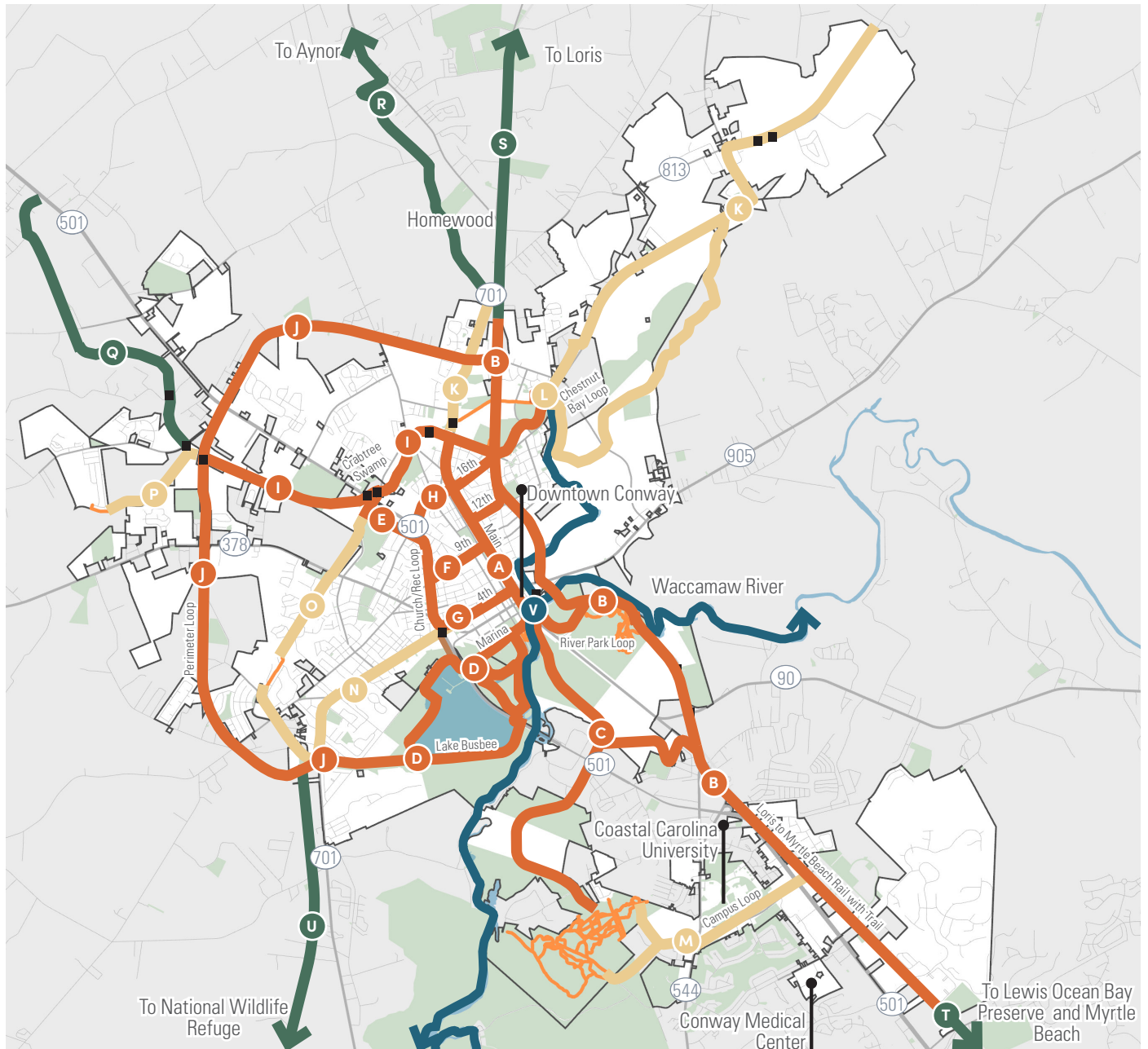




**Figure 3:** Priority pathways and trails identified by the Conway community are closely tied to the connection of institutional, historic and environmental place-anchors which can be leveraged to bolster economic development and tourism. Examples of these anchors include: (clockwise from top left) Conway's Downtown and River Park, the Sports and Fitness Center, educational institutions such as Coastal Carolina University, nearby Horry County destinations such as Myrtle Beach; existing waterside trails such as Lake Busbee, Crabtree Swamp, and the Santee Cooper Ash Ponds, and the Waccamaw National Wildlife Refuge. Credit: Design Workshop



# PATHWAYS AND TRAILS PRIORITIZATION TIERS



## Project Prioritization

The Implementation Plan is an important resource to help Conway develop its financing, staffing and partnership approach for an expanded pathways and trails system. It is also an important tool for engaging the public, showing progress, and demonstrating the positive impact of an expanded system.

### LEGEND

- |                                    |                    |
|------------------------------------|--------------------|
| CITY OF CONWAY BOUNDARY            | TIER 1 TRAILS      |
| MAJOR ROADS                        | TIER 2 TRAILS      |
| ROADS                              | COUNTY-WIDE TRAILS |
| EXISTING TRAIL                     | BLUEWAYS           |
| WATER BODY                         |                    |
| PUBLIC LANDS AND UTILITY CORRIDORS |                    |
| PEDESTRIAN/BIKE WATER CROSSINGS    |                    |

**Table 1:** Range of High-Level Planning and Construction Costs for the Conway Pathways and Trails Plan

PATHWAY & TRAIL CORRIDOR		MILEAGE	RANGE OF PLANNING & CONSTRUCTION COSTS
TIER 1	<b>A Main St. to Crabtree Swamp Trail</b>	1.8 mi	\$954,000 - \$3.6 million
	<b>B Loris to Myrtle Beach Trail</b> (within City limits, with 12th to Main Connector)	8 mi	\$4.2 million - \$16 million
	<b>C River Park Loop West Spur and Cox Ferry Lake Connector</b>	3.6 mi	\$1.9 million - \$7.2 million
	<b>D Lake Busbee and Ash Pond 2 Circuit / Riverwalk Ext.</b>	6.2 mi	\$3.3 million - \$12.4 million
	<b>E Church St / Rec. Loop East Spur</b>	2.6 mi	\$1.4 million - \$5.2 million
	<b>F 9th Ave. / Boulevard Improvements</b> (Church to Main)	.7 mi	\$371,000 - \$1.4 million
	<b>G 4th Ave.</b> (Church to Main)	.7 mi	\$371,000 - \$1.4 million
	<b>H 16th Ave. / Collins Park Connector</b> (Church to Sherwood Park)	1.5 mi	\$795,000 - \$3 million
	<b>I Crabtree Swamp Trail</b>	3.6 mi	\$1.9 million - \$7.2 million
	<b>J Outer Belt -</b> (Perimeter Road Segment) (Cultra Road to Hwy 378 Segment) (Perimeter Road to Lake Segment)	2.8 mi 4 mi .9 mi	\$1.5 million - \$5.6 million \$2.1 million - \$8 million \$477,000 - \$1.8 million
		<b>36.4 mi</b>	<b>\$19.3 million - \$72.8 million</b>
TIER 2	<b>K Homewood Connector</b>	1 mi	\$530,000 - \$2 million
	<b>L Chestnut Bay Loop and Collins Jolly Spur</b>	9.1 mi	\$4.8 million - \$18.2 million
	<b>M Campus Loop Connector</b>	2.4 mi	\$1.3 million - \$4.8 million
	<b>N 4th Ave. to Outer Belt</b>	2.2 mi	\$1.2 million - \$4.4 million
	<b>O Rec Loop West Spur and Outer Belt Connector</b>	2.4 mi	\$1.3 million - \$4.8 million
	<b>P Dunn Short Cut Road to Crabtree Swamp Trail / Oakey Swamp</b>	.9 mi	\$477,000 - \$1.8 million
		<b>18 mi</b>	<b>\$9.5 million - \$36 million</b>
CITYWIDE TOTALS		<b>54.4 miles</b>	<b>\$28.8 million - \$108.8 million</b>
BLUEWAYS COUNTY-WIDE	<b>Q Crabtree Swamp to 501</b>	<b>3.3 mi</b>	<b>\$1.7 million - \$6.6 million</b>
	<b>R Homewood to Aynor</b>	<b>15+ mi</b>	<b>\$530,000 - \$1.2 million per mile</b>
	<b>T S Loris to Myrtle Beach Rail with Trail</b>	<b>18.5+ mi</b>	<b>\$530,000 - \$2 million per mile</b>
	<b>U Wildlife Refuge Trail</b> (Easement Trails)	<b>22.4+ mi</b>	<b>\$340,000 - \$530,000 per mile</b>
	<b>V Waccamaw River Crossing</b> (Bike/Ped Ferry with Landings or Pedestrian Bridge)	-	<b>\$1.3 million - \$3 million (ferry), \$5 million - \$7 million</b>

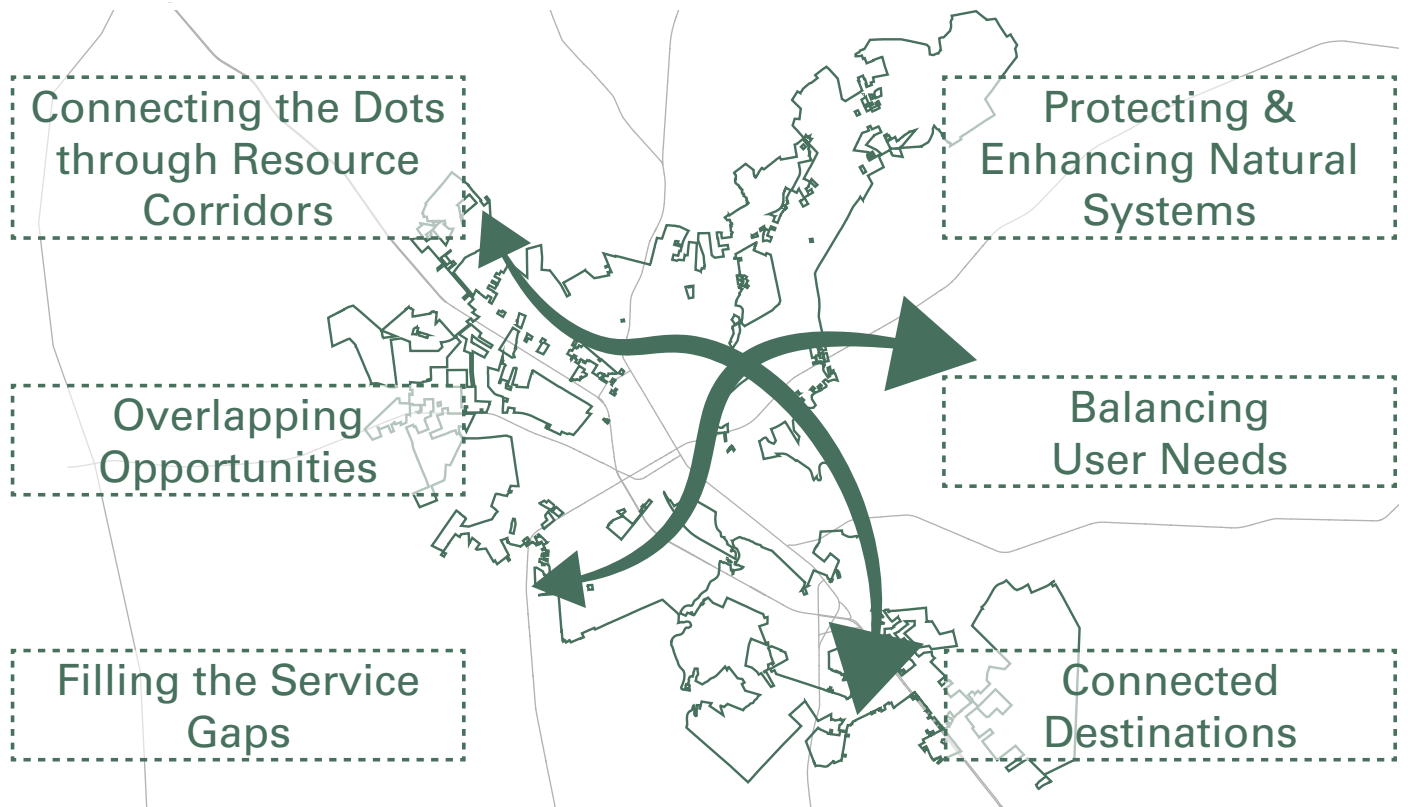






# Project Introduction

- » Project Vision
- » Planning Context
- » Definitions
- » Process and Methods



**Figure 1:** Goals of the Conway pathways and trails planning process

## Project Vision

The Conway Pathways and Trails Plan offers a vision for a trail system that connects Conway residents to each other and their natural environment. The plan is more than “lines on a map” – it demonstrates how a citywide system of pathways and trails can promote quality of life, economic development and resilience. The Pathways and Trails Plan is a blueprint, informed by public priorities, that will guide the City and its partners as they design, fund, build and maintain a high-quality system of pathways and trails over time.

The City will use the Plan to guide the detail-oriented work – identifying specific land opportunities, designing the pathways and trails, leveraging funds and partnerships, applying for permits – of getting the trails built.

The planning process itself lays the groundwork for future implementation, helping the City build critical public and stakeholder support for short- and long-term buildout of a citywide system of pathways and trails.

## Planning Context

Conway currently has 16 miles of trails that are largely isolated within parks, or “trail islands,” and 4 miles of dedicated blueways, or paddle trails. Only about 1,300 residents, or 5% of the City's population, live within a 10-minute walk of those trails' access points.

This planning process was launched at an exciting time, poised to take advantage of public support for more and better connected pathways and trails; to coordinate with recent trail planning efforts; and to unify city,



CONWAY'S PATHWAYS AND TRAILS	Existing Mileage	Miles per 1,000 Residents	City Residents within 10-Minute Walk of Trail Access	% of Population within 10-Minute Walk of Trail Access
	16	0.7	1,300	5%

**Figure 2:** Conway's existing pathways and trails.

county and regional trail enthusiasts behind a common vision and plan of implementation.

Among the previous or ongoing plans and efforts that the Conway Pathways and Trails Plan can build upon:

- » The City of Conway 2016-17 Recreation Needs Assessment and Planning Report showed great public support for new trails and greenways. When asked which recreation facilities they wanted greater access to, the largest proportion of survey respondents (55%) selected more hiking/walking trails/paths, followed by 49% who said they wanted more bike lanes/paths. When asked how they would want the City of Conway to spend \$100, trails and greenways were the top priority of survey respondents. The report recommendations align with those of this planning effort, including:
  - Build connections across the Waccamaw River.
  - Develop a branding and communications plan to promote awareness of pathways and trails.
  - Plan for and fund ongoing maintenance to ensure the long-term quality and usability of the system.
  - Extend partnerships with Horry County to expand multi-modal trail networks throughout and beyond town.
  - Create more places for outdoor recreation and nature-based activities in Conway.
- Increase funding for recreational facilities, including trails, through measures that could include support from non-profit Friends' groups and a park and recreation foundation.
- » The downtown Conway master plan update was underway at the beginning of this planning process and generated excitement about possible new connections to Conway's historic and vibrant downtown.
- » The 2005 City of Conway Greenway Master Plan was consulted by the planning team. It provides recommendations for multi-use trails, bike lanes and sidewalks, largely within existing road corridors, and makes important observations about the potential for trail corridors to preserve natural resources and the opportunity to build new trail connections to and within future residential development.
- » The City of Conway Unified Development Ordinance, updated in September 2021, is a valuable tool in future pathway and trail development. It specifies design standards, incentives and payment-in-lieu programs to encourage the buildout of pathways and trails in pace with Conway's future development.
- » The City of Conway has participated in the FEMA Hazard Mitigation Grant Program since 1999, following Hurricane Floyd. This program provides federal funds for the voluntary buyout of flood-prone properties by the City. Projects such



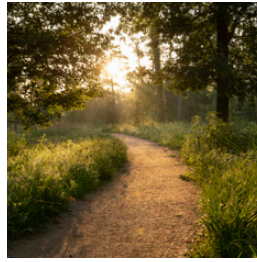
### Multi-use paved paths

separated from road traffic wherever possible and connecting destinations, sidewalks, and road and water crossings



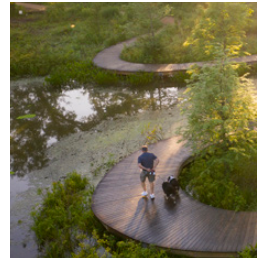
### Shared-use paths through parks and natural areas

paved paths for park and trail system connectivity



### Gravel or other natural surface trails

through natural environments, for walking, hiking, off-road biking and exploration



### Boardwalk trails and other wetland trail structures

for low-impact access to sensitive natural areas



### Blueway trails

along Conway's waterbodies, for paddlers and non-motorized watercrafts

**Figure 3:** Definitions of pathways and trails. Photo credit: Design Workshop

as the Sherwood Forest Masterplan, funded by a resilience project of The Nature Conservancy, demonstrate the potential for such buyout properties to form contiguous pieces of land that can be used for parks and trails in addition to providing flood resilience.

- » The Horry County Trail Summit, held in May 2022, brought together people from across the county to discuss the possibilities for new local and regional connections by way of pathways and trails, including a proposed rail-to-trail from Myrtle Beach to Aynor. This event, organized by the Bicycle Advisory Council of Coastal Carolina University, and others like it present a significant opportunity to engage with people who may act as advocates and stewards of a future pathway and trail system.

## Definitions

Pathways and trails can be used for recreation, for getting from one place to another, or just enjoying Conway's natural and cultural resources. Conway's Pathway and Trails will traverse a variety of urban, suburban, rural and natural contexts and serve many different user types. The primary purpose of the pathway and trail network is to provide connectivity throughout the City through recreational routes for walkers, bicycle users, hikers, and paddlers.

Figure 3 illustrates how pathways and trails are broadly defined for the purposes of this plan. The approach to planning Conway's trail corridors and specific trail typologies is explained in further detail in the Pathways and Trails Plan and Design and Maintenance Guidelines sections of this document.



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## Process and Methods

Conway residents have expressed a desire for more trail experiences in Conway's natural landscapes and for more trail connections to other trails, to downtown, to natural landscapes and to parks. Because of the complex nature of land ownership and geographical constraints, the plan must rely on a variety of "resource corridors" to provide that diversity of trail experiences and to build out citywide connectivity. The Plan recommends different approaches to identifying feasible locations for pathways and trails, as well as appropriate trail types in corridors that meet the needs of Conway's infrastructure and environment.

The planning team studied different types of land that represent the greatest opportunities for stitching together a high-quality and continuous pathway and trail system throughout Conway and other destinations in Horry County. This recipe for potential new pathway and trail alignments includes:

- » Land that is already publicly owned or accessible, or slated to join Conway's future park and public lands system, such as Hawthorne wetland park and bike paths at Chestnut Bay
- » Land that is privately owned but already protected by environmental regulations or a conservation easement
- » Land that is privately owned by a potential organizational partner, such as a utility or homeowners association
- » Land considered a likely site for future development, which could be planned to include open space protection and public trail amenities
- » Areas where future pathway and road improvements are funded and/or formally planned to take place, such as a new road behind Chick-fil-a that will include sidewalks and a round-about, a redesign of 9th Ave from Hwy 501 to 378 to create a boulevard

condition, and widening of Hwy 701 to Loris (the fastest growing area in the region)

- » Maintenance easement areas along City ditches where access is needed to main pathway and trail corridors
- » Safer connections in scenic areas where cyclist activity is already high, such as Highway 813 and existing informal natural surface pathways or roads
- » Limited use of existing and planned future sidewalks in order to create safer connections in urban, historic or high-traffic areas, and where vehicle and pedestrian activity are constrained (i.e. in areas Downtown and near the future expansion of the Riverwalk)
- » Connections based on Community input, including desired destinations and informal yet well-loved routes.

The planning approach for locating the trail corridors builds on this recipe and categorically maps it within a series of overlay opportunities, found in the Conway Pathways and Trail Plan chapter of this document.





# Public Process

- » Engagement Methods
- » Summary of Community, Stakeholder and Focus Group Feedback

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## Engagement Methods

A variety of methods were used to meaningfully engage a broad cross-section of the Conway community during the development of the Pathways and Trails Plan. The needs and values expressed through this process are reflected in the plan's vision, project prioritization, recommendations and implementation. With this plan, Conway residents can continue conversations about the importance of their trail system. Ongoing solicitation of public support and working closely with Conway's variety of pathway and trail user types will help residents feel connected to and invested in the system's future success.

Goals of the public engagement process for this plan were:

- » Create meaningful dialogue with stakeholders and identify long-term system advocates.
- » Provide diverse opportunities for public interaction and engagement.
- » Feature clear, compelling and educational materials about the pathways and trails plan, how it was developed and its future benefits.
- » Provide a transparent, inclusive and educational process that will promote broad public support for the system and its buildout.

The engagement methods used during the creation of the plan provided diverse opportunities for residents and other stakeholders to engage and reduce barriers through an intentional and Conway-specific approach.

Methods included (1) Storymap introducing the Plan process to the public, (9) virtual stakeholder and focus group meetings, (2) public workshops with activities and informational boards that sought feedback in-person, (2) online surveys that asked the public to

participate in or expand upon prompts from the public workshops, a "pop-up" of workshop materials at the May 2022 Trails Summit Event and Conway Recreation Center, and virtual marketing that made project information and feedback methods available through the City of Conway's website, local news channel and social media.

Information sought during these methods of engagement included:

- » Insight and perspectives on the trail and pathway needs of specific user groups and audience types
- » Identification of opportunities to improve, grow, connect and maintain the City's system of pathways, trails and open spaces
- » Identification of potential project and system challenges
- » Community vision for Pathway and Trails system
- » Specific needs and desires for use and location of the City's pathways and trails
- » Potential barriers to current or future use of a pathway and trail system
- » Relative value placed on potential system benefits, such as economic development, connections to destinations, natural resource protection, and recreation
- » Feedback and direction on preliminary plan concepts and recommendations
- » Priorities for City investment and initial system buildout
- » Level of support for public funding of pathways and trails

**Link to the  
Conway  
Pathways  
and Trails  
Plan  
Storymap**





## FOCUS GROUPS AND STAKEHOLDER DISCUSSIONS

Engaged various stakeholders to reveal existing obstacles and potential synergies for future partnerships during phased development.



## ONLINE SURVEYS

An interactive opportunity for residents to identify their current and desired trail uses, trail system priorities, and most important quality of life benefits of trails in Conway.



## POP-UP EVENTS

Afforded transparency for the planning process and informed residents of project milestones and proposals in need of public feedback and input.



## WORKSHOPS

Provided user-specific insight on considerations for trail character, access and amenity needs, and strategies for long-term trail sustainability and equity.



**Figure 4:** Summary of Primary Engagement Strategies and Participation



**Figure 5:** (clockwise from left) Workshop participants discussing trail opportunities in the City, the Conway Pathways and Trails Storymap, and workshop participants pinning desired trail corridor locations with string. Credit: WMBF News, Design Workshop

## Stakeholder and Focus Group Input

Community members and existing trail users who are familiar with gaps in connectivity and opportunities for future partnerships, including stakeholders, interest groups, and representatives of major landholders, participated in discussions organized by the following topics. Summaries of key feedback during these discussions is provided below and in the Appendices of this document.

### ECONOMIC DEVELOPMENT

- » Sees plan as opportunity to bring people to area and enjoy city and county-wide resources

- » Multiuser trail planning is key to encourage high levels of use and to make network feel safe
- » Conway's natural beauty is an untapped resource
- » Use planned expanses of trails and mile markers to encourage physical activity in care plans for seniors

### HORRY COUNTY SCHOOLS

- » Access to the river is more important than strengthening connections across the Waccamaw river
- » Addressing safety concerns (safe for kids to walk to and from school via trails system) is important
- » Opportunity for connectivity to schools is the biggest untapped resource



- » Hope pathways and trails improvements can also facilitate traffic calming
- » Recommends taking precedent from Myrtle Beach – walkable schools within a superblock
- » Strengthen emphasis on keeping up sidewalk and path infrastructure in order to maintain safe routes to school

#### CURRENT RECREATIONAL USERS

- » Importance of strengthening Waccamaw river crossing: important for connections to Coastal Carolina University, Horry Tech and other amenities across the river
- » Opportunities to align natural resource corridors, rail, and utility lines with pathways and trails for recreation

#### Gaps/challenges identified were:

- » Signage, maps, road markings, signalized crossings, safe intersections
- » Considerations for trail hours – lighting and limited access after dark
- » Canal system and major road crossings are a challenge

#### COASTAL CAROLINA UNIVERSITY BICYCLE ADVISORY COUNCIL

- » Long linear trails will provide further connectivity between other trails and destinations
- » Promote improved interactions between those operating motorized vehicles and cyclists.
- » Most important partners for local and regional trail mobility and mobility advocacy include Waccamaw indigenous communities

- » Incorporate planning and collaborate during drafting of Master Plan for CCU campus

#### NATURAL RESOURCES

- » Desire to integrate flood mitigation projects in the plan, and soft surface/permeable trails where possible to mitigate hydrology/flooding concerns
- » Stronger connections to blueways and better access for kayaking with soft launches
- » Dispel belief that public land is scarce and inaccessible
- » People are willing/will be willing to travel to Conway for longer trail experiences (existing short trail experiences are not worth the travel time to reach Conway)
- » Sponsored benches, trees, gardens, lighting, etc. to solicit the support of more stakeholders
- » Establish green space/green corridors for connections between conservation areas
- » Paths that provide benefits including flood mitigation, preservation, connectivity
- » Make trails resilient, floodable and stable. Incorporate resilience measures during trail design and planning.

#### SOUTH CAROLINA DEPARTMENT OF TRANSPORTATION (SCDOT)

- » Mobility and safety is key
- » Adequate lighting and signage, access for seniors
- » Safety walking around town and by bike
- » Navigating the city by car is stressful

- » LPA projects in Myrtle Beach and North Myrtle Beach, fill gaps in East Coast Greenway
- » Carolina Forest, Market Commons as precedents
- » Strong interest in this project type within DOT

#### PRIVATE LANDHOLDERS

Further coordination and conversations will be required in order to plan for trail land acquisition, leasing and obtaining easements for recreational use on privately held lands. Refer to the Real Estate Strategies section of the Plan for methods of trail planning on private lands.

#### PUBLIC PARTNERS

- » Myrtle Beach is a bicycle tourism destination – connect Myrtle Beach to Conway, and strengthen partnerships with the Myrtle Beach Regional Economic Development Corporation in order to serve areas in which people live, work and play
- » Farther reach of trail system could produce benefits for multiple Horry County cities
- » Provide transportation alternatives to traffic congestion
- » Participants expressed the need for a new bridge or pedestrian crossing over the Waccamaw
- » People walk or ride bikes for different reasons, some ride to work
- » To solicit DOT funding, trails need to be highlighted as transportation corridors in addition to recreation corridors
- » The opinions of those who use trails and pathways as their key routes of transportation will be a valuable resource for this effort

- » Desire to connect to Murrells Inlet trails, include paddling as an ecotourism opportunity
- » Recreation division is looking at kayaking corridors via public and private stormwater facilities
- » McLeod Health recognizes that an active trails system is linked with social determinants of health – strengthen partnerships with McLeod Health, discuss trail development opportunities which connect to the public health system, and consider a public campaign which outlines improvement health and well being through the use of trails in Conway

#### SANTEE COOPER

- » Convert overhead utilities to underground as a long-term goal
- » Lighting along roadway trails
- » Natural and soft trails that are accessible dawn to dusk
- » Considerations for how bridge crossing can contribute to safety during hurricane season and provide an emergency corridor
- » Santee Cooper is willing to serve as a resource for the trails planning and design effort
- » Corridors will be reviewed on a case-by-case basis

#### TRAIL SUMMIT, MAY 2022

- » During the Horry County Trail Summit City Staff and participants identified that the rail system is leased to RJ Corman and is used by industrial companies to transport goods and freight that would otherwise be road bound on 18-wheelers.
- » The rail line recently upgraded from a 10MPH line to a 25MPH line



Protecting and building new connections to Conway's natural resources

Building new or better connections to local destinations — such as parks, schools, campuses and Downtown

Building new or better connections between Conway communities — including those separated by the river

Building new or better connections across Horry County — such as other towns, state parks, other trail systems and the beach

## Greatest Focus

**Figure 6:** Ranked response summary to the preferred focus of the future Conway trail system. (Survey 1)

to increase the amount of freight that can be transported. The rail line between Waccamaw River Park and Intracoastal Waterway was highlighted as a critical link for Conway's ongoing destination tourism planning.

- » It is important to plan for consistent right-of-way use for bicycles.

## Community Feedback

Feedback gathered from the public during the stakeholder and focus group discussion, surveys, workshop and pop-up events revealed several common threads which drove the focus of the plan.

## Key Takeaways

- » **Conway residents said protecting and building new connections to Conway's natural resources should be the greatest focus of the Plan.**
- » **Top obstacles that prevent community members from using**

**Conway's trails are an overall lack of trails, lack of connections to desired destinations, and lack of available information on existing trails.**

- » **Pathways and trails should be equitable and far-reaching into the community.**
- » **Conway residents would most like to use future pathways and trails for walking or hiking for exercise, biking for exercise, and being in nature.**
- » **Stakeholders and community members expressed an important need for security, safety, resilience and sustainability, accessibility and comfort to be imbedded in the trail design process.**
- » **Quality-of-life benefits of a trail system that are most important to community members are: recreation and health, improving Conway's competitive edge to attract employers and retailers, and conservation of and access to nature.**
- » **Conway residents prioritize pathways and trails that provide citywide connectivity with an emphasis on connection of institutional, historic and natural places.**



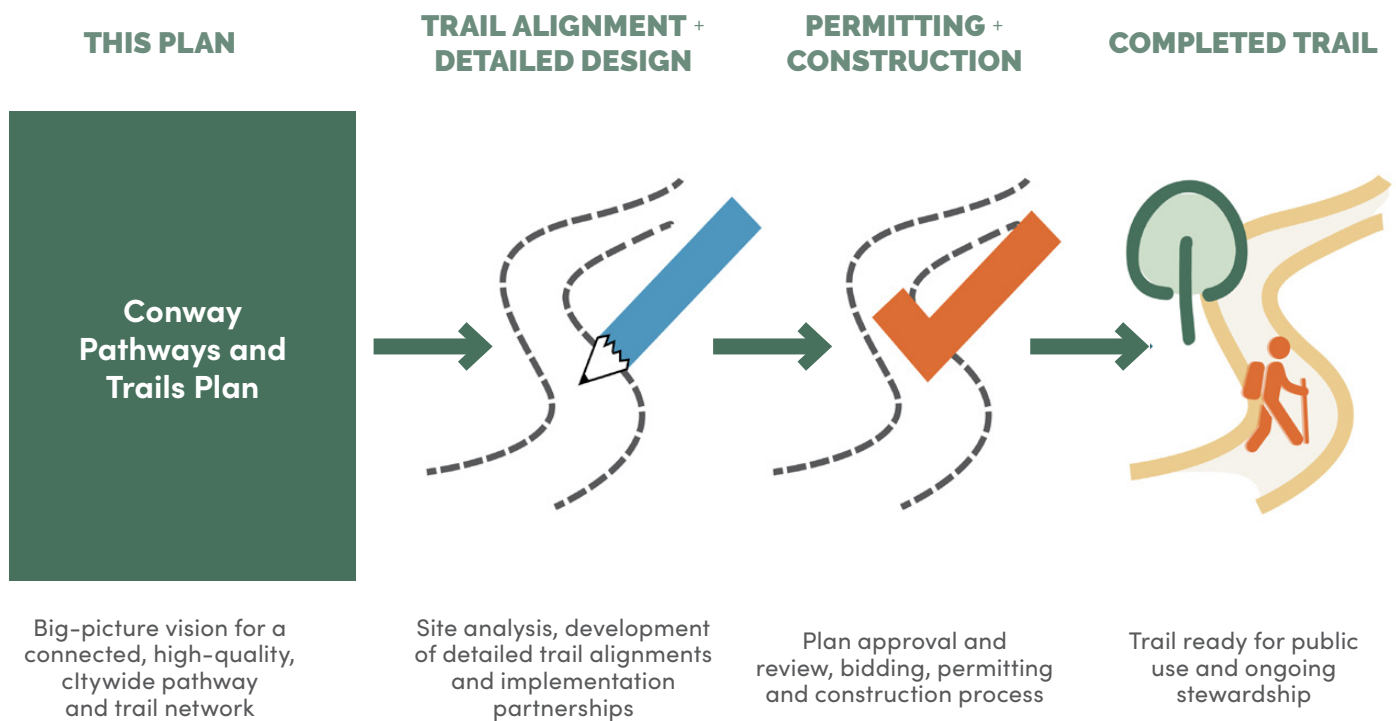




# Pathways and Trails Plan

- » About the Plan
- » Planning Approach
- » Principles, Objectives and Strategies
- » Level of Service
- » Crossing Conway's Waterways





**Figure 7:** How this plan works: steps for Implementation of the Conway Pathways and Trails Plan

## About the Plan

The Conway Pathways and Trails Plan lays out the process for locating and prioritizing opportunities for pathways and trails. It proposes a citywide, and in some instances county-wide, trail network that will be built out over time.

The Plan identifies pathway and trail corridors within which the City and its partners — through future phases of work — will pursue detailed trail alignments, access, appropriate design and construction.

The draft trail corridors in this plan currently identify 54.8 miles of pathway and trail opportunities within Conway and over 71 miles of county-wide connectivity opportunities from the city limits to other Horry County destinations.

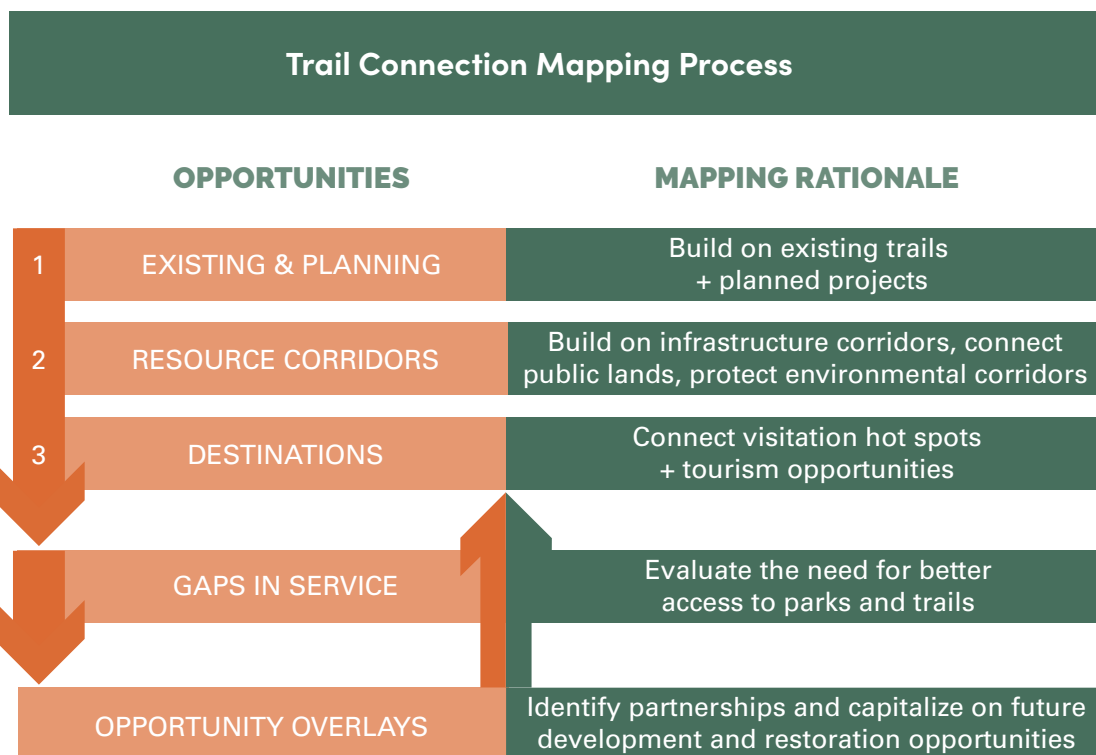
The Plan establishes shared goals and principles that are informed by a public

process. It acts as a reference to aid internal and external coordination of pathway and trail projects. And it helps the City allocate the resources needed to pursue future phases of analysis and implementation.

## Planning Approach

The Conway Pathways and Trails Plan outlines potential pathway and trail locations that are based on overlays of environmental data, cultural destinations and landscapes, ongoing planning efforts, and feedback from the community. The planning team identified different types of land that represent the greatest opportunities for a trail system throughout Conway, listed in the Process and Methods section of the Project Introduction chapter in this document.

Primary opportunities for potential new pathway and trail alignments include



**Figure 8:** Steps in the the iterative planning approach to mapping trail corridor locations.

resource corridors, plus additional and often overlapping private land opportunities. Also incorporated are pending projects for road and sidewalk improvements designated by the City, County, and SCDOT.

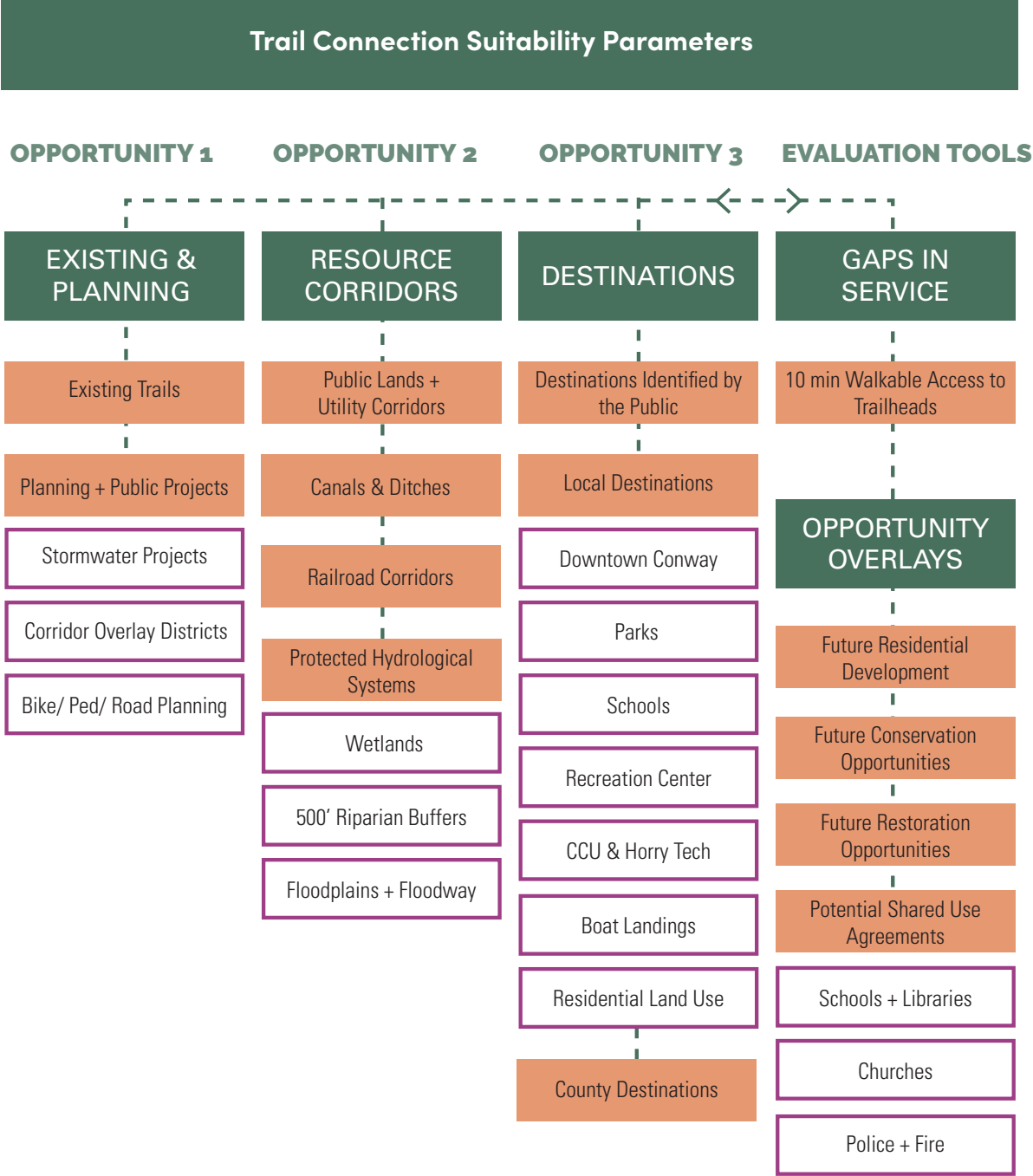
Resource corridors include public lands and natural and cultural landscapes with experience and preservation opportunities that align with the goals and principles of the Plan. These lands often are already protected, either due to public ownership or environmental regulations.

Private land opportunities include privately owned lands that might be considered a good fit for public trail access because of the nature of their ownership, likelihood for future development, or existing easement protections.

Following identification of these diverse resource corridors and private land

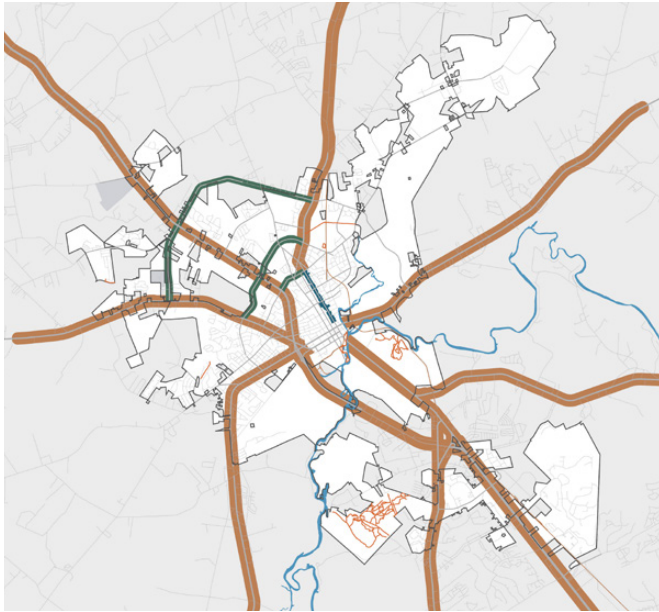
opportunities, the planning process evaluated trail opportunities by building on existing public facilities and previous trail plans with the goal of protecting the Lowcountry character while connecting diverse trail users to natural, historical and cultural destinations in the City.

# PLANNING APPROACH

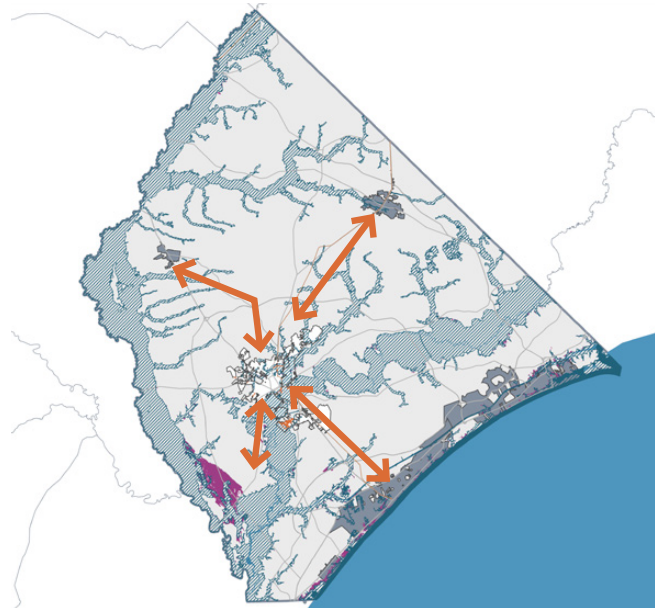


**Figure 9:** Throughout the planning process, feedback from the City and Conway Community helped inform opportunities for potential trail corridors and connections within the City and County. Data layers used for mapping trail corridors are shown above, weighted evenly, and in groups of location suitability opportunities.





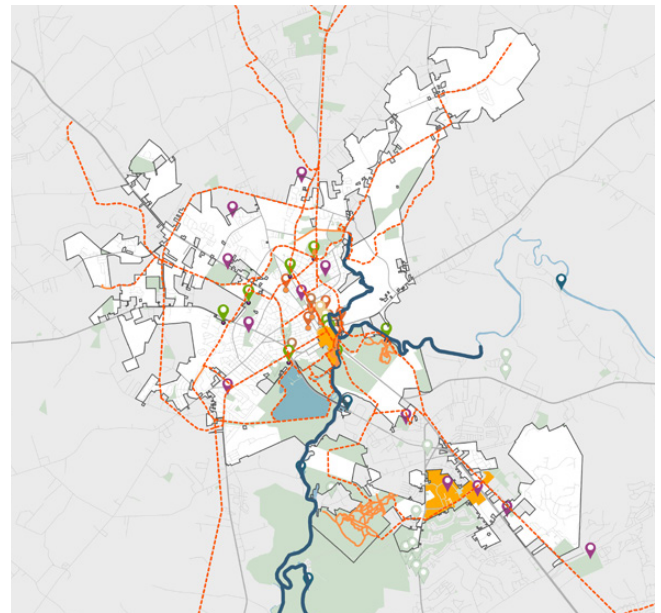
**Existing Trails, Plans & Overlays**



**County-wide Connectivity**



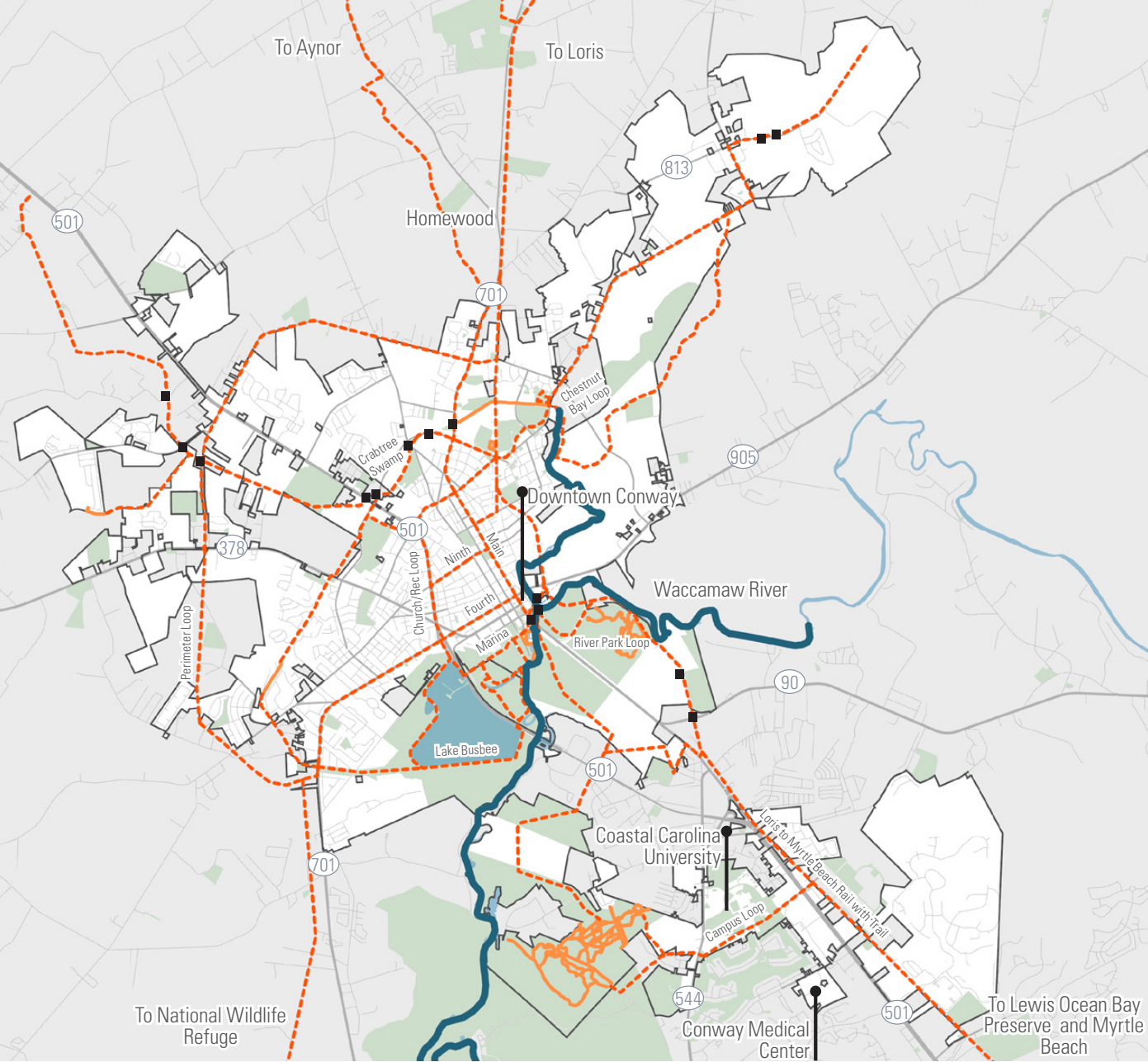
**Natural Resource Corridors**



**Connected Destinations**

**Figure 10:** ESRI ArcGIS software was used to facilitate an iterative planning approach between the City, the design team, and the public during community engagement efforts. These diagrams illustrate trail opportunities and data layers listed in Figure 6.

# CONWAY PATHWAYS & TRAILS PLAN



## Pathway and Trail Corridors

The Conway Pathways and Trails system will offer a variety of experiences, with new pathways and trails designed to suit their specific environmental, cultural and development contexts. The strategies, design guidelines, and prioritization in this Plan document should be referenced to ensure consistent and sustainable trails and a high-quality system that serves multiple users.

**LEGEND**

CITY OF CONWAY BOUNDARY

MAJOR ROADS

ROADS

EXISTING TRAIL

WATER BODY

CRABTREE CANAL

PUBLIC LANDS AND UTILITY CORRIDORS (LIGHT GREEN)

**DRAFT TRAIL CORRIDORS**

PATHWAYS AND TRAILS

BLUEWAYS/PADDLE TRAILS

PEDESTRIAN/BIKE WATER CROSSINGS



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## Principles, Objectives and Strategies

Principles, objectives and strategies will help to guide implementation of the Plan as the system is designed and built out over time by Conway and its public and private partners.

The plan's primary principles are derived from the public planning process and reflect the pathway and trail priorities of City residents and public officials. The following are critical to the success of the Pathways and Trails Plan:

### Enhance quality of life in Conway by connecting communities and destinations across the city.

1. Build important new physical connections, including:
  - » Across the Waccamaw River
  - » Between local parks, public lands, and natural open space
  - » Between Coastal Carolina University and downtown
  - » Between neighborhoods
  - » To important historical and cultural destinations
  - » Through partnerships, to destinations and trail systems beyond the city limits
2. Promote local health and wellness through connections to fitness and recreation destinations, and provide trail system loops to promote daily use by a variety of user types.
3. Support economic development, attract employers and retailers, and enhance property values.
4. Provide equitable access to recreation and nature, as well as ecoliteracy through outdoor learning opportunities along the pathways and trails system.
5. Work with City and municipal partners to create and/or improve active

transportation connections—such as bike lanes, separated bike lanes, and sidewalks—between local communities and trail corridors.

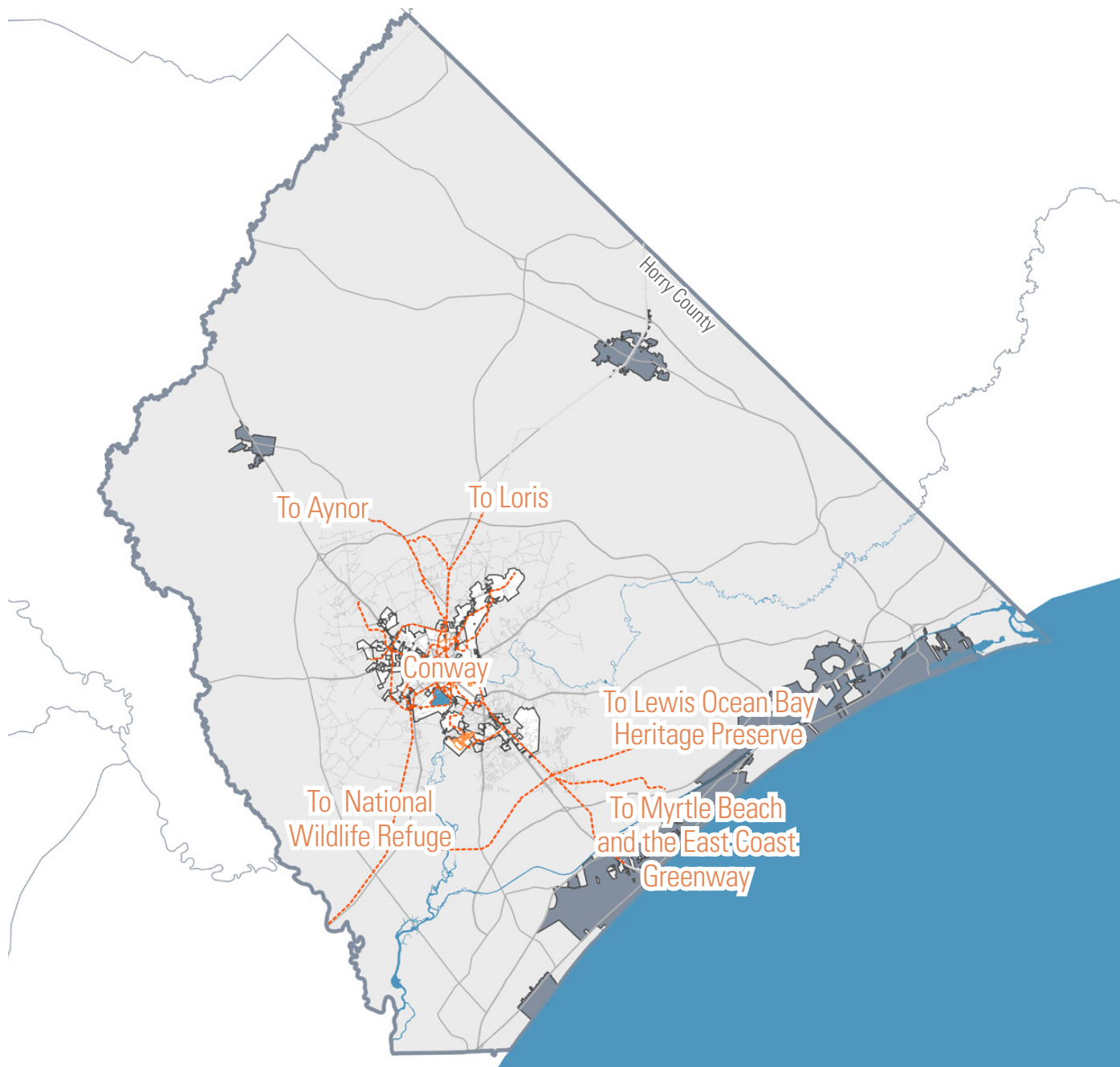
6. Work with local communities and public officials to prioritize local trail connections based on resident needs and preferences.
7. Continue to encourage the development of privately owned and maintained feeder trails—such as those maintained by Homeowners' Associations—in lieu of public trail easements in areas with significant residential population but limited potential to serve the larger community.
8. Enhance trail connections to local and regional destinations.
9. Avoid park and recreation "islands." Ensure that individual public parks and pathway and trail systems are connected within a larger system.

### Provide a high-quality pathway and trail experience.

#### *Design pathways, trails and intersections for comfort and accessibility.*

10. Improve existing trails for comfort and accessibility where possible.
11. Provide opportunities for shade, sitting and respite along ADA compliant trails.
12. Create grade separated crossings at roads and railroads where possible.
13. Where pathway and trail connections must be made within road corridors, enhance trail user comfort levels by providing shade, reducing vehicle speeds, and/or placing trails away from the edge of roadways.
14. Tie into Conway's Tree City program and prioritize tree planting along current and future trail corridors to mitigate heat.





## County-wide Connectivity

Though the Plan prioritizes potential pathway and trail projects within the City's jurisdiction, the Conway Pathways and Trails system also identifies opportunities for connectivity at a county-wide level. Major opportunities for these connections include, and will require coordination with SCDOT, Utility entities, and the Towns of Aynor and Loris, and City of Myrtle Beach.

15. Create looped trail systems of varying mileage to accommodate different types of users.

***Design unique and context-specific trail experiences.***

16. Develop trail and amenity design standards that reflect and celebrate Conway's different ecological and built contexts.
17. In natural resource corridors, site trails to take advantage of views and natural assets while protecting sensitive environments.
18. Where public trailheads are located in or near residential neighborhoods, utilize design standards, signage and enforcement to minimize impacts to neighborhoods.

***Design pathways and trails to meet and balance the needs of multiple users.***

19. Provide a diversity of trail types and linkages to promote walking, paddling, biking and hiking.
20. Encourage the use of trail loops and design standards such as site distances and grade reversals to manage user conflicts on trails.
21. Be clear in marketing materials about the different age groups, modes of travel, and ability levels appropriate for each trail loop.
22. Provide staging and parking areas that are appropriate for all users.

## **Connect Conway to other communities and destinations in Horry County.**

***Enhance citywide trail connections.***

23. Identify opportunities for external public and private funding partnerships.
24. Create new multi-use connections between Conway, public lands and

existing regional trail corridors such as the East Coast Greenway.

25. Work with utilities to create and plan for multi-use trails along easements and utility lines where appropriate.

## **Protect natural resources and make trail corridors resilient.**

***Align pathway and trail planning with the protection of natural heritage and natural resources.***

26. Prioritize pathway and trail corridor acquisition within areas of natural biodiversity and habitat value, for multiple recreational and environmental benefits.
27. Prioritize pathway and trail alignments on protected open space within or immediately adjacent to river corridors and wetlands.
28. Prioritize large contiguous pathway and trail corridors rather than smaller or piecemeal connections.
29. Design trail alignments to protect sensitive environments and cause minimal site disturbance.
30. Establish context-specific trail maintenance regimes that protect and enhance the surrounding natural environment and native plant communities, including through the protection and enhancement of impaired streams and their tributaries.
31. Promote tree planting and preservation within pathway and trail corridors to reduce heat island effect, manage stormwater runoff, and improve water quality, air quality and wildlife habitat.

***Design immersive pathway and trail experiences that connect users to their natural environment.***

32. Design trail alignments that protect sensitive environments while exposing users to the unique natural and scenic qualities of the City's forests,

woodlands, wetlands, floodplains, streams, and rivers.

33. Provide educational signage and programming that highlight the diverse ecological communities of the Lowcountry.
34. Make use of existing corridors – including railroads, linear wetlands, waterbodies and utility and drainage easements – while also identifying new opportunities and mechanisms for corridors to be built out over time.

## **Promote economic development and tourism.**

### *Connect pathways and trails to local and regional destinations.*

35. Align pathways and trails so that residents and visitors can utilize the system to reach cultural and tourism destinations.
36. Work with Conway Economic Development, Downtown Alive, Parks, Recreation and Tourism, and local businesses to promote the pathway and trail system and to improve its ability to connect users to points of interest.

### *Design pathways and trails that are destinations unto themselves.*

37. Offer a diversity of pathway and trail experiences that spotlight the rural, natural, cultural and historical resources of Conway.

## **Provide safe, equitable and inclusive access to pathways and trails.**

38. Identify and remove potential barriers to pathway and trail use.
39. Provide universal access where possible through the careful design of trailhead locations, wayfinding, ADA

compliance, lighting, marketing and programming.

40. Consider community context—such as population density and car ownership rates—when determining the accessibility and priority of potential trailhead locations.
41. Maintain dialogue with community stakeholders to ensure potential barriers are identified and addressed in the future.
42. Identify vulnerable or historically marginalized communities using a methodology developed with input from local stakeholders, to include demographic data that identifies car ownership rates, median household income, people of color, seniors, children, and people with disabilities.
43. Establish Level of Service standards that ensure high-quality trail access is provided in these communities.
44. Establish prioritization criteria that ensure short-term implementation projects are built in these communities.
45. Work with community organizations and the public to ensure trail amenities and programming serve the needs of these communities.

## **Ensure the plan is implemented.**

46. Coordinate trail planning with local, regional, state and federal transportation and recreation plans.
47. Develop and maintain an up-to-date regional GIS database of existing and proposed trails, for use by partner agencies and trails advocacy groups.
48. Review the City's Unified Development Ordinance (UDO) to understand and implement additional regulations or requirements.

### *Develop partnerships.*

49. Collaborate with local, regional, state and federal coalitions, agencies and organizations to facilitate



and coordinate pathway and trail development across the city.

50. Maintain an updated map of existing and proposed citywide pathways and trails and provide clear guidelines, best practices and design standards in order to streamline collaboration and the approval and construction of specific projects.
51. Work with trail advocacy organizations, volunteers, and friends' groups to coordinate long-term trail development and management.

#### ***Work with landowners.***

52. Strengthen incentive programs to encourage the donation of land or easements by private landowners to preserve rural and natural areas. Create educational materials to promote the benefits of granting easements and address potential concerns, such as privacy and liability.
53. Work with local, regional, state and federal public landowners to improve and expand trail access on public lands.
54. Work with private tax-exempt landowners—including HOAs, utilities, authorities, and conservation-oriented nonprofits—to explore opportunities for public trail access and management partnerships on private land.
55. Establish shared-use agreement templates and educational materials to share with private institutional landowners in pursuit of public access to existing private pathways and trails.

#### ***Pursue diverse strategies for fundraising and financing.***

56. Explore establishment of a Pathways and Trails foundation as a separate but affiliated, private, non-profit entity to pursue grants, donations and easements.

#### ***Develop marketing materials that promote awareness and facilitate use.***

57. Develop branding guidelines to create consistent and recognizable materials

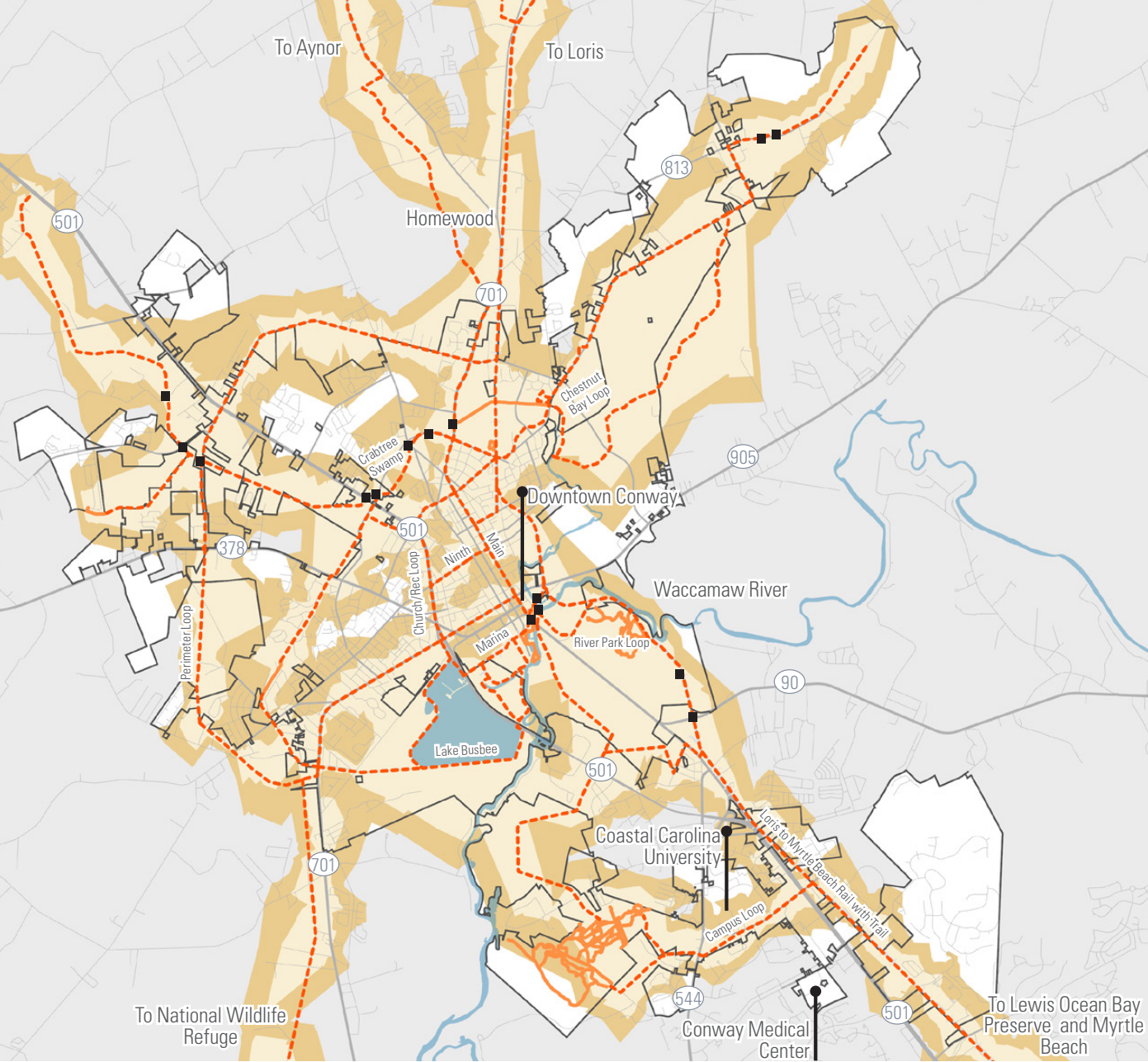
for signage, wayfinding, and printed and online pathway and trail maps.

58. Develop and regularly update a citywide printed map of pathways, trails, blueways and open space.
59. Develop and regularly update a City website with interactive pathway and trail maps and up-to-date information about access, trail information and events. Include blueway trails and up-to-date information about water levels and access.
60. Work with trail advocacy partners to provide updated pathway and trail information through a mobile mapping application.
61. Maintain an up-to-date website with information about in-progress and in-the-pipeline pathway and trail projects, prioritization criteria, and funding sources and partnerships.

#### ***Explore opportunities for partnership and stewardship.***

62. Maintain open and transparent dialogue with the public and community organizations about pathway and trail projects and community needs.
63. Establish a community process for each pathway and trail construction project. This will help ensure community needs and wants are addressed and will promote local buy-in and long-term stewardship.
64. Create online, app-based and phone systems to allow trail users to report maintenance issues or ideas for improvements.
65. Actively pursue agreements with nonprofit organizations for long-term stewardship of new and existing pathways and trails. Develop clear trail management standards that can be used by volunteers. Consider implementation of an Adopt-a-Trail program, whereby organizations could support pathways and trails through volunteer management or funded sponsorships.

# LEVEL OF SERVICE



## Access to Conway's Pathways and Trails

To track the impact of pathway and trail improvements on measures such as overall system access and connectivity, the City will establish Level of Service (LOS) standards, starting with a goal of **maximizing public access within a 10 minute walk of pathways and trails.**

### LEGEND

- CITY OF CONWAY BOUNDARY
- MAJOR ROADS
- ROADS
- EXISTING TRAIL
- WATER BODY

### DRAFT TRAIL CORRIDORS

- PATHWAYS AND TRAILS
- PEDESTRIAN/BIKE WATER CROSSINGS

### WALKSHEDS

- 5 MINUTE WALK TO TRAILS
- 10 MINUTE WALK TO TRAILS

COMPARISON OF LEVELS OF SERVICE	Existing Mileage	Proposed Mileage	Existing + Proposed	Mi/1,000 Residents from Existing	Mi/1,000 Residents from Proposed
Trails and Multi-Use Paths	16	54.4	70.4	0.7	2.9
Blueways	4	15	19	0.2	0.8
TOTALS	20	69.4	93.4	0.9	3.7

**Figure 11:** This table compares metrics for Conway's existing trails and Level of Service (miles per 1,000 people) with the new trail mileages proposed in the Conway Pathways and Trails Plan. In addition, the Plan also identifies an 71 miles of county-wide trail opportunities not factored into the table above.

## Level of Service

Implementation of the Pathways and Trails Plan will more than double the size of Conway's trail system, setting an ambitious target of around **2.9 miles of trails and mutli-use paths and 0.8 miles of blueway trail per 1,000 residents.**

Methods used to plan pathway and trail corridors with equitable access include:

- » Implementing feedback from the community regarding service routes that align with affordable housing and residential development, commutes to school and work, and access to everyday necessities that are essential to Conway's quality of life.
- » Identifying and removing potential barriers to pathway and trail use.
- » Analyzing community context and overlaying demographic data that identifies car ownership rates, median household income, people of color, seniors, children, and people with disabilities, population density and car ownership rates.

The Plan will result in a broad expansion of the City's pathway and trail system, providing an extensive trail network that reaches throughout the City. In order to reach that goal, the City will need to develop partnerships and identify future opportunities to build out secondary

trails and spurs over time. The City could implement phased LOS goals, such as a 5 minute walkshed in the heart of the City, to help prioritize short-term investments and gauge their impact. For other phased LOS goals, the City might consider using the LOS metric of facilities per capita. For example, in Prince George's County, Maryland, the Parks and Recreation department established an LOS goal of 0.4 miles of hard surface trail and 0.1 miles of natural surface trail per 1,000 residents by the year 2040.

The pathway and trail network should be evaluated alongside both the City's Transportation Plan and the County-wide Transportation Network. This plan identifies an additional 71 miles of county-wide trail opportunities which, in coordination with Horry County, SCDOT, neighboring municipalities and landholders, would significantly increase LOS along major transportation and commuter corridors such as Highways 701 and 501.

By including connections to county-wide trail service, Conway's population of 24,000 and neighboring county residents will have expansive access to employment centers, commercial destinations, natural open space, residential land use areas, historic places, and Horry County's waterways.



# WATER CROSSINGS



## Waccamaw River Crossing

Public input ranked a Waccamaw River Crossing as somewhat-to-very important for the future of Conway's Pathways and Trails. Two options along the Riverwalk were studied and presented as part of the Plan: one in the location of a historic bridge, and one that connects to a utility easement along the Wildlife Refuge corridor. The City should consult SCDOT, the US Coast Guard and Santee Cooper to facilitate an in-depth feasibility study for both options.

### LEGEND

- CITY OF CONWAY BOUNDARY
- MAJOR ROADS
- ROADS
- EXISTING TRAIL
- WATER BODY
- PUBLIC LANDS AND UTILITY CORRIDORS (LIGHT GREEN)

### DRAFT TRAIL CORRIDORS

- PATHWAYS AND TRAILS
- POTENTIAL WACCAMAW CROSSING LOCATIONS
- PEDESTRIAN/BIKE WATER CROSSINGS



**Figure 12:** (clockwise from left) Liberty Suspension Bridge, Swamp Rabbit Trail, Greenville, SC; Pedestrian Swing Bridge Sheboygan, WI and a Bike Ferry on Island Line Trail, South Hero, VT. Credit: Swamp Rabbit Trail, Rosales Partners, Local Motion

## Crossing Conway's Waterways

Conway's extensive network of hydrological systems and blackwater recreation opportunities makes waterbody crossings at various scales essential to connecting the city's pathways and trails.

During the public feedback process, the majority of survey respondents rated a pedestrian and bicycle Waccamaw River crossing as a medium-to-high priority project in Conway's future Pathways and Trails system. The same survey revealed respondents' top two preferred options for that potential type of crossing, starting with their most preferred:

1. Pedestrian and bicycle suspension bridge (Figure 12, left)

2. Pedestrian and bicycle swing bridge (Figure 12, top right)

The third most preferred river crossing option from that survey, and potentially the lowest cost option, is a bike and pedestrian ferry or water taxi (Figure 10, bottom right) that could offer a fun, Conway-centric experience. The City could consider this method in the short term in order to promote tourism and work with the community on a feasibility plan for a pedestrian bridge.

To further establish Conway as a blueway destination, the City should collaborate with American Rivers and riverkeepers to study locations for blueway facilities along the Waccamaw Blue Trail, including put-ins and take-outs, rest stops, and rentable primitive campsites or kayak platforms.







# **Design and Maintenance Guidelines**

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## Introduction

The Conway Pathways and Trails system will offer a variety of experiences, with new pathways and trails designed to suit their specific environmental, cultural and development contexts. Coordination will be needed with the South Carolina Department of Transportation and neighboring jurisdictions to ensure consistent standards and high-quality connections across systems. In pathway and trail corridors that are good candidates for state, federal or private grant opportunities, the City should adapt standards as needed to suit funding requirements – for example, by ensuring roadway and crossings meet AASHTO and SCDOT standards.

The full pathway and trail system will include a variety of primary, secondary and tertiary trails. The design guidelines address the primary trails as shown in the Plan – those cross-city trail corridors to which other systems and future trails will connect. As the prioritized projects are pursued, the following should guide more detailed alignment and trail design:

- » Create trails that have a diversity of scenic experiences and connect to other points of interest.
- » Foster transparency and support from local landowners during the planning process.
- » Minimize impacts in environmentally and culturally sensitive areas.
- » Connect to existing trail networks and communities.
- » Provide loop and stacked loop trails (loop trails connected to additional loop trails) and avoid dead-end trails when possible.
- » Manage water sustainably by incorporating drainage devices such as dips, grade reversals, drainage lenses, and culverts.
- » Prioritize alignments that avoid expensive infrastructure.

- » Promote best practices in avoiding user conflict, particularly with motor vehicles and roads.
- » Provide a diversity of experiences.
- » Promote trail etiquette.
- » Provide amenities, safety features and wayfinding to enhance the trail experience.
- » Strive to allow for multiple users when possible.
- » Do not allow motorized vehicle access except for maintenance and emergencies.
- » Stabilize soil during and after construction.
- » Use plants native to the site's Level 3 ecoregion.
- » Design floodplain trails to conform with city and state requirements.

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## Sustainable Trails

The Conway Pathways and Trails Plan presents a wonderful opportunity to embed modern, sustainable best practices to create a world-class, citywide system. A comprehensive sustainability strategy strives to integrate environmental, economic and social best practices simultaneously.

The City should consider utilizing a third-party sustainable rating system such as the Sustainable Sites Initiative (SITES) as minimal standards for the planning, design, construction and stewardship of pathways and trails.

### SUSTAINABLE PATHWAY AND TRAIL GOALS

#### 1. Environmental

- » Conserve, preserve and restore natural systems and their integrity to provide ecosystem services for the community and future generations.
- » Embed resiliency in the design of the pathways and trails to

withstand and rebound from future environmental changes and disturbances.

## 2. Economic

- » Utilize durable and regionally-available materials.
- » Form mutually beneficial partnerships to execute the plan, including public-private partnerships.

## 3. Social

- » Create an equitable plan that addresses all communities and their specific needs.
- » Conserve Conway's unique cultural landscape and historic resources.
- » Prioritize the health, safety and well-being of all communities by providing comfortable, safe and accessible pathways and trails for people of all ages and abilities.

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## Trails and Natural Resource Protection

Conway has abundant natural and cultural resources that are character-defining, promote a unique sense of place, and are why people choose to live, visit and do business in the city.

The pathways and trails plan is an extraordinary opportunity to protect and enhance natural and cultural resources while Conway continues to grow. By layering recreational and restorative uses along pathways and trails throughout the city and across Horry City, there is also a responsibility to conserve and restore these corridors in order to protect and not diminish their integrity. As such, this plan offers several recommendations to protect or enhance these critical resources.

### OVERALL NATURAL RESOURCE RECOMMENDATIONS

- » Utilize existing natural resource corridors as one of the potential

resource corridors for pathways and trail corridors, such as floodplains, riparian corridors, and ridges.

- » Pathways and trails serve as green infrastructure corridors that connect beyond the city. These corridors should be as wide as possible to protect environmental integrity and the scenic quality of the corridor, at least 300' wide where possible (500' is ideal) or 100' on either side of the high water level line (whichever is greater), while minimizing disturbance.
- » For areas whose watershed drains 100 acres or more, the guidance for the cumulative corridor width is:
  - The 100-year floodplain, and
  - Adjacent steep slopes (>25%) starting within 50 feet of the edge of the 100-year floodplain and extending no greater than 100 horizontal feet beyond the edge of the 100-year floodplain, and
  - A 50-foot Management Buffer measured from the edge of the 100-year floodplain, or a minimum stream buffer measured as the area located within 100 feet of both sides of the stream or river, measured as a line extending perpendicularly from the stream bank of the active channel of the stream or river
- » Align trails away from species of concern for the state of South Carolina and sensitive environmental features, such as wetlands, wet soil, steep slopes, stream banks, and rare habitat (threatened habitat as defined by NatureServe and the International Union for the Conservation of Nature — critically endangered, endangered, vulnerable) with at least a 30' buffer. This includes buffers of high quality rivers and streams, and water supply reservoirs.
- » Align corridors to connect existing high-integrity ecological patches such as protected land and parks.
- » Limit disturbance within the corridor as much as possible.



- » Tailor design and construction methods to the unique conditions of the site, such as wet soils or dense endemic vegetation.

## SITE PROTECTION AND PREPARATION

- » Avoid disturbing tree roots. When disturbing tree roots is unavoidable, take special measures to minimize damage. Consult with a certified arborist.
- » When clearing vegetation, avoid straight lines by feathering and thinning remaining vegetation. Always minimize clearing and disturbance as much as possible and consider transplanting plants and soil for reuse.
- » Use non-invasive temporary cover crops, such as winter oats, when soil will be left bare as outlined in the regulatory requirements for erosion and sedimentation control.
- » Remove invasive plant material within the limits of disturbance before planting. This may require two rounds of treatment to ensure species are removed. Prioritize removal of wind-dispersed species before seed set. Avoid herbicides by using suppressive mulches, physical removal, girdling, altering hydrology, prescribed burning, shading, etc.

## WATER

- » Within the watersheds of surface drinking water sources, take great care to develop a “net-zero” water management strategy for stormwater runoff and water quality and maintain at least a 50’ buffer from the waters edge/ bank full from any soil disturbance, with the exception of creation of nongraded, 4’ wide or less soil surface trail tread or invasive plant removal.
- » Manage stormwater as close to where it falls as possible and utilize green stormwater management devices and natural materials as opposed to gray infrastructure (pipes, curbs, drains, etc.) when feasible.

- » Provide convenient disposal sites for domestic animal waste to protect water quality.
- » Provide vegetative buffers between trails and water bodies and steep slopes.
- » Partner trail design and construction with stream restoration when possible to enhance the integrity of the riparian corridors.
- » Locate blueway trail access away from high quality ecological areas.
- » Infiltrate stormwater on plateaus above steep slopes to avoid sending water down slopes.
- » Use infiltration berms, check logs, and retentive grading at the top of slopes.
- » Infiltrate stormwater as close to the sources of run-off as possible. Do not allow stormwater to flow unmanaged to wetland areas or other sensitive features.
- » Protect drainage channels with bioengineering techniques. (See Soil Stabilization.)

## SOIL AND EARTHWORK

- » Stabilize soil and prevent erosion and sedimentation.
- » Protect soil during construction and limit compaction.
- » Prioritize corridors that protect prime farmland soil, but also minimize disturbance of the soil during construction.
- » Stabilize slopes greater than 15% with bioengineering techniques, which use plant materials and structures to stabilize slopes. Possible techniques include live stakes, live fascines, brushlayers, branchpacking, live cribwalls, vegetated rock gabions, vegetated rock walls, joint planting, and compost blankets [Refer to USDA NRCS Engineering Field Handbook Chapter 18 (Gray, 1996) for more information].
- » Utilize small plant material (bareroot, whips, containers) on slopes for ease of installation and increased survivability.

- » Re-mediate compacted soil (soil greater than 200 PSI using a soil penetrometer) by ripping, scarifying, vertical staking, and/or incorporating organic matter.
- » Leave leaf litter in-place.
- » Incorporate diverse soil microorganisms in the soil to promote soil health by using third party-certified compost or third party-certified compost tea at least twice a year during establishment when feasible.
- » Avoid excessive cuts and fills.
- » Retain existing rock formations, vegetation, and effectively drained soils when possible.
- » Prohibit dumping or sloughing excess earth/rock on downhill slopes.
- » Use retaining walls to reduce the amount and extent of earthwork. Use culturally relevant or local, natural materials and colors for the walls.
- » Use landforms that mimic surrounding form, line, color and texture to hide disturbance.
- » Stockpile existing soil for reuse. Do not stockpile soil in mounds greater than 7 ft.
- » Do not disturb soil when it is wet.
- » Specify weed-free mulch and seed mixes.
- » Plant native plants to the region (EPA Ecoregion Level III at minimum).
- » Do not use plants that are toxic to horses.
- » Take into account future projections of climate change and changes in USDA hardiness zones when selecting native plant material.
- » Choose a resilient planting strategy by using native plants that can withstand a range of conditions and occupy many layers and niches.
- » In woodlands and forests, choose a planting palette that addresses multiple layers- ground, shrub and canopy, while keeping clear lines of sight for people and horses.
- » Prioritize a diverse plant palette that provides food for wildlife across seasons and are larval hosts for important pollinators, particularly butterflies and moths.
- » Remove diseased or dying or poor quality trees at the guidance of a certified arborist.
- » Replant areas of disturbance to mimic natural plan density patterns found in the area - clumps-and-gaps- and with mix of types of plants (herbaceous plants, shrubs, trees) and sizes. Do not plant in uniform rows or with only one or two different sizes or species of plant material.
- » Provide gradual transitions between different landscape types (wetlands, meadows, woodlands, forests).
- » Use plants that can thrive in the existing or new environmental conditions. Choose locally grown species that are straight species (no cultivars except near buildings) and are local genotypes.
- » Propose a mix of diverse plants: deciduous and evergreen, early-successional (majority) and late-successional (minority), and different habits and forms.
- » Propose plants that are diverse in color, texture, type and size.

## VEGETATION

- » Protect vegetation during construction and consider transplanting vegetation that must be disturbed during construction.
- » Pathways and trails can be corridors that spread invasive species. Eradicate invasive species during construction and over the long term management of the corridor.
- » Utilize groundcover plants to cover bare soil.
- » Specify dense planting and temporary cover crops in areas known to be hotspots for invasive species, such as areas near existing invasive species and sunny areas, such as south-facing edges.

- » Plant in densities that can out-compete invasive species, particularly adjacent to areas with invasive species and south-facing edges and slopes. Manage invasive species over the long-term to protect new plants before and after planting.
- » When seeding, properly prepare the seedbed by decompacting the soil and ensuring good seed-to-soil contact. Consider using techniques such as land imprinting to prepare the soil and soil drills or a two-step hydroseeding/hydromulching method for seeding.
- » Plant at the best time for each group of plants. Do not plant anything in the summer and avoid planting fall hazard trees in the fall. Use non-invasive, annual cover crops to stabilize soil in the winter.
- » Protect plants from herbivory by deer, etc.
- » Consider stockpiling native vegetation in areas to be graded in a temporary nursery so they can be reused on the project.

## VIEWSHEDS

- » Balance prioritizing view cones and viewsheds to high quality visual resources from trails, with protecting views from established high visual resources outside of the corridors to the trails and to people.
- » Utilize visual buffer elements such as vegetation, architectural features or art to hide undesirable manmade features and infrastructure.

## HABITAT AND WILDLIFE

- » Utilize strategies that minimize wildlife disturbance and human-wildlife conflicts.
- » Route trails through already disturbed areas or at the edge of high quality habitat, rather than through its center.
- » Minimize the number of times prominent landscape corridors— such as riparian zones— are crossed by a trail. These corridors may serve as

important conduits and habitat for wildlife.

- » For both habitat and maintenance reasons, it is better to run a trail just outside the riparian area (perhaps on a topographic bench) and bring it in at strategic places, than to keep it continuously close to a riparian area.
- » In routing a trail near a pond or lake, don't run it completely around the body of water. Instead, leave some shoreline without a trail to allow water birds the option of moving away from people to the far side of the pond.
- » Avoid crossings where two or more streams come together. These are particularly important nodes for wildlife.
- » Either avoid wildlife breeding areas or close trails through them at the times such wildlife are most sensitive to human disturbance.

## MANAGEMENT

- » Use adaptive management strategies and best practices in restoration ecology to manage the pathway and trail corridors.
- » Track changes in vegetation and environmental conditions on an annual basis at a minimum. Set a baseline condition and goals for the landscape at set time intervals for monitoring. Set indicators to monitor to assess whether or not the landscape is on a positive or negative trajectory. Indicators to monitor include:
  - Vegetation and litter cover
  - Soil structure (compaction, crusting, aggregate size)
  - Organic matter
  - Infiltration, water-holding capacity of soil
  - Erosion and sedimentation, gullies and rills
  - Plant diversity, invasive species
  - Soil biology, presence of earthworms
  - Soil chemical properties (pH, salinity, CEC, etc.)



- Excessive herbivory
- » Devote resources to controlling invasive species, pests, and disease on a biannual basis at a minimum, including gypsy moths and sudden oak death.
- » Monitor the site after any disturbances in the landscape that may encourage invasive species. Opportunities in landscapes and wetlands caused by natural or man-made events (hurricanes or large storms, runoff occurrences or hydrologic change from upland development, etc.) where invasive species tend to proliferate include:
  - Canopy gaps, where trees have fallen
  - Nutrient pulses (for example, from runoff from a recently fertilized adjacent site)
  - Sediment deposition and bare soil (for example, soil erosion from an upland area where stormwater is not controlled)
- » Devote resources for supplemental planting in the event of a large disturbance. Use rhizomatous and/or fast-growing native species with temporary cover crops on the ground layer supplemented with native trees and shrubs in order to discourage invasive species establishment.
- » Protect new plants from herbivores.

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## Equitable Trail Design

Equitable trail planning focuses on a fair and context-specific distribution of pathway and trail resources. An equitable trail system takes into account the varied needs of diverse users and seeks to identify and remove potential barriers to use. Barriers can be addressed through thoughtful design of trailhead location, wayfinding, trail maintenance, ADA compliance, lighting, marketing and programming.

Planning and design of the pathway and trail projects should begin with an inclusive planning process that allows community members to weigh in with their needs, goals, concerns and priorities. An inclusive planning process can lay the groundwork for long-term health of the system by securing community buy-in and cultivating a sense of ownership and stewardship.

Best practices for equitable trail design include:

- » Seek the expertise and leadership of community-based organizations and stakeholders.
- » Utilize a systems approach that recognizes the interaction of the pathway and trail project within larger systems of housing, transportation and public safety.
- » Use outreach materials that are accessible to people who speak different languages or are visually impaired.
- » Within longer-term corridor project buildout, prioritize short-term implementation projects that can demonstrate progress and build trust with historically marginalized communities.
- » Work with communities to develop programming and maintenance regimens that meet local needs.

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## Trail Types

Conway's Pathway and Trails Plan utilizes the City's Unified Development Ordinance standards and the U.S. Department of Agriculture Forest Service (USFS) Trail Classifications to describe and categorize the City's pathways and trails. These classifications – and the Pathways and Trails Plan Design Guidelines – apply to recreational trails, as distinguished from the sidewalks and shared-use paths. For paved trails – which primarily are used to connect to or within Downtown Conway or cross-city systems – the AASHTO Bike and Pedestrian Guides should be followed.

## PATHWAY AND TRAIL DESIGN FOR PEDESTRIANS AND HIKERS

- » **Surfaces:** Compacted aggregate, resin stabilized aggregate, natural soil surface, wood chips
- » **Running grades:** Maximum 5% for accessibility and otherwise maximum 8-10%, with minimum running grades of 1%
- » **Cross slopes:** Minimum 1% and maximum 5%
- » **Trail tread width:** Minimum 5' for primary cross-city trails and as narrow as 1' for primitive trails
- » **Vertical clearance:** Minimum 8' from trail surface
- » **Horizontal clearance:** 2' shoulders on either side of tread
- » **Typical trail distances:** 2-4 miles for casual walkers / 4-10 miles for general hikers / 6-15 miles for fitness hikers / 3-15 miles for trail joggers

## MULTI-USE PATHWAY AND TRAIL DESIGN

- » **Surfaces:** Compacted aggregate, resin stabilized aggregate, asphalt, boardwalk, or compacted natural soil surface
- » **Running grades:** Vary widely, but average grades should not exceed 10% for maximum sustainability
- » **Trail tread width:** 8' Minimum 8'
- » **Vertical clearance:** Minimum 8' from trail surface
- » **Horizontal clearance:** 1' shoulders on either side of tread
- » **Typical trail distances:** 3-12 miles for general hikers and recreational bikers / 6-20 miles for advanced bikers / 10-25 miles for recreational network bikers / 15-50 miles for advanced network bikers

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## Trail Design Guidance

### AVOIDING TRAIL USER CONFLICTS

The Conway pathway and trail system will serve hikers, bicycle users, walkers, blueway users, and within those broad user categories are individuals with a variety of abilities, expectations and needs. Trail design should anticipate potential user needs and provide trails that promote safety, protect natural resources and provide high-quality experiences for all users.

Wherever the pathway and trail corridor and environmental conditions allow, the primary trails of the Conway system should provide separated treads for different user types, with well-designed crossings and varied buffer widths that promote positive interactions between different users. The National Recreational Trails Advisory Committee and Federal Highway Administration provide the additional guiding principles for the planning and design of trails that prevent user conflict:

- » Provide adequate trail mileage to reduce congestion and allow all users to feel their needs are being met.
- » Minimize the number of contacts between users, particularly in congested areas and at trailheads.
- » Engage local users in the project-specific planning and design process, to allow all voices to be heard and to ensure trail design accommodates community-specific needs.
- » Promote trail etiquette and promote responsible trail behavior through educational materials featured on trail websites and on-site signage.
- » Bring users together through efforts such as volunteer trail maintenance or advisory councils.

### ACCESSIBILITY

In order to serve the broadest possible cross-section of the Conway community,

the primary cross-city trails of the pathway and trail system should always seek to follow universal design principles.

Universal design for trails means that tread conditions and connections accommodate users of all ability levels through manageable slopes, firm and stable surfaces, and adequate horizontal and vertical clearances. Universal design also entails providing signage that is legible for all people and pathway and trail amenities – such as parking, picnic tables and drinking fountains – that can be used by all visitors.

Accessible trails will not be feasible in all contexts. The City will determine on a project-by-project basis whether environmental conditions allow for accessible design. The U.S. Access Board defines the following limiting factors to help identify conditions in which accessible trails might not be feasible:

4. Trail grades or cross slopes exceed 40% for 20' or more.
5. The trail surface is not firm and stable for 45' or more.
6. The tread width is less than 12" for 20' or more.
7. Obstacles of 30" or higher are present across the full width of the trail.

If any of those limiting factors are present, the City and its trail design team should evaluate whether accessibility is practical and how to balance the system's universal design and natural resource protection goals.

Where accessible trail design is pursued, the following minimum standards should apply:

- » Choose a trail route that is relatively level; can be modified to allow two users to pass; and does not have excessively steep slopes, drop-offs or tight turns.
- » Routes should be at least 36" wide for single users and 60" to allow for passing. Minimum vertical clearance

of 80" should be maintained except where prevented by natural intrusions such as tree limbs. Constructed objects such as signs should not protrude into the minimum 36" wide by 80" high clearance space.

- » Running slopes should not exceed 10%. Slopes over 5% should not exceed 50' in length, and slopes over 8% should not exceed 30' in length. Target slopes should be 5% or less.
- » Cross slopes should not exceed 3%. Target cross slopes should be between 1% and 2%.
- » Trail surfacing should be firm and stable and can include compacted aggregate, resin stabilized aggregate, boardwalk and compacted native soil. Soil hardeners or resin binders can be utilized where site soil cannot be suitably compacted to become firm and stable.
- » Accessible trails should have trailheads with accessible parking and signage that clearly describes the trail conditions and difficulty – including description of trail length, surface type, typical and minimum tread width, and typical and maximum running and cross slopes.

The City and its partners should be aware of whether funding sources for individual projects require certain accessibility standards – for example, the U.S. Access Board's Accessibility Guidelines for Outdoor Developed Areas or the USFS Outdoor Recreation Accessibility Guidelines.

## NATURAL SURFACE TRAILS

The majority of Conway's recreational trails are unpaved and fall within natural or cultural heritage resource corridors, such as stream valleys or protected rural open space. Natural surface trails are well-suited for hiking, mountain biking and horseback riding and support the recreation- and natural resource-oriented goals of the City's pathway and trail



system. Trail surfaces should comply with city and state guidelines, including limitations within floodplain.

Natural surface trails include trails with the following surface materials:

- » Native soil
- » Wood chips
- » Compacted aggregate
- » Resin stabilized aggregate
- » Sand

## ROAD CROSSINGS

### At-grade Crossings

Wherever existing or proposed pathways and trails must cross a roadway, City parks and transportation staff should collaborate to create a safe and high-quality crossing experience.

The City should refer to the latest guidance from SCDOT and AASHTO for determining minimum standards for pedestrian and bicycle crossing accommodations. For multi-lane roads, crossings should include a median pedestrian refuge.

Signalization is strongly recommended for at-grade crossings with heavy traffic or high speed traffic and signalization should be accessible to the height of cyclists and pedestrians.

### Separated Crossings

Trail bridges or underpasses allow trail users to avoid crossing lanes of traffic but can add significant expense and grading challenges to trail systems and are preferred in contrast to at-grade crossings. Underpass crossings are recommended where trails follow a stream corridor or other feature that crosses underneath an existing roadway bridge. In these scenarios, trails should be at least 14' wide where possible, with at least 10' minimum vertical clearance for hikers and bicycle users. Underpass crossings should be well-lit with vandal-proof light fixtures.

## Trails along Roadways

Wherever trail gaps mandate that alignments must follow roadways for a certain distance, trails should be set back at least 8' from the roadway, in keeping with City standards for shared-use paths.

## STREAM CROSSINGS

Stream crossings should be avoided wherever possible to maximize trail safety and minimize environmental impacts. Wherever stream crossings are essential for system connectivity and access, options include:

- » Fair weather crossing structures where streams are relatively shallow, water volume is light, and banks are gently sloped. Fair weather crossings allow water to flow over a concrete pad, with cylindrical concrete stepping stones elevated above water level to allow hikers and dismounted bikers to cross.
- » Bridges where water volume is high, stream banks are steep, and the level of trail use justifies the expense. When the expense is justified, bridge designs offer a unique opportunity to reinforce the branding and identity of the pathway and trail system.

## TRAIL EXPERIENCES

The pathway and trail system should offer a variety of trail experiences that reflect a sense of place and the diversity of Conway's cultural and natural landscapes.

Each individual trail project should be designed with user experience in mind, with alignments that create and sustain interest and spotlight special features in the landscape. By following the contours of the land, weaving through different environments and taking advantage of subtle changes in topography and viewshed, trails can be designed to give their users a sense that there is always something ahead to be discovered.

Designers of Conway's pathway and trail projects should follow these general principles of trail design.

- » **Trail shape:** In determining alignments, use trail shapes that suit their context. For example, straight or constant-radius curved trails often suit urban or dense suburban areas because they are responding to the built rather than natural environment. Incorporating arbitrary irregular curves in an urban environment feels unnatural and is not good trail design. In natural areas, on the other hand, trail alignments should feature irregular curves that respond to the landscape.
- » **Anchors:** Identify anchors, or points of interest that draw the eye, along the trail corridor. Align the trail to respond to these anchors – for example, by wrapping the trail around the anchor, or by aligning the trail so that it approaches and then turns away from the anchor, positioning the anchor in the user's sightline.
- » **Edges:** The same principle holds for how trails should interact with edges, such as transition areas between different habitat types, waterbodies or ridgelines. Trails should either follow and wrap the edgeline, or position the edge in the sightline of the trail user. For transitional areas between different environments, it can be effective to generally follow the edgeline within one zone and then occasionally weave the trail into the neighboring zone and back out.
- » **Gateways:** Trail alignments should seek out natural gateways, or framing devices such as arching tree branches that give the impression of compressing an open space. Gateways can also be created – for example, through structures that frame entrances to boardwalks or at trailheads.

Material and planting palettes are another opportunity to reinforce sense of place and high-quality trail experiences.

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## Trailheads and Amenities

Trailheads are the public access points for the pathway and trail system. Primary trailheads provide parking and a kiosk or signage with trail information; they can also offer restrooms, picnic shelters and other amenities. Secondary trailheads provide trail access with limited signage and might or might not provide parking or other amenities.

Primary trailheads should be located as needed in urban and suburban areas, with easy access to residential areas, public roads and transit stops. The City should take advantage of opportunities to co-locate trailheads with public parks, schools and community centers and to share parking and restroom facilities.

In rural areas, primary trailheads should be spaced every 5 miles for heavily used trails and every 10 miles for lesser used trails.

Secondary trailheads should be pursued wherever practicable, such as where trail alignments cross public land or rights-of-way, in order to maximize the accessibility of the pathway and trail system.

The following amenities should be provided at primary trailheads:

- » Parking lots
- » Restrooms
- » Seating
- » Interpretive and directional signage
- » Litter receptacles
- » Bike racks and repair stations
- » Pet waste containers
- » Lighting
- » Kiosks

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## Wayfinding and Signage

### BRANDING

Conway should develop branding and signage guidelines that reflect the unique natural and cultural character of the system. Given the importance of connectivity to and within other trail systems, the City should coordinate these guidelines with partners including the South Carolina Department of Transportation, Towns of Loris and Aynor, and City of Myrtle Beach.

City-specific branding and signage should incorporate complimentary colors, fonts, materials, iconography and legibility. Consistent and recognizable wayfinding elements will help to orient trail users and facilitate connections across different regional trail systems. It will also strengthen public awareness and support for citywide pathways and trails.

### SIGNAGE

Legible and consistent signage helps to orient, prepare and educate pathway and trail users. Below are the types of signs that will be used throughout the system.

- » **Regulatory signs:** These signs share information about requirements for speed, stop and yield locations, usage rules, and other safety-related messages. Certain regulatory signs are required by state and federal funding agencies, and those standards rely on the Manual on Uniform Traffic Control Devices by the Federal Highway Administration. Regulatory signs should be posted at trailheads and wherever required by local or state law.
- » **Warning signs:** These signs alert trail users to be aware of their surroundings and should be placed wherever needed to call attention from trail users.
- » **Informational signs:** These signs share information that helps trail users learn about the trail and plan their

trip. Information might include trail difficulty, length, trailhead locations, emergency contact information, and the locations of nearby attractions and boat ramps. Kiosks, panel signs and milemarkers are all examples of informational signs. Informational signs should be placed at trailheads, to mark trail destinations, and ideally every half-mile for milemarkers.

- » **Educational or interpretive signs:** These signs teach trail users about the natural, historical or cultural features of the trail and help forge connections between trail users and their environment. Interpretive signs should be placed at trailheads and along the trail as needed to call out special features and learning opportunities.
- » **Bridge, “Banner” or “Billboard” signs:** Large, visually appealing signs that are cohesive with the city’s branding and the branding of the Pathways and Trails system can be used to indicate publicly accessible destinations or trail experiences along corridors and at major corridor access points.

### APPS AND DIGITAL GUIDES

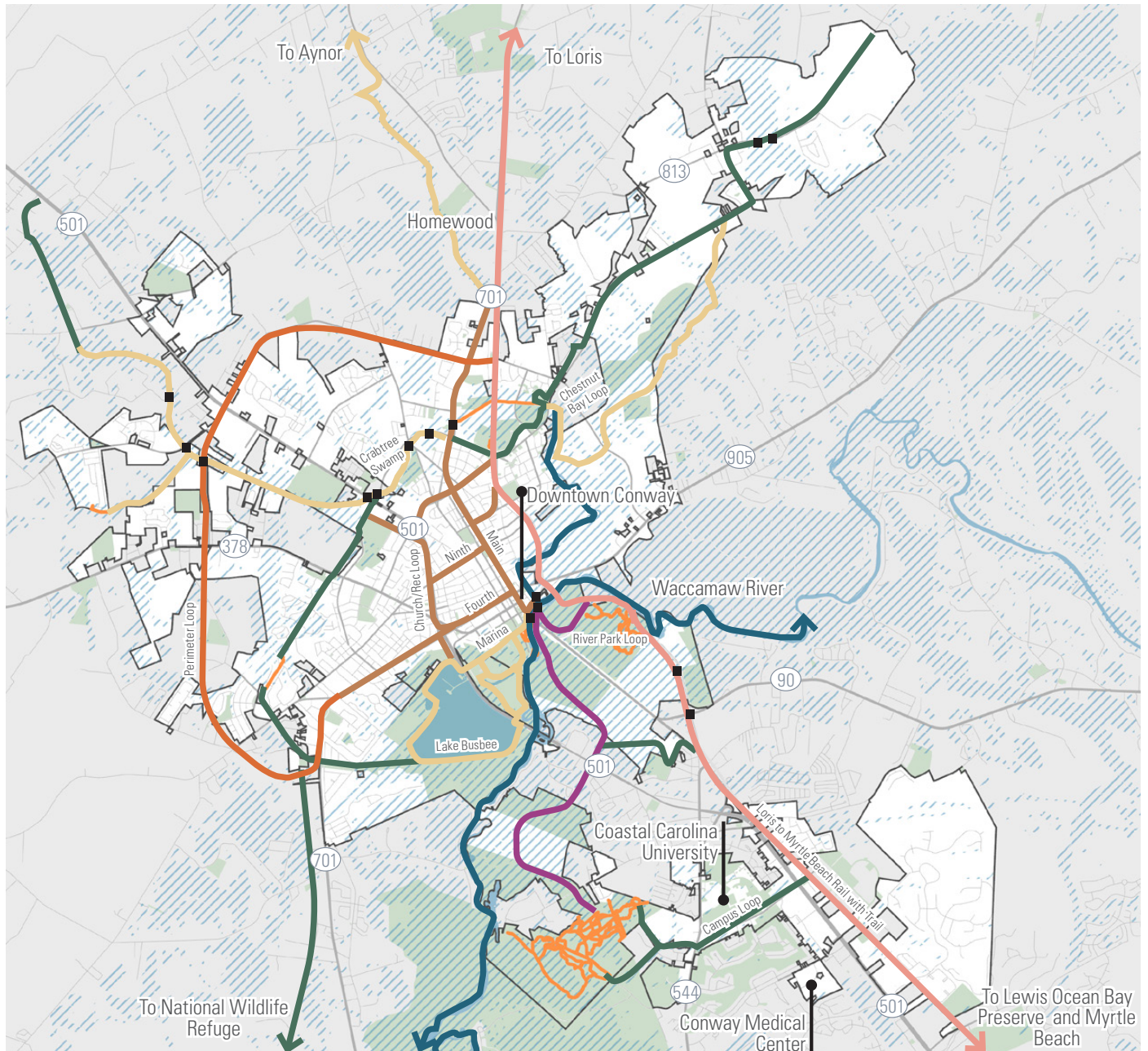
Pathways and Trails users can benefit from a digital guide that follows their journey throughout the corridors, identifies key destinations or stopping points, interprets the historic, cultural and natural landscapes, and provides current information and alerts related to maintenance, accessibility, and use. The proposed corridors from the Plan are rich in Native American history, Revolutionary War and Civil War history, agricultural and settlement history, and natural history. Opportunities for learning about history and nature can be integrated into interpretive wayfinding and signage along trails via a mobile or web-based app, QR code readers, or digital navigation/mapping applications. An app can also provide information about outfitters or cultural destinations along the corridor, as well as educational opportunities for all ages.

### Public Input

The top two ways people said they would like to be able to find information about pathways and trails in Conway is via a website or mobile app. (Survey 1)



# TRAIL TYPOLOGIES



## Trail Typologies

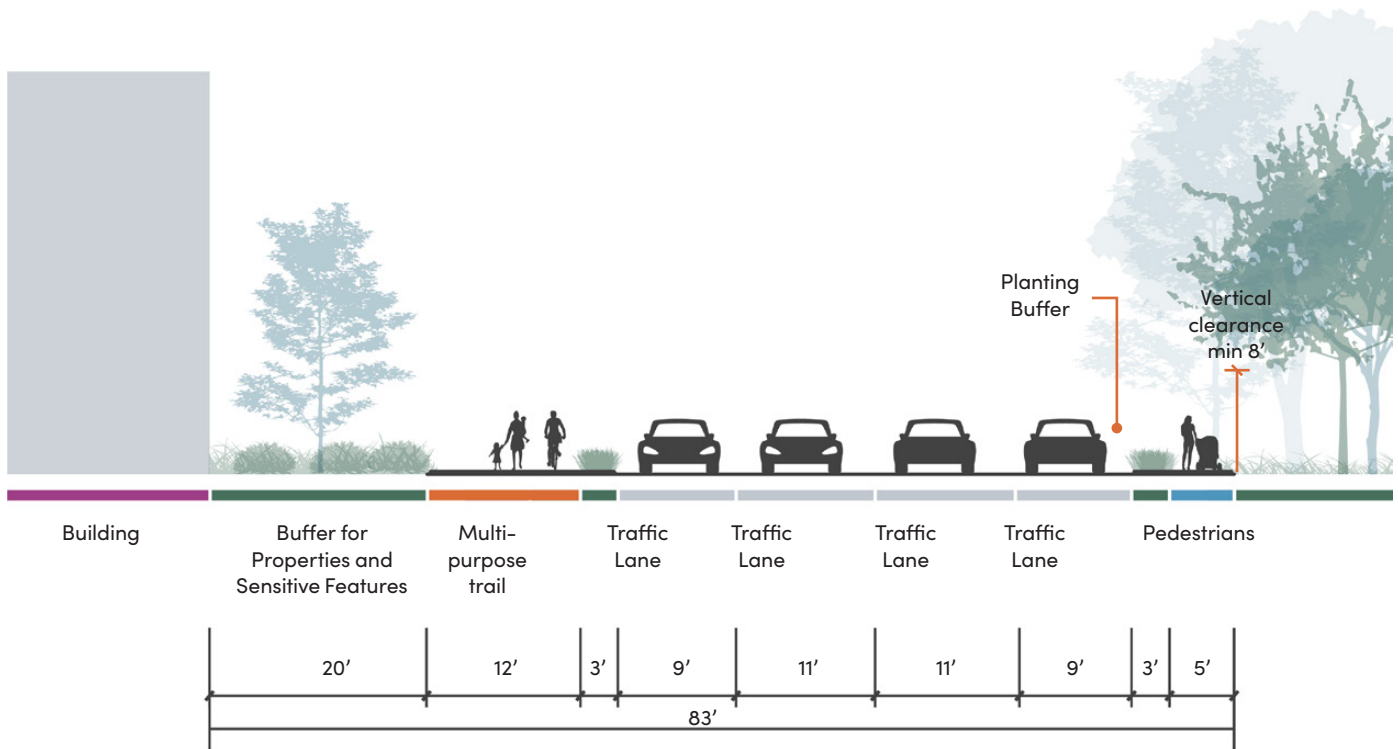
By stitching together a citywide system of pathways and trails that follow canals, the railroad, stream corridors and road corridors – and further imagining spurs and loops through a diversity of natural and cultural landscapes – the plan can satisfy residents' vision for an interconnected system of natural, urban and suburban trails with distinct experiential qualities.

### LEGEND

- |  |                                       |
|--|---------------------------------------|
| CITY OF CONWAY BOUNDARY                          | MULTI-USE TRAIL (URBAN/SUBURBAN)      |
| MAJOR ROADS                                      | MULTI-USE TRAIL (RURAL)               |
| ROADS  | SWAMP, LEVEE & CANAL TRAILS           |
| EXISTING TRAIL                                   | HISTORIC, BOULEVARD & MAIN ST. TRAILS |
| WATER BODY                                       | LORIS TO MYRTLE BEACH RAIL WITH TRAIL |
| PUBLIC LANDS AND UTILITY CORRIDORS (LIGHT GREEN) | WETLAND OR SENSITIVE LANDS TRAIL      |
| WETLANDS   | BLUEWAYS/PADDLE TRAILS                |
| PEDESTRIAN/BIKE WATER CROSSINGS                  |                                       |

# URBAN / SUBURBAN TRAILS - TYPE A

## COMMERCIAL AREAS



**Table 2:** Design Guidelines table

<b>Users</b>	Pedestrians and bicycle users
<b>Contexts</b>	Town, suburban, urban, high traffic areas
<b>Typical Tread Widths</b>	5-12'
<b>Materials</b>	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
<b>Maintenance</b>	Every 5 to 10 years
<b>Slopes</b>	Target 5% or less for ADA paths / Cross slope 2% max
<b>Buffers and Clearance</b>	Min 20' from roadways, min 8' vertical clearance
<b>Amenities, Safety and Other Design Features</b>	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%

# URBAN / SUBURBAN TRAILS - TYPE B

## RESIDENTIAL AREAS

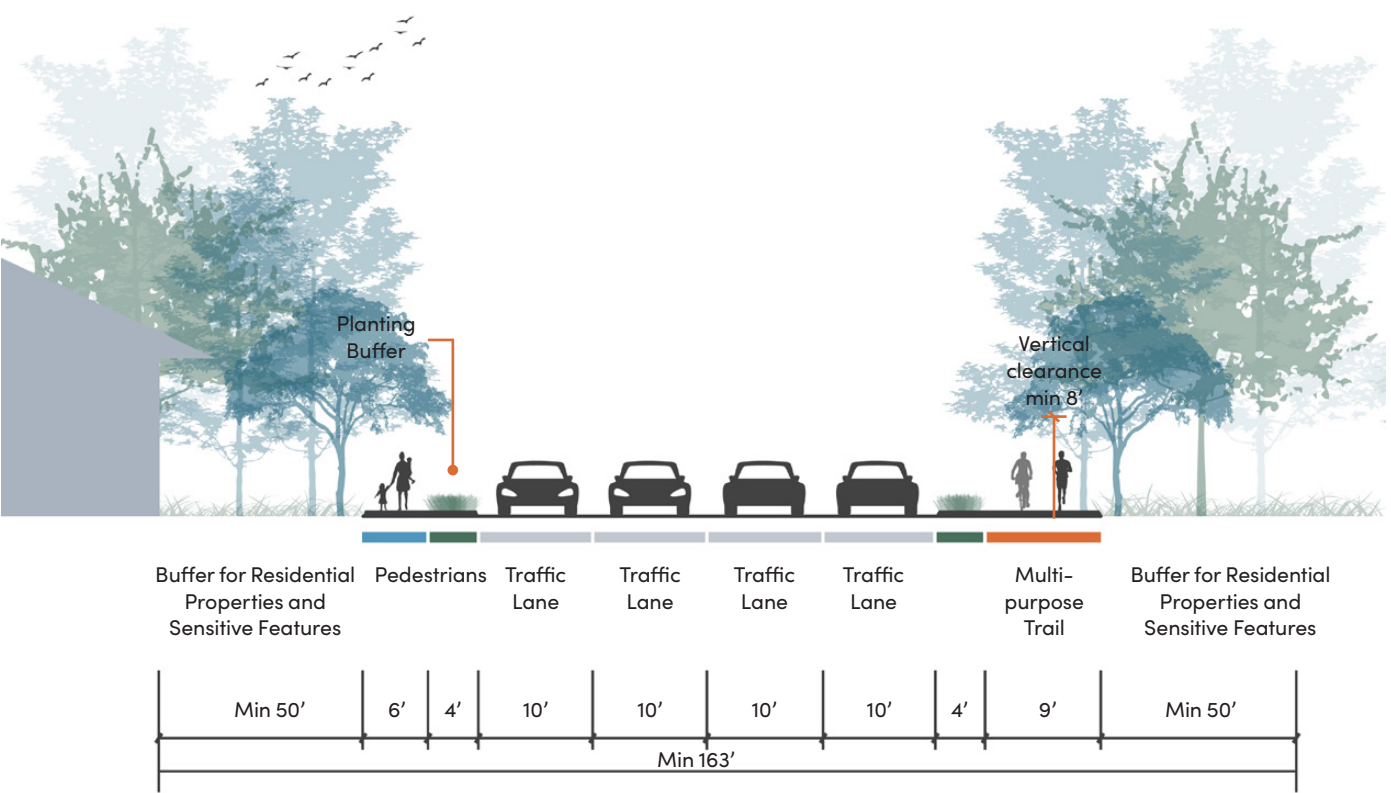


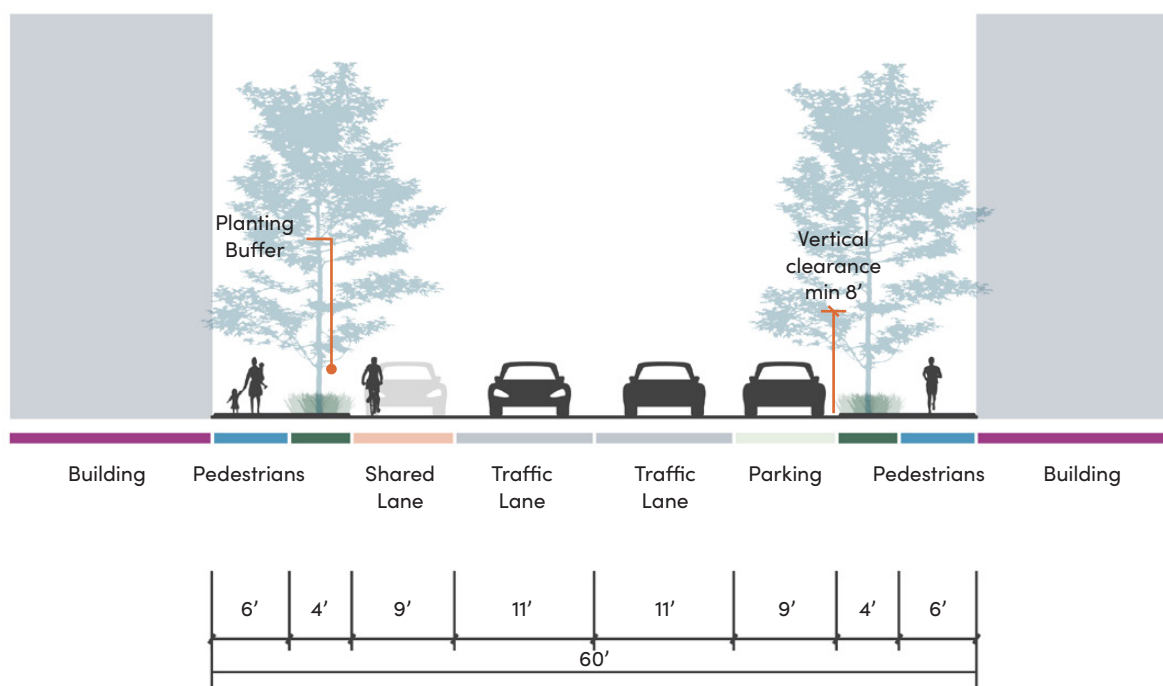
Table 3: Design Guidelines table

Users	Pedestrians and bicycle users
Contexts	Town, suburban, urban, high traffic areas
Typical Tread Widths	6-9'
Materials	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
Maintenance	Every 5 to 10 years
Slopes	Target 5% or less for ADA paths / Cross slope 2% max
Buffers and Clearance	Min 50' from roadways, min 8' vertical clearance
Amenities, Safety and Other Design Features	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%



# HISTORIC / BOULEVARD / MAIN ST TRAILS – A

## WITH SHARED PARKING ZONE



**Table 4:** Design Guidelines table

<b>Users</b>	Pedestrians and bicycle users
<b>Contexts</b>	Town, suburban, urban, high traffic areas
<b>Typical Tread Widths</b>	6-9'
<b>Materials</b>	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
<b>Maintenance</b>	Every 5 to 10 years
<b>Slopes</b>	Target 5% or less for ADA paths / Cross slope 2% max
<b>Buffers and Clearance</b>	Min 8' vertical clearance
<b>Amenities, Safety and Other Design Features</b>	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%

# HISTORIC / BOULEVARD / MAIN ST TRAILS – B

## WITH SHARED DRIVE LANE

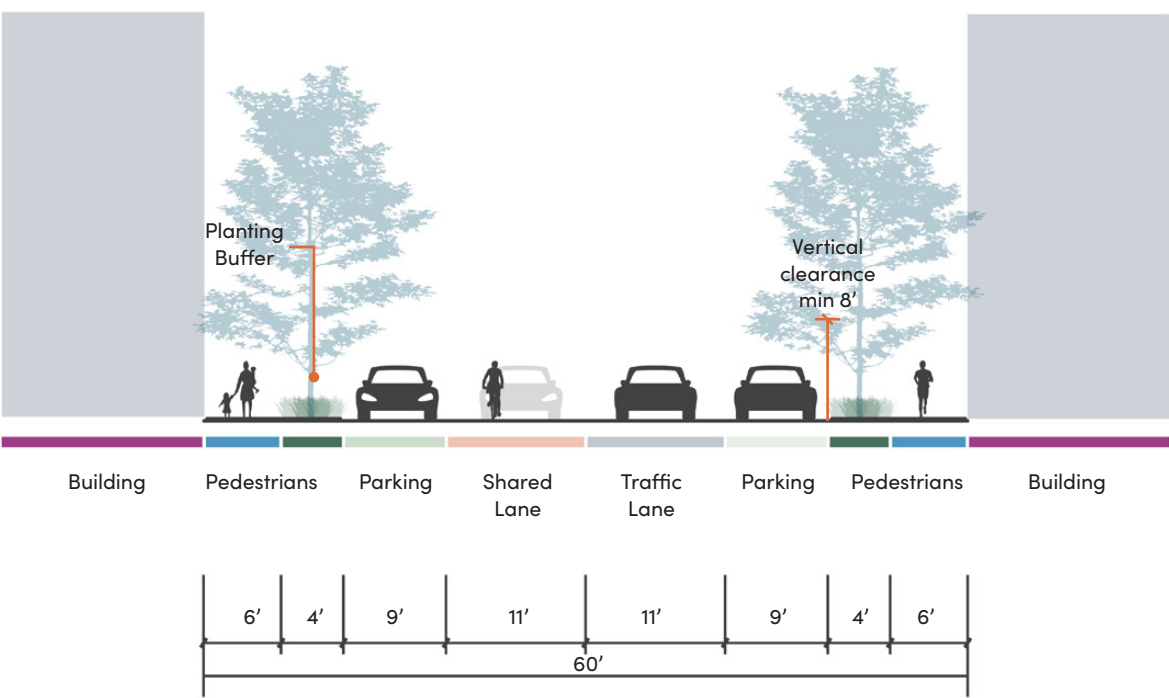
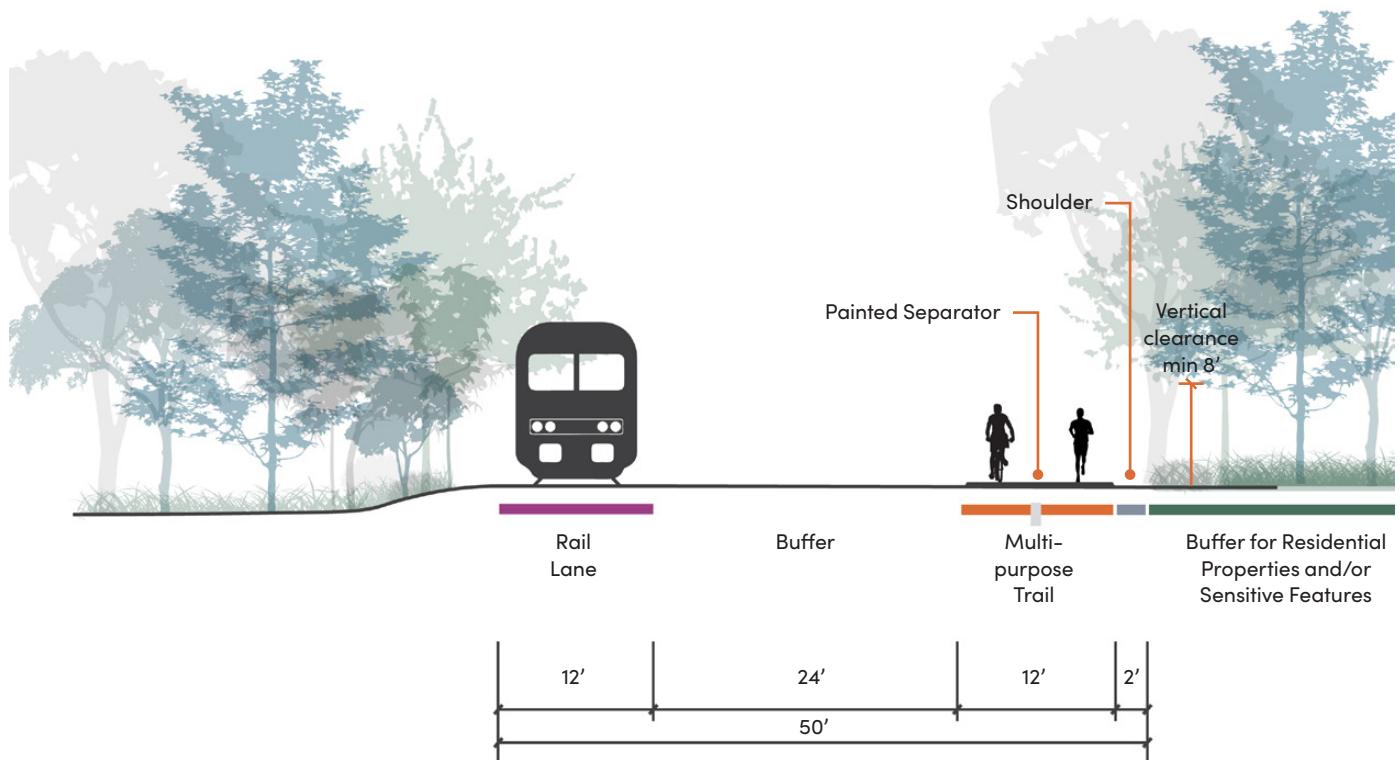


Table 5: Design Guidelines table

Users	Pedestrians and bicycle users
Contexts	Town, suburban, urban, high traffic areas
Typical Tread Widths	6-11'
Materials	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
Maintenance	Every 5 to 10 years
Slopes	Target 5% or less for ADA paths / Cross slope 2% max
Buffers and Clearance	Min 8' vertical clearance
Amenities, Safety and Other Design Features	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%

# LORIS-TO-MYRTLE BEACH RAIL WITH TRAIL



**Table 6:** Design Guidelines table

<b>Users</b>	Pedestrians and bicycle users
<b>Contexts</b>	Town, suburban, urban, high traffic areas
<b>Typical Tread Widths</b>	12'
<b>Materials</b>	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
<b>Maintenance</b>	Every 5 to 10 years
<b>Slopes</b>	Target 5% or less for ADA paths / Cross slope 2% max
<b>Buffers and Clearance</b>	Min 24' from roadways, min 8' vertical clearance
<b>Amenities, Safety and Other Design Features</b>	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%



# PEDESTRIAN / BIKE WATERBODY CROSSING

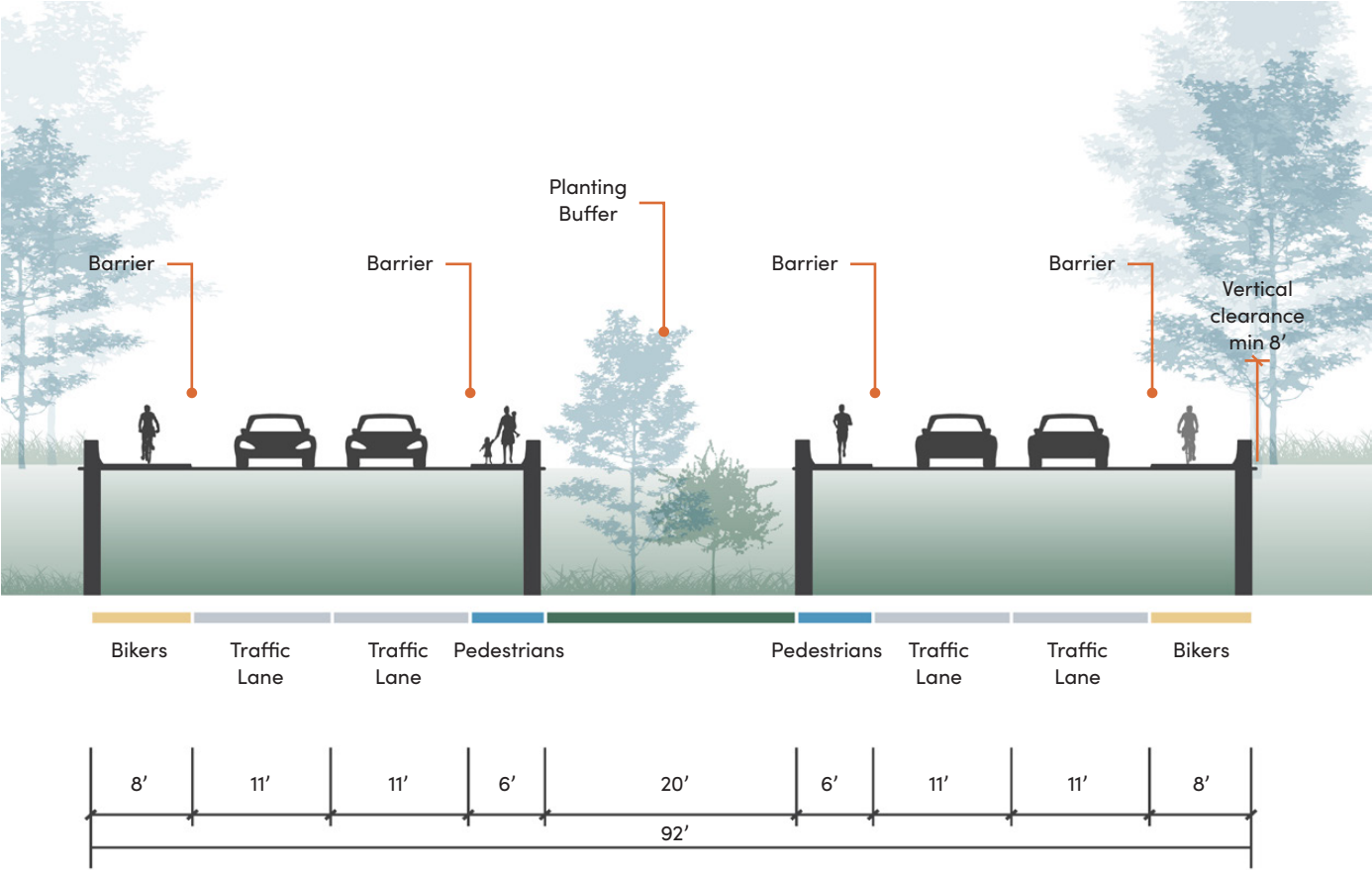


Table 7: Design Guidelines table

Users	Pedestrians and bicycle users
Contexts	Town, suburban, urban, high traffic areas
Typical Tread Widths	6-8'
Materials	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
Maintenance	Every 5 to 10 years
Slopes	Target 5% or less for ADA paths / Cross slope 2% max
Buffers and Clearance	Min 8' vertical clearance
Amenities, Safety and Other Design Features	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%

# RURAL TRAILS – TYPE A

## SCENIC ROUTE

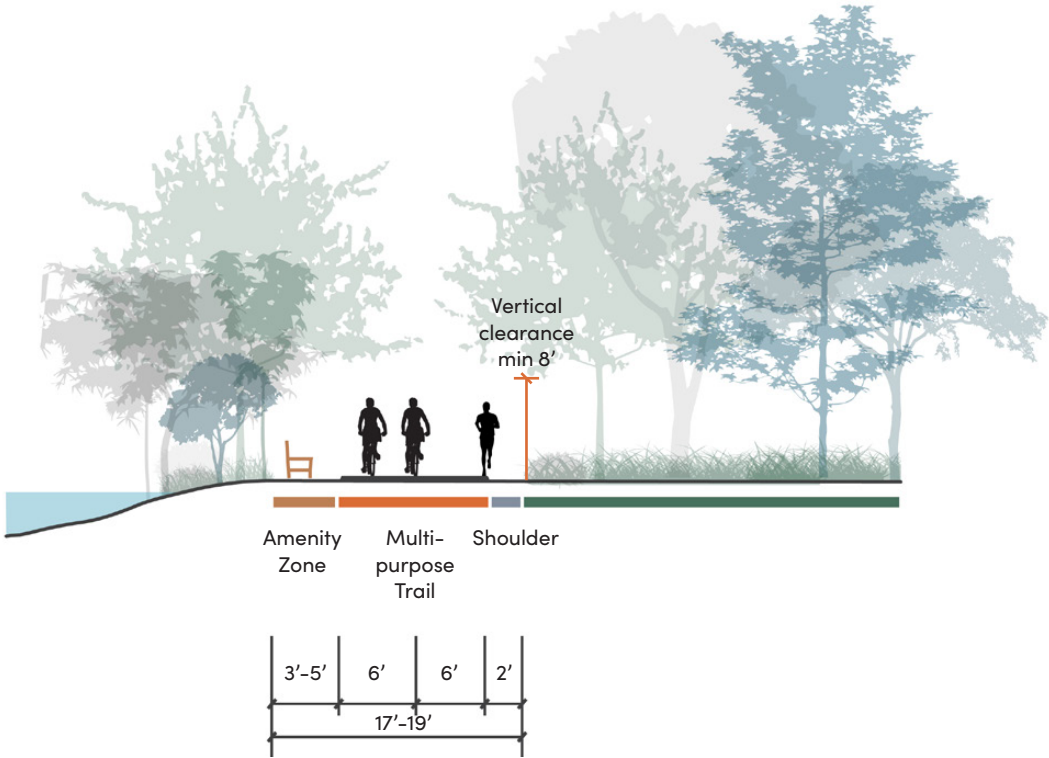
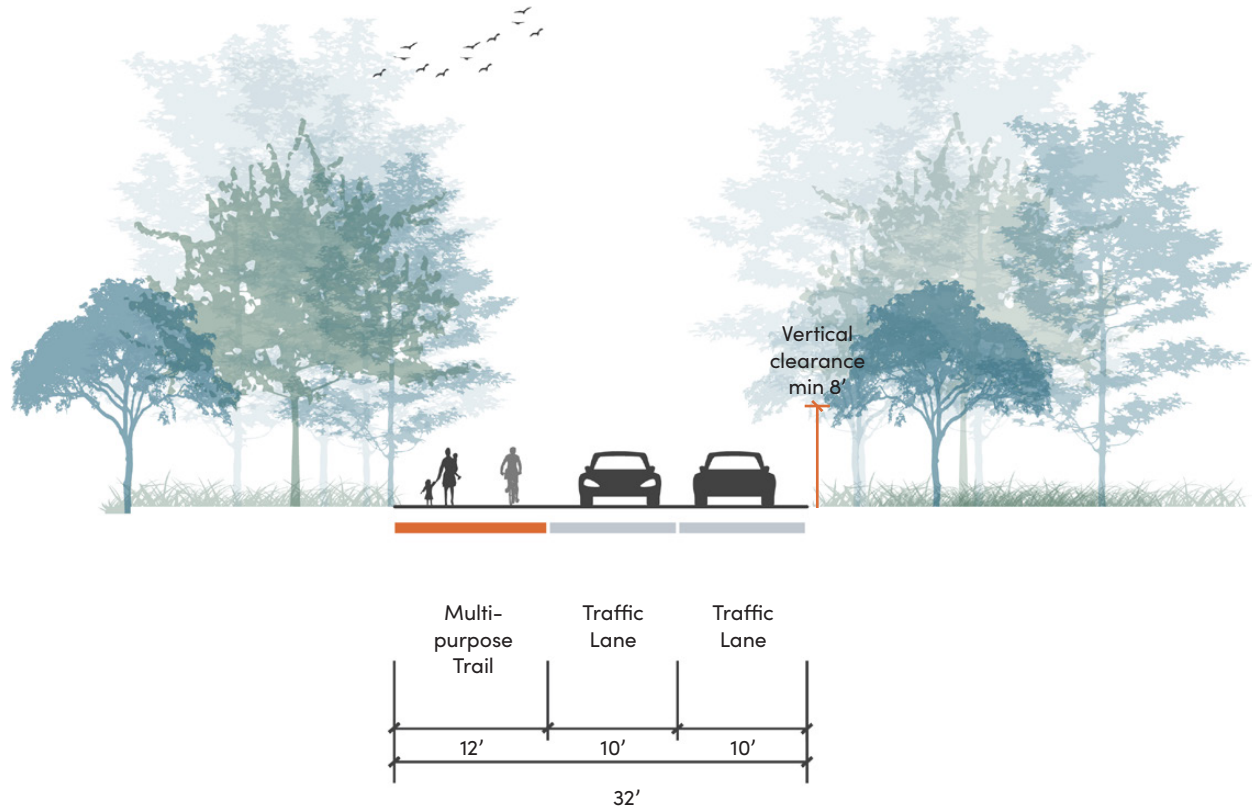


Table 8: Design Guidelines table

Users	Pedestrians and bicycle users
Contexts	Suburban, rural areas
Typical Tread Widths	12'
Materials	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
Maintenance	Every 5 to 10 years
Slopes	Target 5% or less for ADA paths / Cross slope 2% max
Buffers and Clearance	Min 8' vertical clearance
Amenities, Safety and Other Design Features	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%

# RURAL TRAILS – TYPE B

## ADJACENT TO ROADS

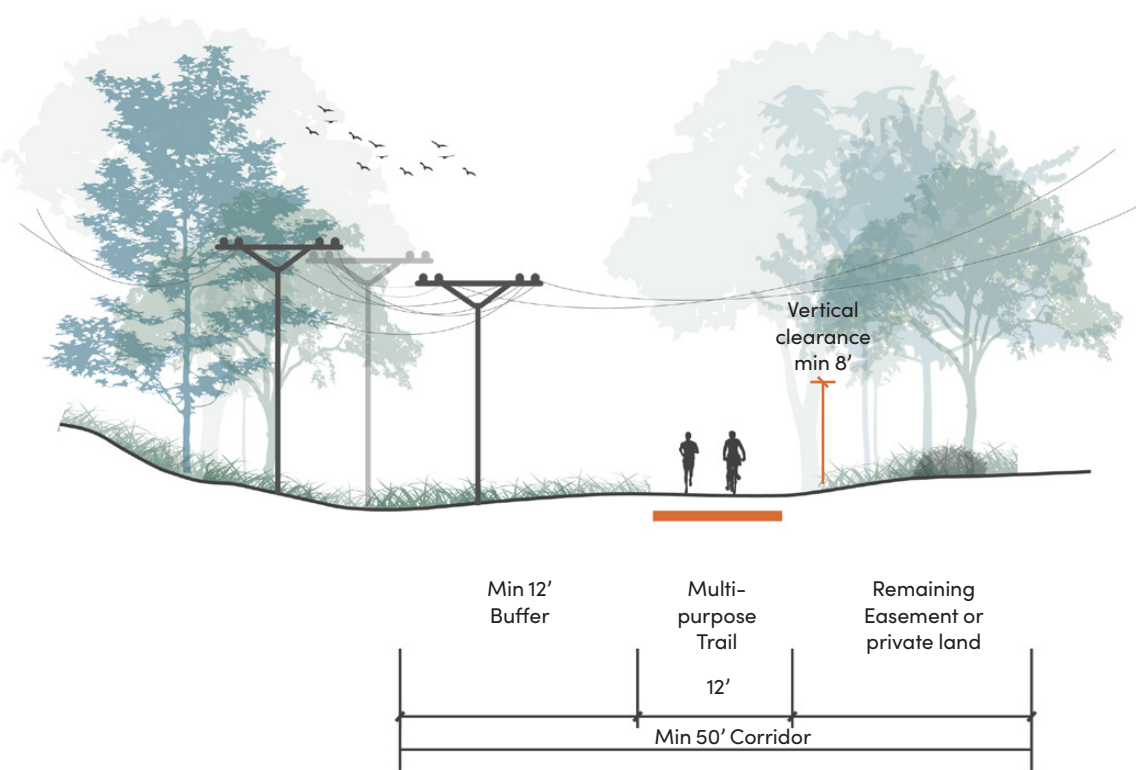


**Table 9:** Design Guidelines table

<b>Users</b>	Pedestrians and bicycle users
<b>Contexts</b>	Suburban, rural areas
<b>Typical Tread Widths</b>	12'
<b>Materials</b>	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
<b>Maintenance</b>	Every 5 to 10 years
<b>Slopes</b>	Target 5% or less for ADA paths / Cross slope 2% max
<b>Buffers and Clearance</b>	Min 8' vertical clearance
<b>Amenities, Safety and Other Design Features</b>	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%



# RURAL TRAILS – TYPE C (UTILITY EASEMENTS)



**Table 10:** Design Guidelines table

Users	Pedestrians and bicycle users
Contexts	Rural, suburban
Typical Tread Widths	12'
Materials	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
Buffers and Clearance	Min 8' vertical clearance, minimum 25' away from power structures, perpendicular trail crossings and approved use of existing utility roads for parallel trails only

# SWAMP, LEVEE OR CANAL TRAILS

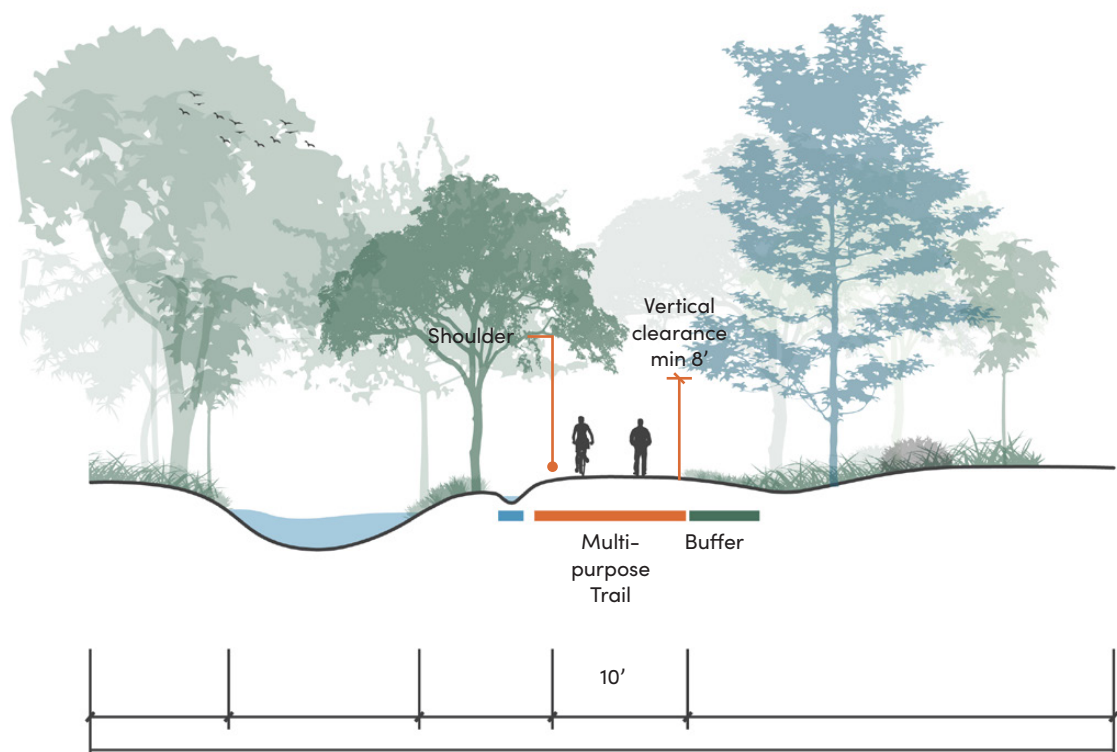


Table 11: Design Guidelines table

Users	Pedestrians and bicycle users
Contexts	Floodplains
Typical Tread Widths	10'
Materials	Natural soil surface, wood mulch, sand
Buffers and Clearance	Min 8' vertical clearance, railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%

# WETLAND TRAILS

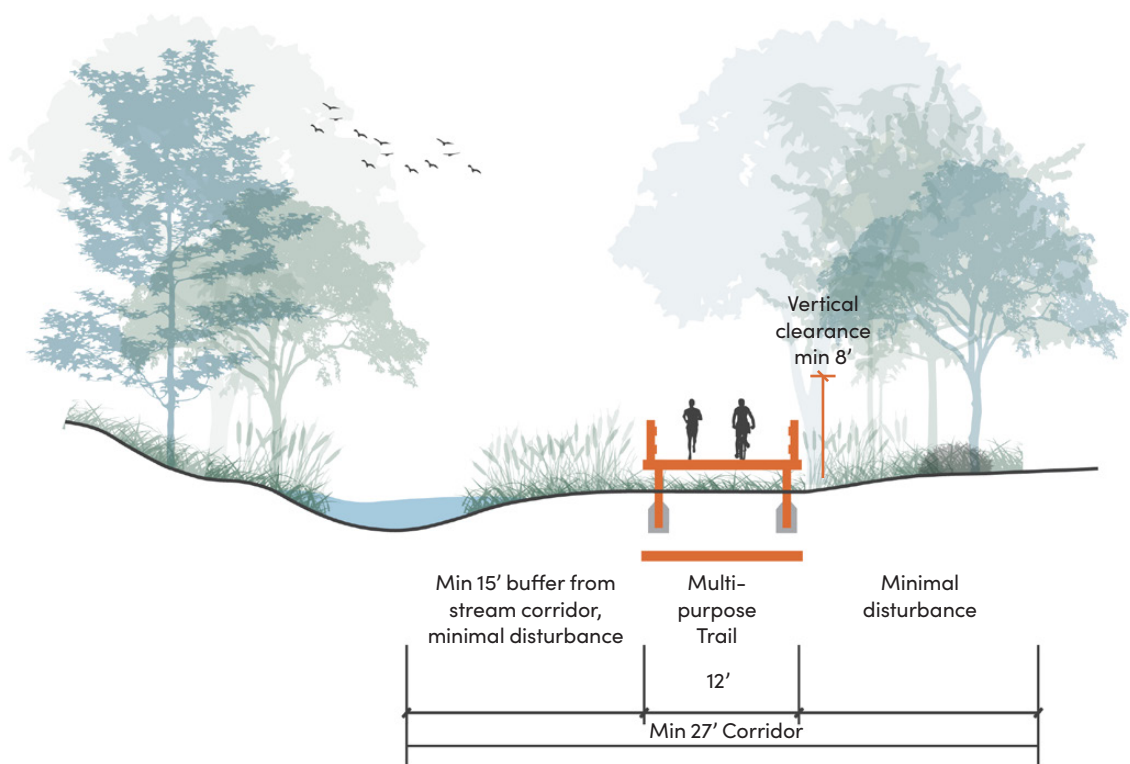
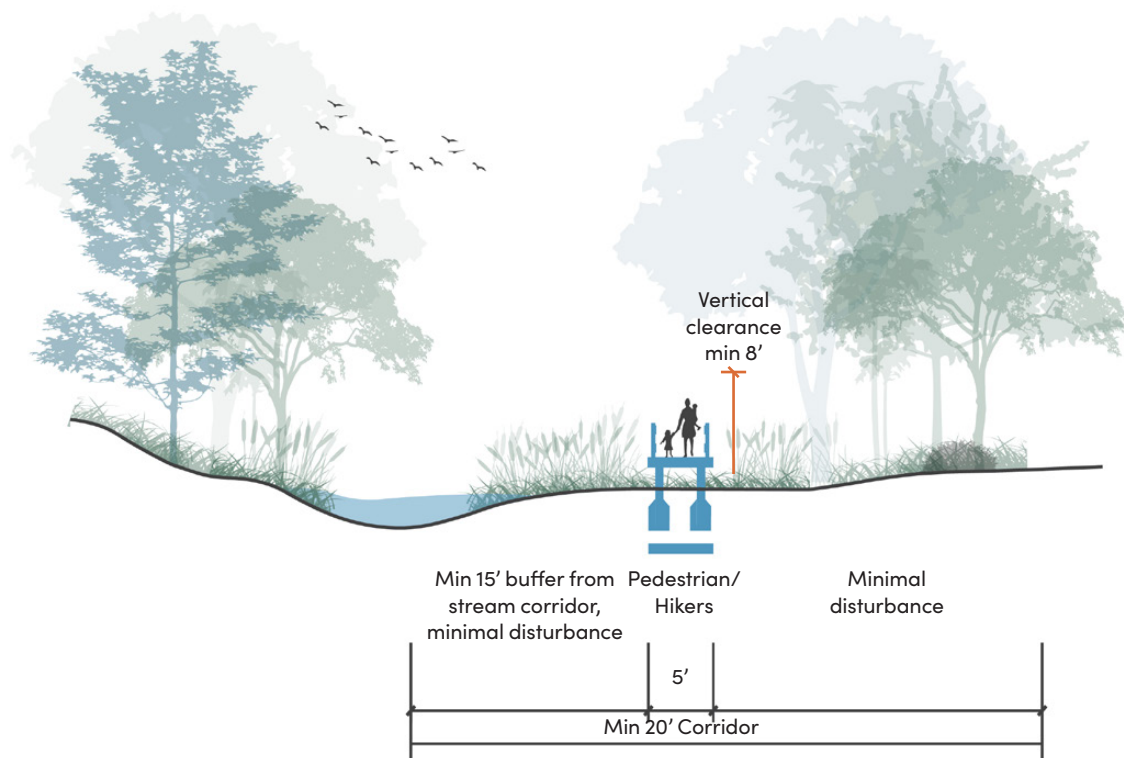


Table 12: Design Guidelines table

Users	Pedestrians and bicycle users
Contexts	Wetland Areas
Typical Tread Widths	12'
Materials	Wood, reinforced precast concrete
Maintenance	Every 15 years for wood, every 50 to 75 years for concrete
Slopes	
Buffers and Clearance	Min 8' vertical clearance, min 15' from stream corridors / top of bank
Amenities, Safety and Other Design Features	54" railing when surface is 30" above grade / 6" curb when surface is less than 30" above grade / Surface 12" above 10-year storm elevation



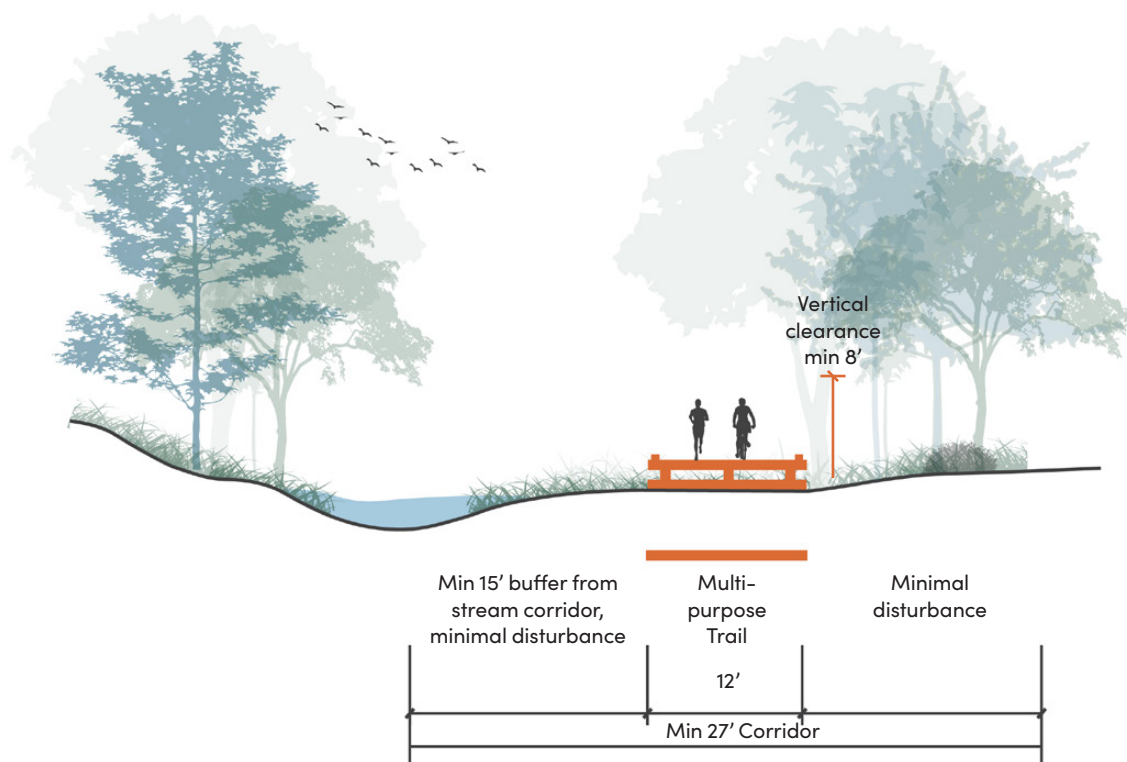
# WETLAND TRAILS



**Table 13:** Design Guidelines table

<b>Users</b>	Pedestrians
<b>Contexts</b>	Wetland Areas
<b>Typical Tread Widths</b>	5'
<b>Materials</b>	Wood, reinforced precast concrete
<b>Maintenance</b>	Every 15 years for wood, every 50 to 75 years for concrete
<b>Slopes</b>	
<b>Buffers and Clearance</b>	Min 8' vertical clearance, min xx' from stream corridors / top of bank
<b>Amenities, Safety and Other Design Features</b>	54" railing when surface is 30" above grade / 6" curb when surface is less than 30" above grade / Surface 12" above 10-year storm elevation

# SENSITIVE LANDS TRAILS - TYPE A



**Table 14:** Design Guidelines table

<b>Users</b>	Pedestrians and bicycle users
<b>Contexts</b>	Sensitive lands
<b>Typical Tread Widths</b>	12'
<b>Materials</b>	Wood
<b>Maintenance</b>	Every 15 years
<b>Slopes</b>	
<b>Buffers and Clearance</b>	Min 8' vertical clearance, min 15' from stream corridors / top of bank
<b>Amenities, Safety and Other Design Features</b>	6" curb at trail edges

# SENSITIVE LANDS TRAILS - TYPE B

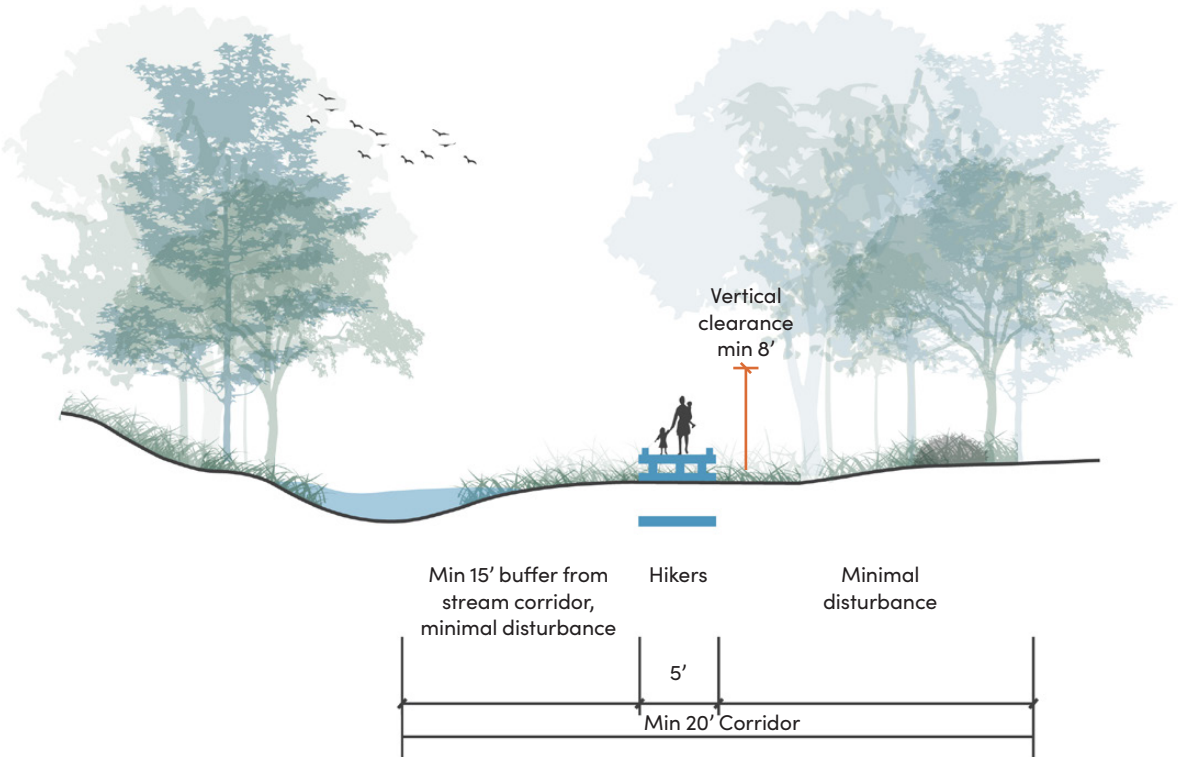


Table 15: Design Guidelines table

Users	Pedestrians
Contexts	Sensitive lands
Typical Tread Widths	5'
Materials	Wood
Maintenance	Every 15 years
Slopes	
Buffers and Clearance	Min 8' vertical clearance, min 15' from stream corridors / top of bank
Amenities, Safety and Other Design Features	6" curb at trail edges



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## BLUEWAYS

Blueways are designated routes along waterbodies that can be used by non-motorized watercraft such as kayaks, canoes and paddleboards. Due to Conway's frontage on the Waccamaw River and Blue Trail and extensive hydrological network, blueways are a natural fit for tourism, enjoyment of the outdoors and regional economic development.

Necessary components for a successful water trail are public access points at suitable distances, adequate parking at access areas and rest stops with sanitation facilities. Longer trips may require canoe-in campsites, drinking water, food and other supplies. Blueway planners can practice sensitivity to the concerns of property owners by understanding regional and federal water rights and limiting trespassing on private lands. The needs and safety of paddlers should be addressed at appropriate intervals along a water trail.

### TRAILHEADS

Access points for blueways should be clearly advertised and located on public land or on properties where public access has been negotiated. Blueway trailheads should include:

- » Parking
- » Signage
- » Canoe and kayak storage lockers or racks
- » Restrooms
- » Picnic tables
- » Wash stations to limit the spread of invasive aquatic species

Other put-in and take-out sites with fewer amenities should be located along the blueway for additional access and rest points. Some Conway residents who wish to use the public blueway system will not have access to a kayak, canoe or paddleboard. The City may consider

offering rentals of non-motorized watercraft, or partnering with local outfitters to offer rentals, and should clearly advertise blueway trailheads where free or low-cost rentals are available.

### BLUEWAY FACILITIES

- » Typical launches will be boat ramps with grades between 5% and 15%, located in areas protected from the wind, without underwater obstructions and in waters deep enough to be navigable by kayaks and canoes. Where feasible and environmentally sound, the City should offer ADA-accessible boat docks.
- » Rest stops should be located every two to three hours along a blueway.
- » Blueway signs should be located close to the shoreline and should extend 6' above the water.
- » Construction materials should be water- and salt-resistant and can include marine plywood, cedar, enameled aluminum or plastic.
- » In places where blueway users must carry their craft over land to avoid certain water conditions or hazards, those portage areas should be designated on public land or on land where public access has been negotiated.

### SAFETY CONSIDERATIONS

Conway should work closely with local paddle organizations and the state department of Natural Resources to develop blueway trail alignments that protect water quality, critical habitat and user safety. Water trail signage and online information should alert users to possible hazards. The City website and printed blueway materials and signage should direct blueway users to websites, such as PaddleSC, where they can find current water levels and other information that will help them prepare for their visit and determine whether conditions are appropriate for paddling.

# BLUEWAY / PADDLE TRAILS

## LAUNCH RAMP

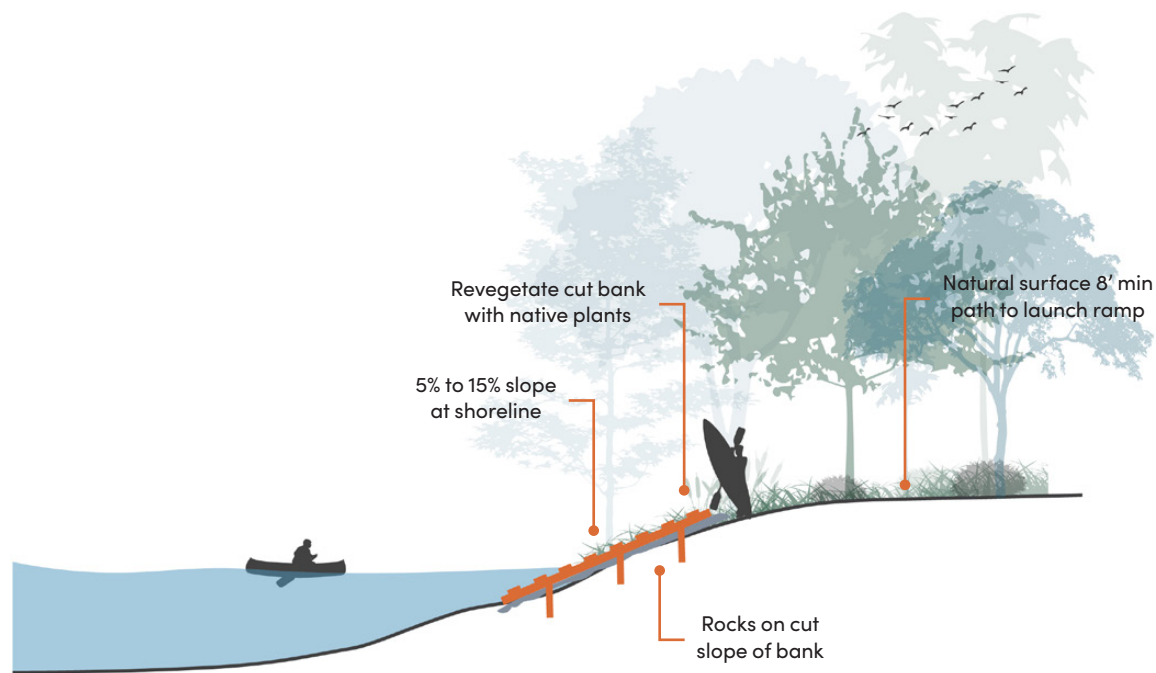


Table 16: Design Guidelines table

Users	Paddlers
Contexts	Waccamaw River
Typical Tread Widths	8'
Materials	Compacted aggregate (prohibited in floodplain), resin stabilized aggregate (prohibited in floodplain), natural soil surface, wood mulch, sand
Maintenance	Every 5 to 10 years
Slopes	Target 5% or less for ADA paths / Cross slope 2% max
Buffers and Clearance	Min 8' from roadways
Amenities, Safety and Other Design Features	Railing or fence 54" tall when adjacent to a drop-off of 1' or greater / Resting places every 200' for slopes less than 8.33%, every 30' for slopes 8.33% to 10%, and every 10' for slopes greater than 10%, 50' or less from parking or unloading areas





VANITE' BEAUTY SALON

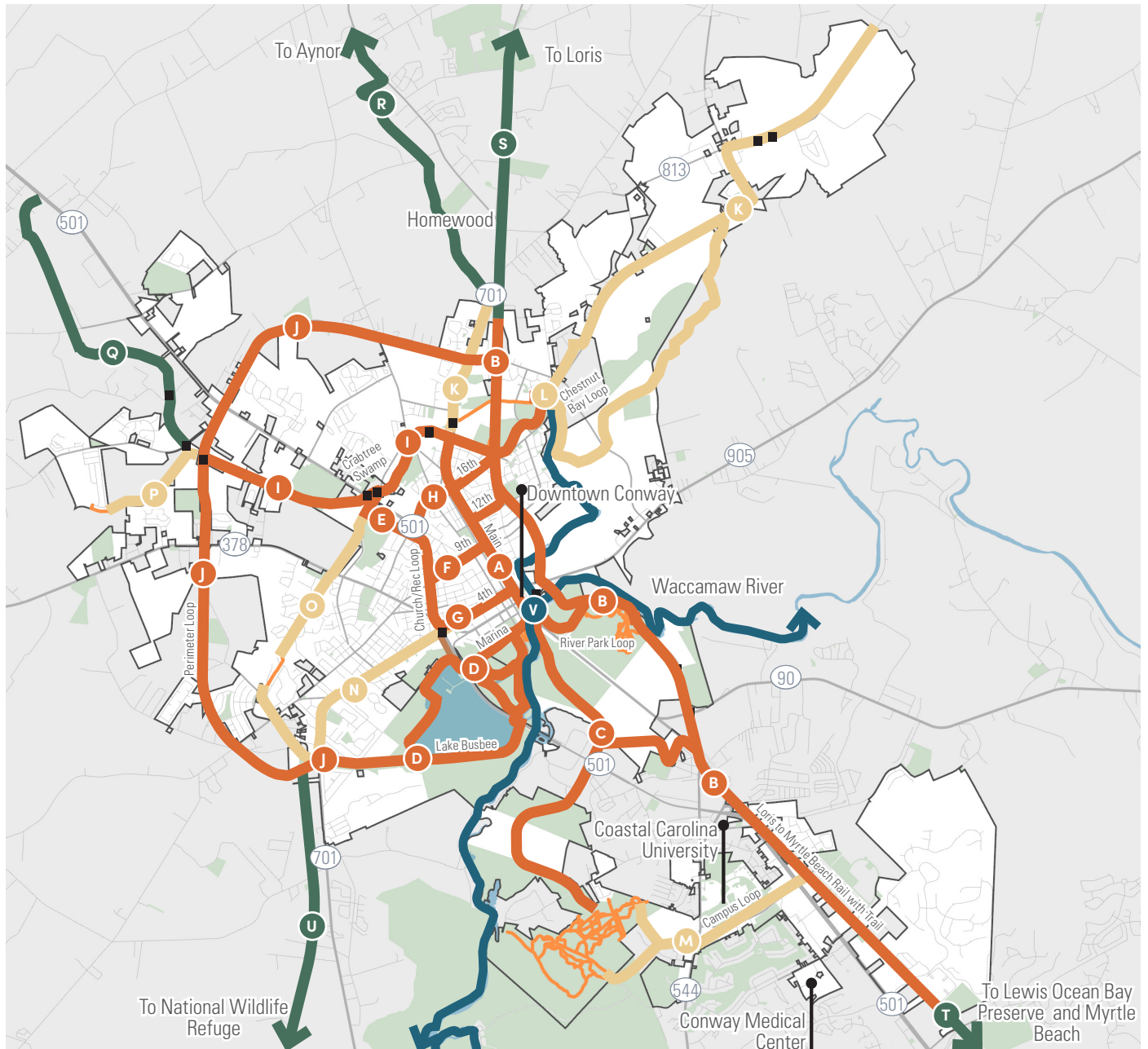
Gifts  
Wine  
Florals



# Implementation Plan

- » Project Prioritization and Phasing
- » Actions and Strategies

# PATHWAYS AND TRAILS PRIORITIZATION TIERS



## Project Prioritization

The Implementation Plan is an important resource to help Conway develop its financing, staffing and partnership approach for an expanded pathways and trails system. It is also an important tool for engaging the public, showing progress, and demonstrating the positive impact of an expanded system.

### LEGEND

- |                                    |                    |
|------------------------------------|--------------------|
| CITY OF CONWAY BOUNDARY            | TIER 1 TRAILS      |
| MAJOR ROADS                        | TIER 2 TRAILS      |
| ROADS                              | COUNTY-WIDE TRAILS |
| EXISTING TRAIL                     | BLUEWAYS           |
| WATER BODY                         |                    |
| PUBLIC LANDS AND UTILITY CORRIDORS |                    |
| PEDESTRIAN/BIKE WATER CROSSINGS    |                    |

**Table 1:** Range of High-Level Planning and Construction Costs for the Conway Pathways and Trails Plan

PATHWAY & TRAIL CORRIDOR		MILEAGE	RANGE OF PLANNING & CONSTRUCTION COSTS
TIER 1	<b>A Main St. to Crabtree Swamp Trail</b>	1.8 mi	\$954,000 - \$3.6 million
	<b>B Loris to Myrtle Beach Trail</b> (within City limits, with 12th to Main Connector)	8 mi	\$4.2 million - \$16 million
	<b>C River Park Loop West Spur and Cox Ferry Lake Connector</b>	3.6 mi	\$1.9 million - \$7.2 million
	<b>D Lake Busbee and Ash Pond 2 Circuit / Riverwalk Ext.</b>	6.2 mi	\$3.3 million - \$12.4 million
	<b>E Church St / Rec. Loop East Spur</b>	2.6 mi	\$1.4 million - \$5.2 million
	<b>F 9th Ave. / Boulevard Improvements</b> (Church to Main)	.7 mi	\$371,000 - \$1.4 million
	<b>G 4th Ave.</b> (Church to Main)	.7 mi	\$371,000 - \$1.4 million
	<b>H 16th Ave. / Collins Park Connector</b> (Church to Sherwood Park)	1.5 mi	\$795,000 - \$3 million
	<b>I Crabtree Swamp Trail</b>	3.6 mi	\$1.9 million - \$7.2 million
	<b>J Outer Belt -</b> (Perimeter Road Segment) (Cultra Road to Hwy 378 Segment) (Perimeter Road to Lake Segment)	2.8 mi 4 mi .9 mi	\$1.5 million - \$5.6 million \$2.1 million - \$8 million \$477,000 - \$1.8 million
		<b>36.4 mi</b>	<b>\$19.3 million - \$72.8 million</b>
TIER 2	<b>K Homewood Connector</b>	1 mi	\$530,000 - \$2 million
	<b>L Chestnut Bay Loop and Collins Jolly Spur</b>	9.1 mi	\$4.8 million - \$18.2 million
	<b>M Campus Loop Connector</b>	2.4 mi	\$1.3 million - \$4.8 million
	<b>N 4th Ave. to Outer Belt</b>	2.2 mi	\$1.2 million - \$4.4 million
	<b>O Rec Loop West Spur and Outer Belt Connector</b>	2.4 mi	\$1.3 million - \$4.8 million
	<b>P Dunn Short Cut Road to Crabtree Swamp Trail / Oakey Swamp</b>	.9 mi	\$477,000 - \$1.8 million
		<b>18 mi</b>	<b>\$9.5 million - \$36 million</b>
CITYWIDE TOTALS		<b>54.4 miles</b>	<b>\$28.8 million - \$108.8 million</b>
BLUEWAYS COUNTY-WIDE	<b>Q Crabtree Swamp to 501</b>	<b>3.3 mi</b>	<b>\$1.7 million - \$6.6 million</b>
	<b>R Homewood to Aynor</b>	<b>15+ mi</b>	<b>\$530,000 - \$1.2 million per mile</b>
	<b>T S Loris to Myrtle Beach Rail with Trail</b>	<b>18.5+ mi</b>	<b>\$530,000 - \$2 million per mile</b>
	<b>U Wildlife Refuge Trail</b> (Easement Trails)	<b>22.4+ mi</b>	<b>\$340,000 - \$530,000 per mile</b>
	<b>V Waccamaw River Crossing</b> (Bike/Ped Ferry with Landings or Pedestrian Bridge)	-	<b>\$1.3 million - \$3 million (ferry), \$5 million - \$7 million</b>



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## Project Prioritization and Phasing

The Pathways and Trails prioritization model offers clear immediate action items and a long-term blueprint for the City, its partners and developers to reference and coordinate efforts. The model also identifies short- and long-term funding and partnership recommendations. Cost estimates and design and maintenance guidelines will help the City and its partners establish clear expectations for initial and ongoing costs and maintenance needs. Project prioritization and phasing recommendations are driven by community priorities and grounded in physical and economic realities.

The Principles, Objectives and Strategies of the Plan informed the distribution of pathway and trail projects into tiers, with Tier 1 being the highest priority for short-term projects that promote equity in the system while building important connections. Table 1 lists the projects by tier. The higher the project's place in the table, the higher it scored according to the system goals of equity, connectivity and providing public interest.

**While all trail corridors radiate from the heart of the City and are looped where possible to maximize connectivity, higher tier projects prioritize trail corridors which fall within the City's jurisdiction, corridors or lands that are already slated for transportation or park-related improvements, and signature corridor projects with potential to create foundational opportunities for mobility, tourism and public support for pathways and trails.**

County-wide trails and Blueways are not included in the Level of Service during project prioritization, and should be studied as potential adjacent and collaborative opportunities to the projects in Tiers 1 and 2. Each project is shown with its estimated range of planning and construction costs. For planning

and budgeting purposes, the plan recommends that the City begin to pursue detailed planning and construction of the entire pathway and trail system, with the following qualifications.

- » The tiered approach allows the City and its partners to focus their proactive design, public engagement, land acquisition, grant writing and fundraising efforts on projects that best reflect public priorities and are therefore best positioned to act as demonstration projects for the entire system. Investment in high-quality pathways, trails and amenities within higher-tier corridors should be first priority.
- » However, progress can and should be made within all corridors, and opportunities taken for closing short-term gaps through public right-of-way, land leases and other measures.
- » In close coordination with partners and trail user groups, the City should take advantage of opportunities for phased implementation through the short-term construction of primitive trails and loops. These projects can be implemented by volunteers, building community support and capacity while demonstrating incremental progress toward meeting the City's unmet pathway and trail needs. These projects would be particularly appropriate within lower-tier corridors.

The plan recommends a staggered budgeting approach whereby higher-tier projects are budgeted at a higher per-mile cost and lower-tier projects at a lower per-mile cost. The result will be an emphasis on funding high-quality pathways, trails and blueways within those corridors that are most important for building a system that serves residents equitably, while ensuring progress is made on the trail projects that are lower-priority but still critical for realizing city and county-wide connectivity.

In two public surveys administered as part of the engagement process for this plan,

the majority of participants expressed support for investment in this system, and willingness to promote securing the land, funding and partnerships to make it a reality.

## COST SCENARIOS FOR PLANNING AND CONSTRUCTION

The Plan identifies corridors within which the City and its partners will pursue the detailed alignment, design and construction of pathway, trail and blueway projects. The cost of those projects within the Plan corridors will depend on a range of factors, including trail type, site conditions or uncertainty, and the nature and extent of project amenities such as signage, pavilions, parking lots and nested trail loops.

This Plan provides a range of possible costs to help the City establish funding expectations and priorities for design and build-out of the system. High-level costs are calculated per mile of each corridor and account for the 2022 inflation rate.

- » This Plan prices pathway and trail corridor buildout at **\$530,000 to \$2,000,000 per mile**.
- » The cost ranges are based on and checked against planning and construction figures from:
  - *Conway Comprehensive Transportation Plan*, 2008
  - *East Coast Greenway Alliance Design Guide*, 2021
  - Case studies involving built multi-use trails and pedestrian bridge crossings in the United States in the years between 2015–2021.
  - *Maintenance Practices and Costs of Rail-Trails*. Rails-to-Trails Conservancy, 2015
  - Costs verified by *The City of Conway Greenway Master Plan*, 2005
- » Dollar ranges do not include staffing costs; potential acquisition, permitting

or environmental mitigation costs; or ongoing maintenance.

- » The corridor cost figures do not account for scenarios in which developers or other regional partners share trail planning and construction costs.
- » Waterbody crossings (bridge retrofits, pedestrians bridges, etc) are factored into Table 1 and are not inclusive of potential costs that may be incurred for professionally modelled "No-rise" flood and hydrological impact studies. The need for these studies should be determined on a case-by-case basis.

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## Action Plan

The City should prioritize the following action items to maintain the momentum of the pathway and trail planning process, work efficiently toward buildout of the Pathways and Trails Plan, and position itself to take advantage of funding, acquisition and partnership opportunities when they arise. More detailed information about funding, real estate, policy and partnership strategies follow.

## FUNDING

1. Coordinate across City departments to develop a funding strategy that draws from a diversity of municipal, state, federal and private funding streams to meet the capital and operational needs of the pathway and trail system buildout.
2. Prioritize short-term funding for items that will position the City to leverage other funds—such as additional staffing to support grant and volunteer coordination; planning, design and approval processes to ensure high-priority projects are shovel-ready; and matching funds that will be required for short-term grant pursuits.

## STAFFING

1. Hire a pathway and trail manager to oversee grant pursuits, internal and external committees, volunteer efforts and overall coordination of implementation efforts.
2. Hire maintenance staff as needed to coordinate and supplement the pathway and trail construction and maintenance efforts of volunteer groups.

## REAL ESTATE

3. Continue to pursue land donations, easements and the construction of public pathway and trail facilities through future development, with the Pathways and Trails Plan as a guiding document for the City and developers.
4. Build relationships with institutional partners that are well-positioned to help the City reach out to private landowners and negotiate public trail access—including non-profit easement holders, homeowners associations and utilities.
5. Develop a flexible toolkit for acquiring public trail access on private land—including fee simple acquisitions, purchase options, easements and leases. Develop marketing materials to help the City communicate to private landowners the implications and benefits of various approaches to providing public trail access.

## POLICY

6. As part of the City's next Unified Development Ordinance update, incorporate strengthened language

for contiguous open space requirements, cluster developments, public pathway and trail requirements, and the establishment of impact fees to fund pathway and trail system buildout and meet the service demands of a growing population.

7. Pursue establishment of Purchase of Development Rights programs to secure open space protection and pathway and trail corridors within those protected lands.

## MARKETING

8. Hire a consultant to develop branding guidelines for the pathway and trail system signage and print and online marketing materials. The consultant should work with trail partners and stakeholders to develop a clear and coordinated marketing and communications strategy that promotes pathway and trail awareness, public support, and high-quality user experiences.
9. Pursue development of an online hub and facility tracking software to provide up-to-date public information about the state of the City's pathways, trails and blueways while informing the ongoing efforts of City and volunteer maintenance teams.

## PARTNERSHIPS

10. Continue to build partnerships with other local and regional governments, commissions to ensure regional connectivity and to take advantage of opportunities to share costs of system implementation and maintenance.



**11.** Continue to build partnerships with local and regional pathway and trail non-profits and foundations to take advantage of opportunities for collaboration through planning, grants, facility construction, maintenance, education and advocacy.

## **PATHWAYS & TRAILS FOUNDATION**

**12.** Pursue establishment of a Conway Pathways and Trails Foundation to support implementation and awareness of the pathway and trail system and other City recreational facilities through fundraising, advocacy and volunteer coordination.

community support and a vote of approval from residents. If approved, bonds can secure the long-term financing necessary for the purchase of larger properties, many of which rely on purchase payments being made over several years.

In the case of future pathway and trail development, Conway will want to pursue the issuance of General Obligation (GO) Bonds. A GO bond is a type of municipal bond "backed solely by the credit and taxing power of the issuing jurisdiction rather than the revenue from a given project." Any purchase strategy involving GO bonds should be tied to a larger public engagement campaign that builds community support for leveraging tax revenue for pathway and trail development.

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## **Funding Strategies**

### **BUDGET ALLOCATIONS THROUGH THE CITY'S GENERAL AND HOSPITALITY TAX FUNDS**

Conway should allocate funds from the City's General and Hospitality Tax Funds for pathway and trail projects. These funds may be used for the purchase of land in fee simple transactions, trail upkeep and maintenance, and/or capital projects to improve the pathway and trail system.

### **GENERAL OBLIGATION (GO) BONDS**

Debt instruments such as bonding have proven to be a valuable tool for large expenditures, such as land needed for the development of parks, trails and open space. A bond is a type of fixed income instrument that is generally issued by a corporation or government to finance large-scale projects or initiatives. Local municipalities generally have the authority to issue bonds, though if the bond is funded through tax revenue, the municipality likely requires strong

### **FEDERAL, STATE OR PRIVATELY FUNDED GRANT PROGRAMS**

Conway should explore a variety of grant funding options. Grants for parks, open space and trail conservation/development are funded through public or private entities and are often tied to specific development or programming initiatives. Grants generally have a formal application process that requires specific submission requirements from all applicants. Additionally, grant programs often involve a reporting requirement that looks to quantify the grant's impact. Conway should engage in a yearly review process of all relevant private and public funding and grant opportunities, including:

- » Community resilience grants from National Fish and Wildlife Foundation (NFWF), Federal Emergency Management Agency (FEMA), and National Oceanic and Atmospheric Administration (NOAA),
- » Transportation grants from SC Transportation Alternative Program (TAP), TIGER (Transportation Investment Generating Economic Recovery Discretionary Grant), CMAQ

(Congestion Mitigation and Air Quality Improvement Program), FTA (Federal Transit Administration Capital Funds), HSIP (Highway Safety Improvement Program), FLP (Federal Lands Programs), Transportation Alternatives Set-Aside (formerly Transportation Alternatives Program), and SCDOT (SC Department of Transportation)

- » Open space and recreation grants from the Open Space Institute, Land and Water Conservation Fund (LWCF) and the Environmental Protection Agency (EPA)
- » and the South Carolina Department of Parks, Recreation and Tourism (SCPRT), which administers several grant programs for agriculture, land preservation, open space access and historic preservation.

decision-making about the best ways to address visitor access, agricultural management, ecological restoration and other management issues.

The buyer, or party that holds the fee title, can take the form of a variety of entities including a private individual, public agency, or a non-profit organization. A fee simple transaction can also take the form of a bargain sale, wherein land is sold for below market value with the seller (landowner) donating a portion of the property's value to the buyer. A bargain sale is particularly beneficial to landowners because they can receive payment for the land while deducting the value of the gifted land from their federal income taxes.

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## Real Estate Strategies

Acquiring access to the land necessary to develop a pathways and trails network is a challenge. The mix of public and private ownership along the Plan's corridors will require the City to have multiple tools and strategies at its disposal to best work with landowners to acquire ownership or access to land for public use. The following list of tools is meant to illustrate and inform the multiple pathways and strategies available to Conway as it works to acquire ownership of, or access to, the land necessary to develop its pathways and trails network.

### FEE SIMPLE ACQUISITION

A fee simple acquisition or purchase of land transfers full ownership of the property, including the underlying title from a landowner (seller) to a purchaser (buyer). Once a landowner grants the sale of land, the landowner generally retains no ownership of the property and gives up all rights. This allows the buyer to have the advantage of full control over the management of the property's resources and provides the greatest flexibility for

### EASEMENT

Easements are perhaps the most widely used tool for land conservation and public access. According to the Cornell Law School's Legal Information Institute, an easement is "the grant of a nonpossessory property interest that grants the easement holder permission to use another person's land." Easements can take a variety of forms and be used for a wide range of purposes including, but not limited to, granting a public or private utility access to a property, public or private road development, land conservation, or development restriction. Open space easements or conservation easements would be the most appropriate type of easement for the purpose of pathway and trail development in Conway.

Easements typically take the form of either an appurtenant easement or a gross easement. An appurtenant easement runs with the land and benefits a property rather than an individual or entity. Access easements, open space easements and conservation easements are typical examples of appurtenant easements. In contrast, a gross easement does not benefit a parcel of land, but rather an

individual or entity. Utility easements are a typical example of a gross easement, as the power company benefits from the agreement, but the land does not.

The South Carolina Conservation Easement Act of 1991 defines an open space easement as “a nonpossessory interest of a holder in real property imposing limitations or affirmative obligations, the purposes of which include one or more of the following: (a) retaining or protecting natural, scenic, or open-space aspects of real property; (b) ensuring the availability of real property for agricultural, forest, recreational, educational, or open-space use; (c) protecting natural resources; (d) maintaining or enhancing air or water quality; (e) preserving the historical, architectural, archaeological, or cultural aspects of real property.”

## LAND LEASE

A land lease, also known as a ground lease, is a type of commercial real estate lease in which a lessee is permitted to occupy, use and/or build on the rented land. Land leases can be used for a variety of situations, including commercial real estate development, property occupation by a homeowner with a mobile home, farming, or hunting and recreation. The terms of lease vary by situation, but generally establish the rights of both the landlord and tenants, the duration of the lease, use provisions, and which party owns any improvements to the property made during the lease period. Under a land lease scenario, tenants generally assume responsibility for all land-related expenses, including construction, repairs, renovations, improvements, taxes, insurance, and any financing costs associated with the property.

There are two common types of land leases, subordinated and unsubordinated. In a subordinated land lease, according

to Investopedia.com, “the landlord agrees to a lower priority of claims on the property in case the tenant defaults on the loan for improvements.” This means that the landlord allows the land itself to act as collateral toward debt financed improvements made by the tenant. In the case of default by the tenant, the sale of the land would be used to pay off the debt of the tenant. While riskier, subordinated land leases generally allow landowners to charge a higher rental rate for their property. An unsubordinated land lease, on the other hand, “lets the landlord retain the top priority of claims on the property in case the tenant defaults on the loan for improvements.” Because of the lack of collateral, lenders may be hesitant to offer financing to projects with an unsubordinated land lease. Additionally, the owner of the land may have to charge a lower rent for an unsubordinated land lease.

## PROPERTIES WITH REPETITIVE LOSS DUE TO FLOODING

Lands acquired by the City through the Federal Emergency Management Agency (FEMA) Flood Mitigation Assistance Program and other Federal Buyout programs can provide multiple benefits to the community, including increased safety and resilience during extreme weather events, and connections to the natural environment.

Due to development and use restrictions on buyout properties, Conway can leverage these lands to restore and conserve natural open space, facilitating resilience against future floods and providing potential pathway and trail connectivity for the enjoyment of the public.



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## Development Policies

The following tools should be considered as part of future updates to the City's Unified Development Ordinance.

### OPEN SPACE AND FACILITY REQUIREMENTS

There are several tools that allow Conway to offset the impact of new development by requiring certain levels of open space protection and investments in new public facilities. These requirements are spelled out in the City's Unified Development Ordinance.

Open space requirements acknowledge the importance of preserving significant tracts of open space to offset the environmental impacts of new development. The tracts of open space protected under these requirements should be contiguous and a minimum width of 300' wherever possible in order to accommodate meaningful habitat protection along with the provision of new public pathway or trail access. The Pathways and Trails Plan can help the City work with private developers to identify the ideal location of protected open space and trail alignments on any given parcel, such that a connected system is built out over time.

The Conway Unified Development Ordinance calls for minimum open space requirements in Institutional Campus Districts, and also requires new development to provide publicly accessible open space. As they are with active transportation facilities called out on the City's Comprehensive Transportation Plan, development plans should be required to include the public pathway and trail facilities and public access easements for corridors called out in the Pathways and Trails Plan.

The City could seek authority from the state legislature to establish residential development impact fees, whereby

developers provide a cash contribution to fund infrastructure or public facility improvements that offset the identified impact of the new development. This impact fee would apply even in areas of the City where rezonings are not anticipated or restricted. The City should use this additional measure to fund build-out of the pathway and trail system and meet the service demands of a growing population.

The City could also incentivize the use of cluster, or conservation, developments, whereby new development is clustered at a higher density while large areas of open space—typically at least 30% of the site and located to protect sensitive or high-quality environments—are set aside for permanent protection. Public pathway and trail facilities should be required or incentivized within the resulting open space corridors.

### PURCHASE OF DEVELOPMENT RIGHTS

Purchase of Development Rights (PDR) programs are similar to Transfer of Development Rights (TDR) programs; however they do not create a marketplace to facilitate the transfer of development rights. According to Wetlands Watch, public bodies can use PDR programs to “preserve open space by purchasing future development rights in ecologically valuable areas and place the land in a conservation easement to preserve as open space. Property owners retain use of the land and the easements and land-use restrictions convey if the property is sold.”

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## Partnerships

To pursue the strong public support, funding and land acquisition necessary for buildout of the pathway and trail system, Conway should leverage strategic partnerships with both public and private organizations. Examples of potential partners include the following entities.

## PUBLIC/PRIVATE UTILITIES

Coordinating trail development with public and private utility companies can be both an efficient use of space and a cost-effective way to develop a trail network. According to Rails-to-Trails Conservancy, utilities such as “water, sewer, natural gas, electric and fiber optic, can have their lines buried or encased near or beneath a trail, while telecommunications, cable and electric utilities can run above a corridor using air rights.” This presents an opportunity to develop surface level trails without disrupting or inhibiting the utility.

In most cases, utility providers enter into a utility easement agreement with private landowners to gain access to their property. In cases where there is an established utility easement in place on a property, Conway should engage the landowner to request an additional access easement in exchange for covering costs associated with improvements and maintenance. In addition to the landowner, the City should coordinate with the utility company holding the easement to ensure that trail development will not negatively impact or prohibit the utility.

To improve the likelihood of obtaining an access easement within a utility corridor, Conway should proactively engage area utility providers to better understand the planned utility projects that require the acquisition of a utility easement from a landowner. Once identified, the City should approach the landowner in tandem with the utility company to increase the probability of successfully obtaining the access easement while coordinating and sharing improvements to the easement area with the utility provider.

## HOMEOWNERS ASSOCIATIONS

Homeowners associations (HOAs) often own significant and often contiguous tracts of open space that surround and weave through their respective

communities. These HOA lands are typically unbuildable and in riparian or drainage corridors – set aside as open space or improved with recreational facilities, including trail systems.

Conway should continue to seek partnerships with HOAs in order to:

- » Allow for public access to existing trail amenities on private HOA land, or
- » Allow for the construction and/or maintenance of public trail facilities on private HOA land, or
- » Convince HOAs to build new trail facilities, even if restricted only to residents, in cases where those facilities would provide those community members with off-road access to the public pathway and trail system.

The City should also work to communicate to its HOA partners the benefits of having a public trail corridor in neighborhood open space – in terms of resident health and wellbeing, easy access to an interconnected Citywide system of pathways and trails, and the demonstrated impact of trails in increasing neighboring property values.

## DEDICATED FOUNDATIONS, NON-PROFITS, AND LAND TRUSTS

Conway should build and leverage relationships with area non-governmental organizations (NGOs), such as foundations, nonprofits and land trusts. NGOs can be valuable partners in the development of a pathway and trail network by advocating for the system, helping to facilitate planning and design efforts, coordinating volunteer trail maintenance, and educating the public about the benefits of pathways and trails.

NGOs can also hold and help to negotiate lease agreements, easements and fee simple purchases, and are able to function as a diplomatic, intermediary party between the City and private landowners. NGOs are especially useful when

negotiating with landowners who are willing to provide access to their property but may not be interested in working with a government entity.

NGOs may also be beneficial in scenarios where a landowner is interested in entering into a short-term lease agreement to gauge the impacts of trail use on their property. In these cases, the NGO can hold the lease agreement while developing a relationship with the landowner.

Conway should continue to build its relationships with local and regional land trusts — including South Carolina Land Trust Network, Pee Dee Land Trust, The Nature Conservancy, North American Land Trust, Coastal Conservation League, Land and Water Conservation Fund, American Rivers, Waccamaw Riverkeepers, and Winyah Rivers Alliance. These organizations hold conservation easements and work closely with the private landowners who choose to place their property under an easement. They are important partners in educating landowners about the potential benefits of allowing for public trail access within the language of their parcel-specific conservation easement.

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## A Pathways & Trails Foundation

In addition to working closely with non-profit foundations with allied missions—such as protecting open space or providing outdoor recreation opportunities—parks departments can advocate for the creation of non-profit foundations as private, standalone entities with an explicit mission to support public trails.

A pathways and trails foundation can help to supplement limited tax dollars through grants, private fundraising, volunteer coordination, marketing efforts, advocacy and strategic tools. Since foundations are

private non-profits, they are able to act outside the traditional scope of a public agency.

National Recreation and Park Association conducted a survey of both park and recreation agencies and nonprofit foundations to explore the keys to successful leadership of these organizations. They noticed that foundations were very helpful in philanthropic and fundraising support but less important for day-to-day activities.

### Some of the keys to effective foundations were:

- » **Relationships between organizations:** This study found that clarity and transparency between agencies and foundations is essential to an effective relationship. A key takeaway was that trust, collaboration, and clearly defined roles were essential to the success of the partnership. There must also be trust between the community and both the agency and foundation for effective results.
- » **Soft and technical skills:** Skills that agencies value in their foundation partners include leadership and decision making, professionalism, interpersonal skills, communications, goal setting, and conflict resolution. They also value financial resource management, business acumen, marketing, and political and legislative acumen.
- » **Fundraising and advocacy capacity:** The study found that 73% of respondents viewed fundraising or philanthropic support as “extremely or very important.” The ability of foundations to advocate for the parks and recreation department was crucial as many agencies are not able to lobby. As foundations are non-government entities, they possess greater capacity to fundraise than their governmental counterparts.

### Some of the challenges in agency-foundation relationships were:



- » **Equity:** Foundation support may not reach areas that need it most because fundraising may be focused on more affluent areas.
- » **Organizational structure:** Sometimes there may be competition between organizations for volunteers and board members. Both organizations must coordinate and plan on organizational structure.
- » **Unproductive interpersonal relationships:** Scarce financial resources may be a challenge if there is a lack of communication on who is reaching out to donors, causing more than one solicitation to each donor.
- » **Public advocacy:** This study showed that only a quarter of survey respondents found their foundations to be extremely useful on this front. Clearly communicating efforts between organizations is very important to foundations' ability to effectively advocate. Challenges may also occur from "lack of stakeholder understanding, buy in, and awareness."

The City of Conway may consider holding additional focus group conversations with the City of Greenville and the Greater Greenville Parks Foundation in order to gain understanding of how to create a Conway-specific approach to operations of future pathways and trails.

The Prisma Swamp Rabbit Trail in Greenville, SC, is a common precedent for trail operations and planning in the Southeastern U.S., as well as a local case study used throughout the Conway Pathways and Trails Plan. The Greater Greenville Parks Foundation is a 501 – C – 3 non-profit organization formed to facilitate public support and fundraising, with in-kind support by Greenville's Parks, Recreation and Tourism staff.

The Foundation has found success through approvals of distribution of the City's hospitality and accommodation taxes for trail expansion, as well as managing a donor system which includes support from large regional institutions such as Prisma Health. To encourage other public partners and recognize support, the Foundation runs a program where a donor can "Own" a 25 Foot Section of the Trail at \$25 per foot or trail.







# Appendices

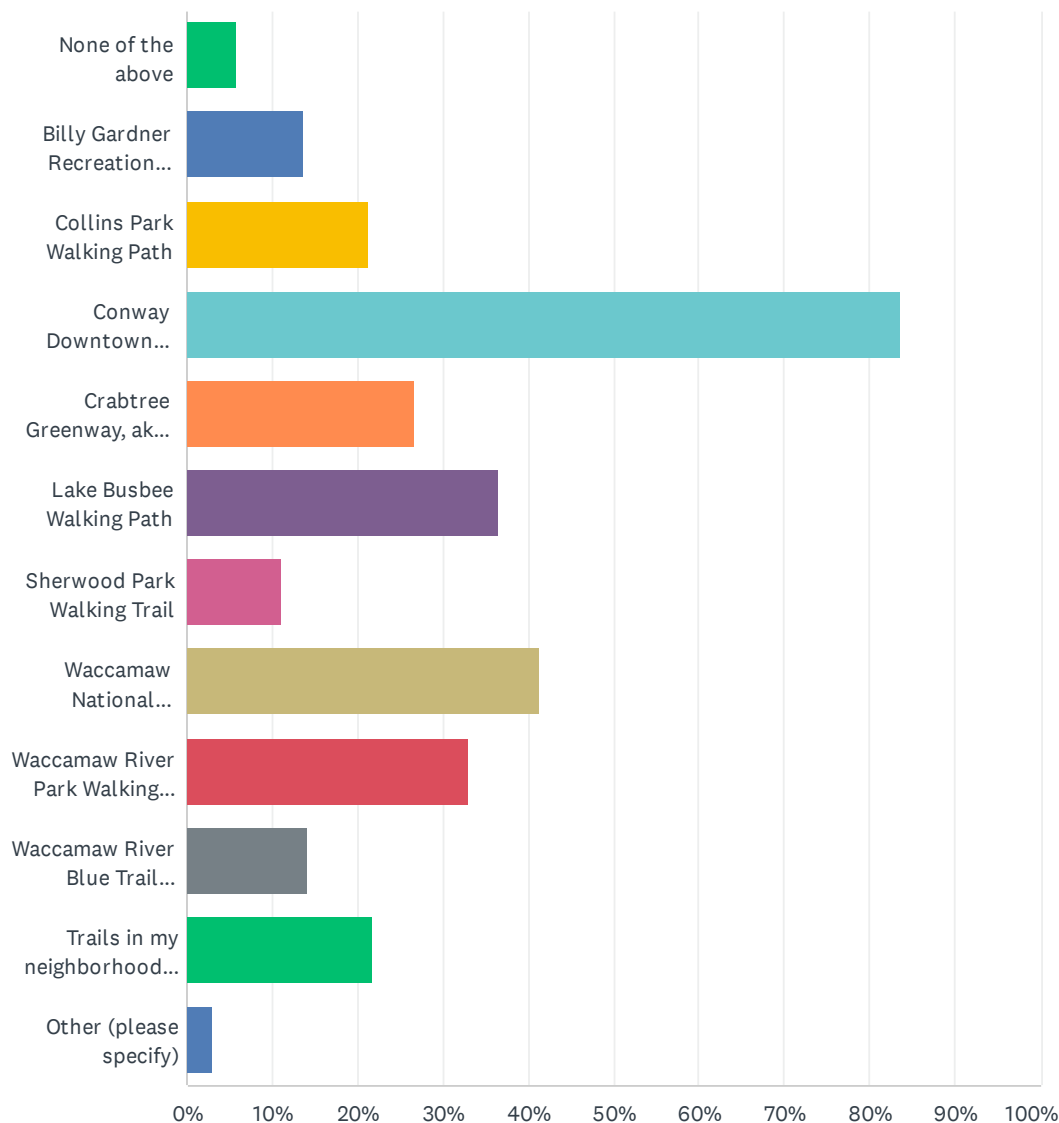


# SURVEY 1 RESULTS

## Conway Pathways and Trails Plan - Survey No. 1

Have you or your household used any of the following trails in Conway during the past 12 months? (Check all that apply.)

Answered: 206    Skipped: 4



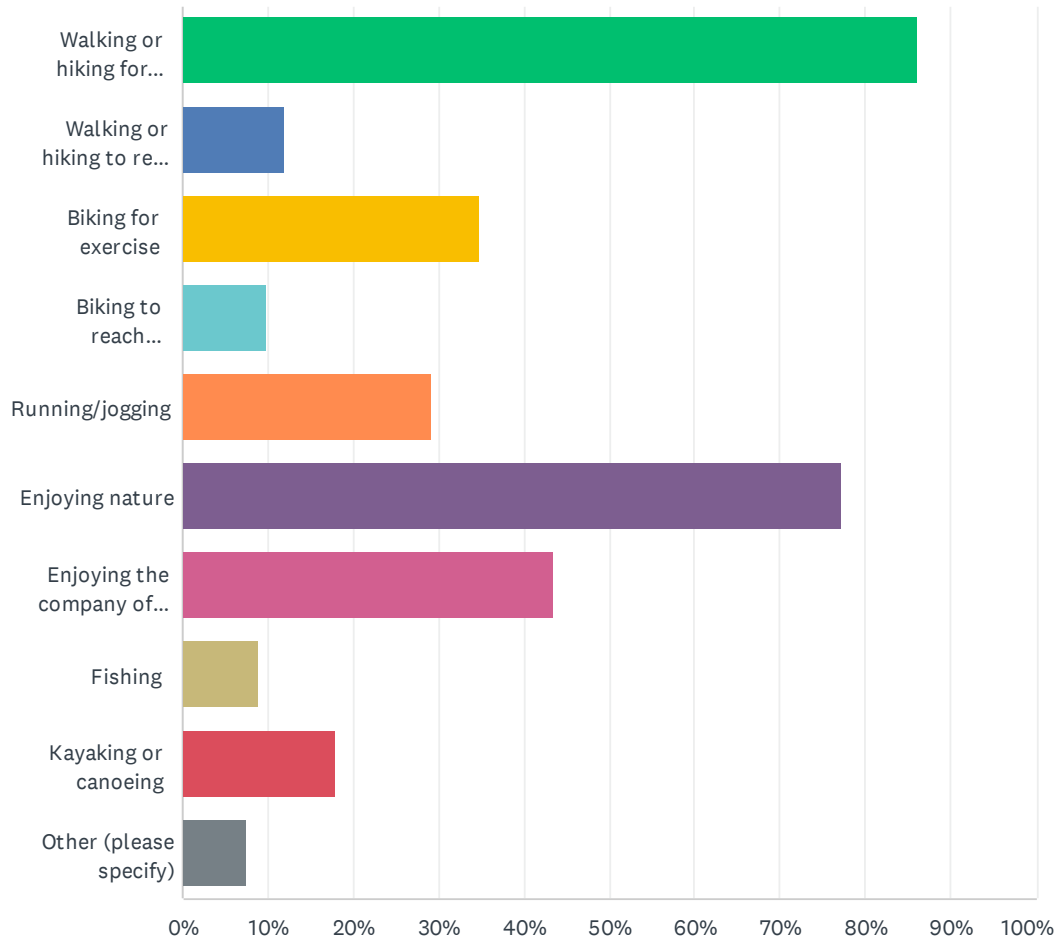
# Conway Pathways and Trails Plan - Survey No. 1

ANSWER CHOICES	RESPONSES	
None of the above	5.83%	12
Billy Gardner Recreation Complex Walking Trail	13.59%	28
Collins Park Walking Path	21.36%	44
Conway Downtown Riverwalk	83.50%	172
Crabtree Greenway, aka Crabtree Swamp Walking Trail	26.70%	55
Lake Busbee Walking Path	36.41%	75
Sherwood Park Walking Trail	11.17%	23
Waccamaw National Wildlife Refuge Trails	41.26%	85
Waccamaw River Park Walking Trails	33.01%	68
Waccamaw River Blue Trail (paddling trail on the river)	14.08%	29
Trails in my neighborhood or HOA	21.84%	45
Other (please specify)	2.91%	6
Total Respondents: 206		

#	OTHER (PLEASE SPECIFY)	DATE
1	The hulk, trail by market common	4/21/2022 10:18 PM
2	bike route roads	3/1/2022 12:09 AM
3	HULK MTB trails	2/24/2022 2:03 PM
4	Coastal Carolina campus	2/23/2022 2:22 PM
5	Cox Ferry Lake Trails	2/16/2022 11:06 AM
6	I tried to walk along Singleton Ridge Road but it is too dangerous	2/15/2022 7:23 PM

## Q2 How have you or your household used those trail(s)? (Check all that apply.)

Answered: 202 Skipped: 8





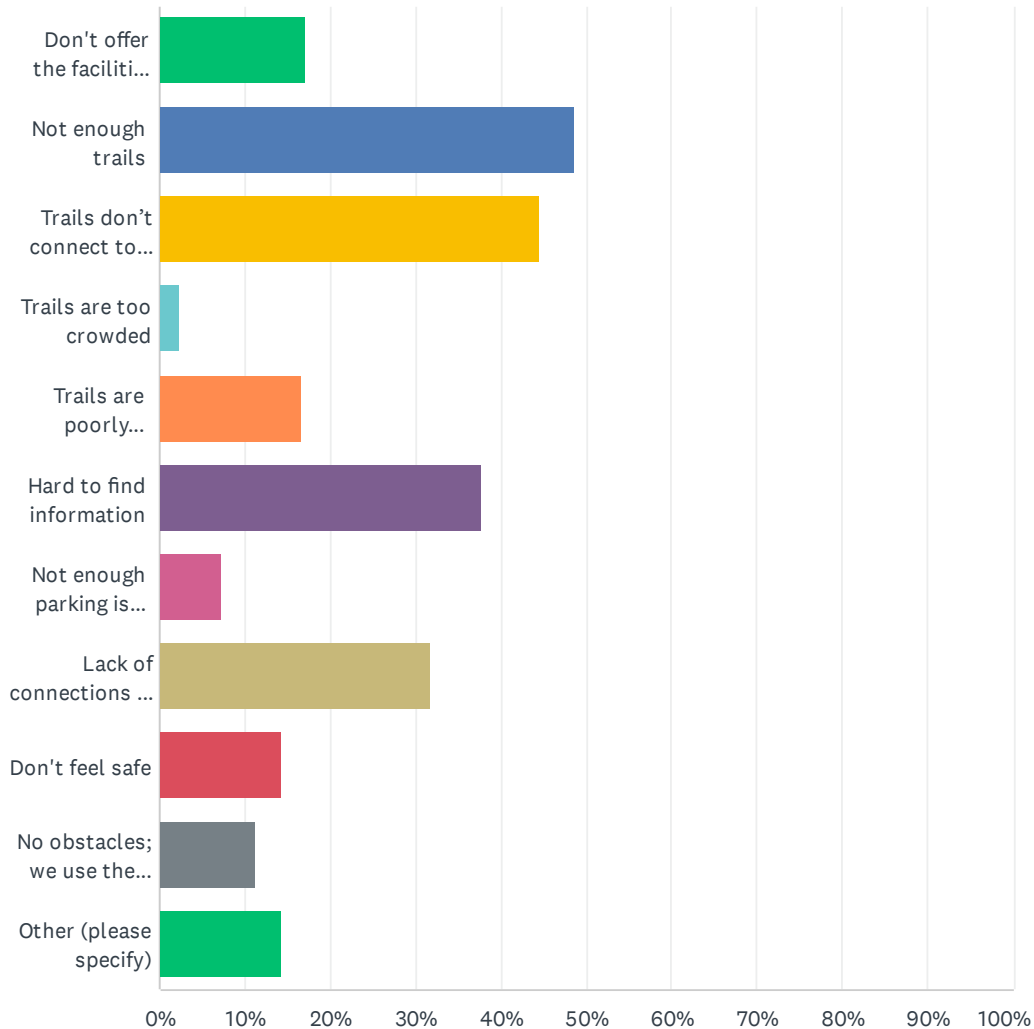
# Conway Pathways and Trails Plan - Survey No. 1

ANSWER CHOICES	RESPONSES	
Walking or hiking for exercise	86.14%	174
Walking or hiking to reach destinations	11.88%	24
Biking for exercise	34.65%	70
Biking to reach destinations	9.90%	20
Running/jogging	29.21%	59
Enjoying nature	77.23%	156
Enjoying the company of others	43.56%	88
Fishing	8.91%	18
Kayaking or canoeing	17.82%	36
Other (please specify)	7.43%	15
Total Respondents: 202		

#	OTHER (PLEASE SPECIFY)	DATE
1	Child-based activity for Family	4/25/2022 12:44 PM
2	Forest bathing	3/8/2022 10:04 AM
3	Frisbee golf	3/7/2022 3:14 PM
4	Fun activities with my young kids	3/7/2022 12:16 PM
5	Mountain Biking	2/25/2022 1:10 AM
6	I have not been on any of those trails, but I primarily ride trails for exercise	2/24/2022 2:31 PM
7	biking for fun and relaxation	2/22/2022 2:56 PM
8	Walking to playground	2/21/2022 9:57 AM
9	Paddleboarding	2/19/2022 7:28 PM
10	would use paths along 701 to get around - need badly	2/18/2022 9:42 AM
11	Business	2/17/2022 11:28 PM
12	singleton Ridge Road should have a walkway for pedestrians	2/15/2022 7:23 PM
13	Current trails and bike ways do not support road bikes, they are unpaved	2/15/2022 7:17 PM
14	Walking dog	2/14/2022 6:23 PM
15	Did not know about them	2/14/2022 3:16 PM

### Q3 Please check any of the below obstacles that prevent you or your household from using Conway trails more often:

Answered: 204 Skipped: 6



# Conway Pathways and Trails Plan - Survey No. 1

ANSWER CHOICES	RESPONSES	
Don't offer the facilities or types of trails we want	17.16%	35
Not enough trails	48.53%	99
Trails don't connect to where I want to go	44.61%	91
Trails are too crowded	2.45%	5
Trails are poorly maintained	16.67%	34
Hard to find information	37.75%	77
Not enough parking is available	7.35%	15
Lack of connections to trails by way of public transportation, sidewalks or other means that don't require a vehicle	31.86%	65
Don't feel safe	14.22%	29
No obstacles; we use the trails often	11.27%	23
Other (please specify)	14.22%	29
Total Respondents: 204		

#	OTHER (PLEASE SPECIFY)	DATE
1	Trails are not long enough	4/21/2022 8:01 PM
2	unfamiliar with trail information/locations	4/21/2022 2:12 PM
3	Far drive	4/21/2022 10:10 AM
4	Not enough time	4/20/2022 3:29 PM
5	I have bad ankles so I worry about rolling them on the uneven terrain	4/20/2022 2:53 PM
6	Not long enough for longer distance running	3/9/2022 10:03 AM
7	None. We use the trails we marked often.	3/9/2022 5:29 AM
8	We would love to have more!	3/8/2022 10:04 AM
9	I would love a 5-10 mile bike trail loop	2/24/2022 1:20 PM
10	I live 200 miles away and visit Myrtle Beach and Conway annually	2/23/2022 4:17 PM
11	Not handicapped accessible	2/23/2022 11:18 AM
12	No paved designated bike trails	2/23/2022 11:10 AM
13	Just don't think about using the trails as often	2/23/2022 5:56 AM
14	I use trails constantly but always with the risk of safety because I do not want to drive to a trail. So I often run or walk, but with dogs and children, that is often dangerous.	2/22/2022 4:09 PM
15	Rough roads leading into some trails	2/22/2022 9:32 AM
16	Don't know where they are	2/21/2022 7:20 PM
17	Flooding- sections of Waccamaw River Park and Waccamaw Wildlife have been hard to pass, even after mild rain	2/20/2022 8:14 PM
18	Not long enough	2/19/2022 7:28 PM
19	Too short. Need a trail That does not run along a roadside that is longer than 5 miles	2/18/2022 5:32 AM
20	Pointless	2/18/2022 12:50 AM
21	Limited kayak access points. Poorly designed kayak launches with large rocks that damage	2/17/2022 11:28 PM

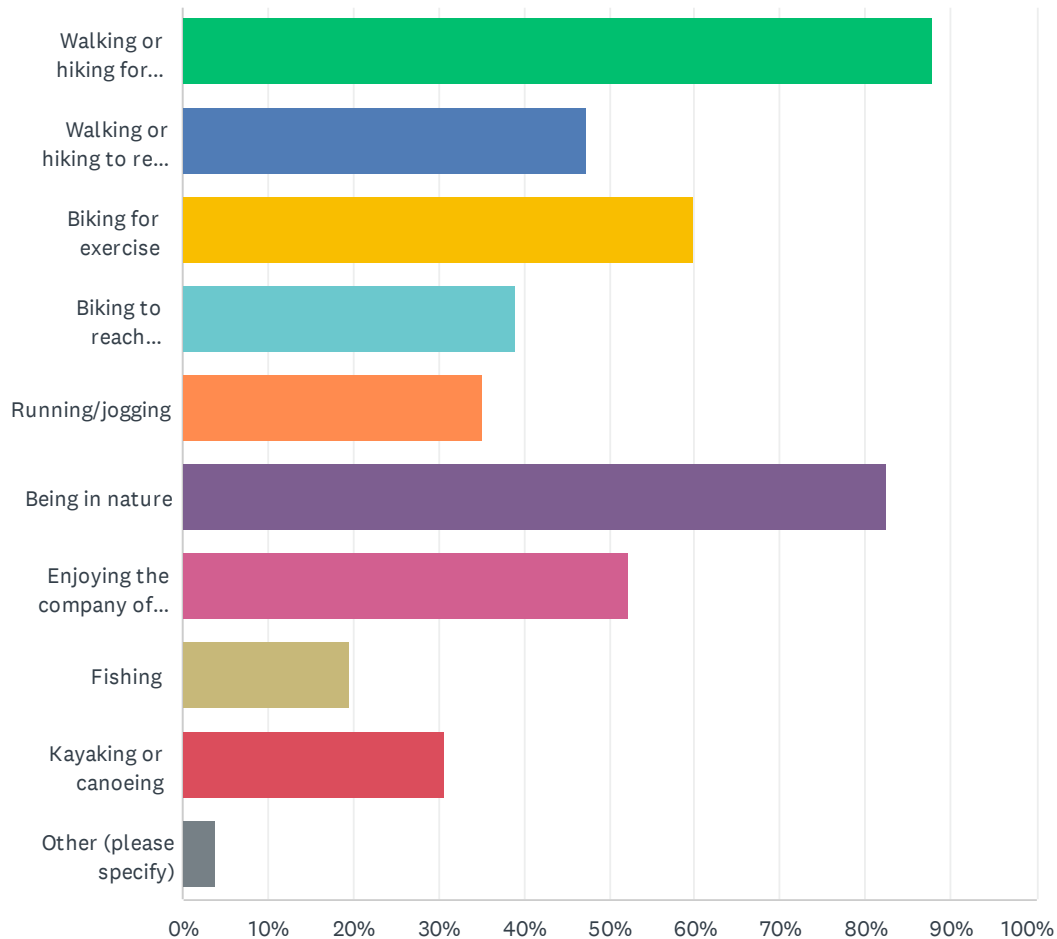


# Conway Pathways and Trails Plan - Survey No. 1

	kayaks	
22	Need connection/ add bike lanes when widening 701 N	2/17/2022 9:29 PM
23	Roads and stoplights are not cyclists friendly	2/17/2022 4:53 PM
24	Looking for more challenging obstacles	2/17/2022 2:29 PM
25	all the trails are too short, makes them boring	2/16/2022 2:59 PM
26	Some trails have been flooded or covered with debris for too long.	2/16/2022 11:06 AM
27	If you want to encourage people to ride to Conway, please plan for sufficient bicycle parking around town.	2/16/2022 8:06 AM
28	Waccamaw River park is almost always closed... there should be a sign on 501 business to indicate whether it is open or closed	2/15/2022 7:23 PM
29	Have to drive to get to them	2/14/2022 6:23 PM

# Q4 When you think about the future system of pathways and trails in Conway, how would you or your household most like to use the trails? (Check all that apply.)

Answered: 205 Skipped: 5



# Conway Pathways and Trails Plan - Survey No. 1

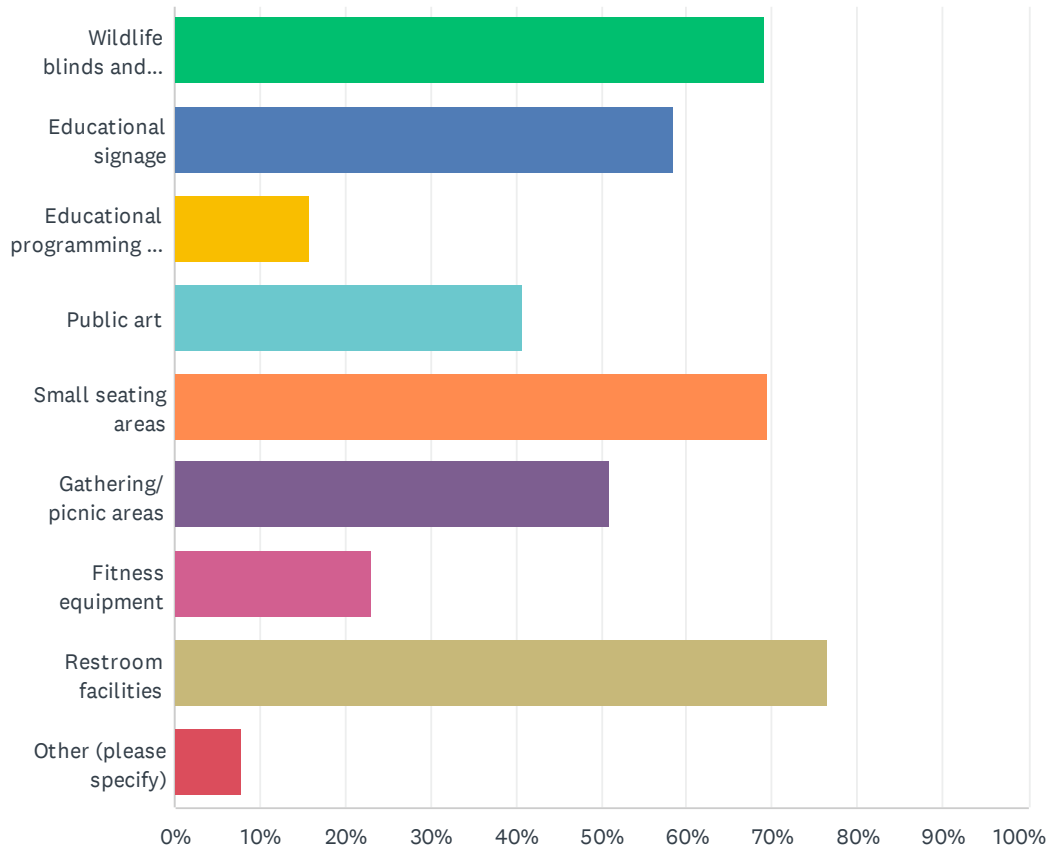
ANSWER CHOICES	RESPONSES	
Walking or hiking for exercise	87.80%	180
Walking or hiking to reach destinations	47.32%	97
Biking for exercise	60.00%	123
Biking to reach destinations	39.02%	80
Running/jogging	35.12%	72
Being in nature	82.44%	169
Enjoying the company of others	52.20%	107
Fishing	19.51%	40
Kayaking or canoeing	30.73%	63
Other (please specify)	3.90%	8
Total Respondents: 205		

#	OTHER (PLEASE SPECIFY)	DATE
1	Need a way to get from Ccu to conway without driving	4/21/2022 9:10 AM
2	Biking with children in the family	3/9/2022 4:18 PM
3	Forest bathing, more seclusion to get away from the "city" life. To meditate and relax	3/8/2022 10:04 AM
4	Mountain Biking	2/25/2022 1:11 AM
5	Especially important for us is safely biking to destinations instead of having to rely on a car. My husband is vision impaired and cannot drive, so safe cycling and walking is also a matter of equity.	2/22/2022 4:09 PM
6	Please add kayak racks along the River	2/17/2022 4:53 PM
7	paved trail system	2/15/2022 7:17 PM
8	Dog walking	2/14/2022 6:23 PM



## Q5 What types of elements/ experiences would you or your household like to encounter along the trails? (Check all that apply.)

Answered: 204 Skipped: 6



ANSWER CHOICES	RESPONSES	
Wildlife blinds and overlooks	69.12%	141
Educational signage	58.33%	119
Educational programming and tours	15.69%	32
Public art	40.69%	83
Small seating areas	69.61%	142
Gathering/ picnic areas	50.98%	104
Fitness equipment	23.04%	47
Restroom facilities	76.47%	156
Other (please specify)	7.84%	16
Total Respondents: 204		

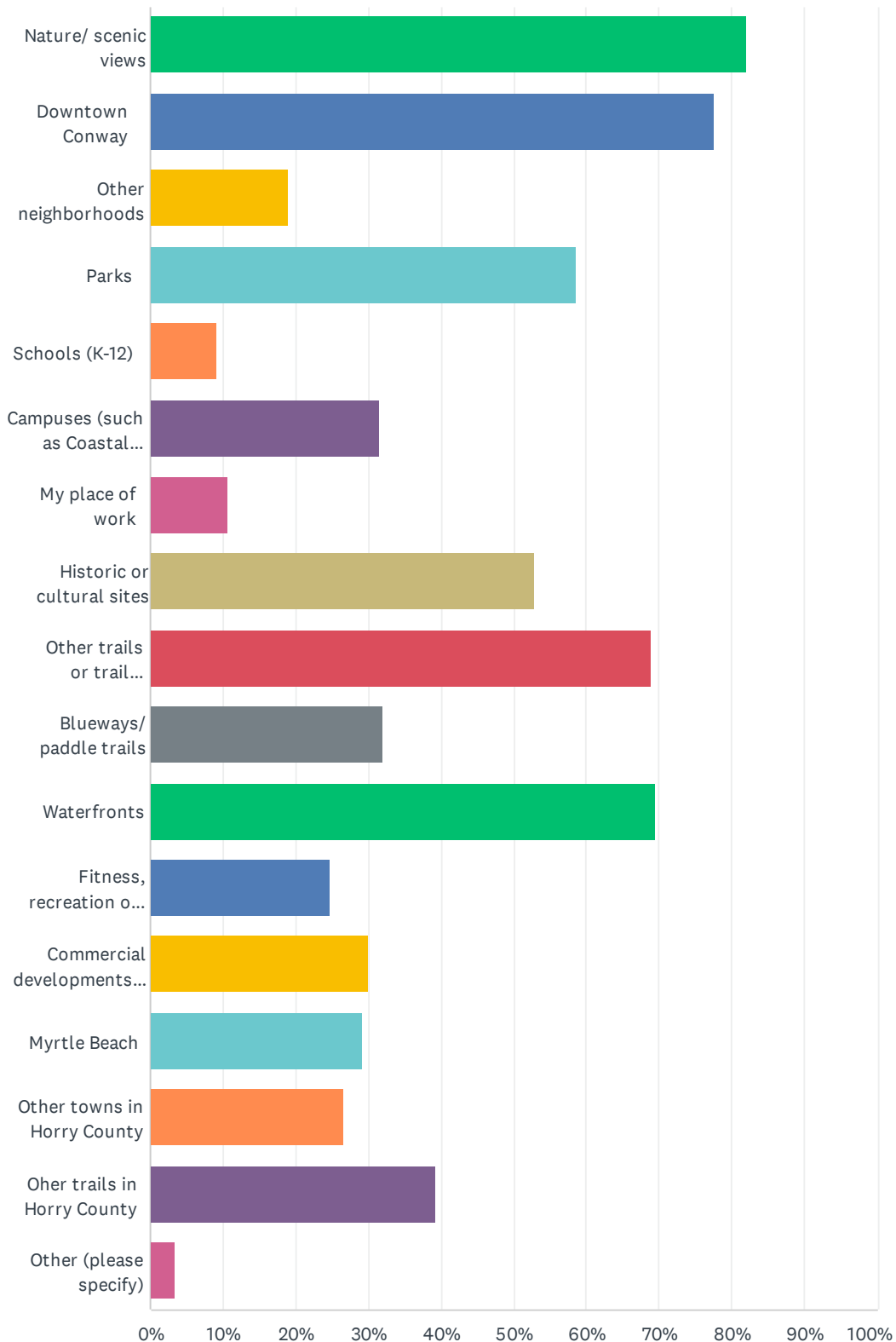
#	OTHER (PLEASE SPECIFY)	DATE
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## Conway Pathways and Trails Plan - Survey No. 1

1	connectivity between trails, variety of sights along a single trails (wooded areas vs open spaces vs along water vs thru neighborhoods etc)	4/20/2022 2:58 PM
2	Water bottle stations	3/9/2022 10:03 AM
3	Play area	3/7/2022 12:16 PM
4	Clearly defined signage to indicate current location and nearby surrounding areas.	3/2/2022 1:25 PM
5	Dining close to trails.	3/1/2022 7:06 PM
6	Fresh water stations	2/24/2022 8:01 PM
7	Jumps/obstacles	2/24/2022 2:34 PM
8	Smooth trails, easy access, mileage indicator for distance walking	2/23/2022 12:25 PM
9	longer bicycle trails	2/23/2022 10:44 AM
10	All options are great, but these three are most appealing to my family.	2/22/2022 4:09 PM
11	Bike/kayak rentals	2/20/2022 10:02 PM
12	The above list is nice, but there needs to be lots of natural trails with none of the above items on the list. Whatever you do, please don't commercialize the trails.	2/18/2022 10:44 PM
13	I love socializing and enjoying nature.	2/18/2022 8:57 AM
14	Benches, picnic tables and handicap accessible kayak launch. All kayak launches are in dangerous areas with high boat traffic at the marina and riverwalk	2/17/2022 11:28 PM
15	Optional bike features place along the trail for fun and skill building	2/17/2022 2:29 PM
16	paved trails	2/15/2022 7:17 PM

## Q6 What types of destinations do you wish you could access by way of public trails? (Check all that apply.)

Answered: 206 Skipped: 4



# Conway Pathways and Trails Plan - Survey No. 1

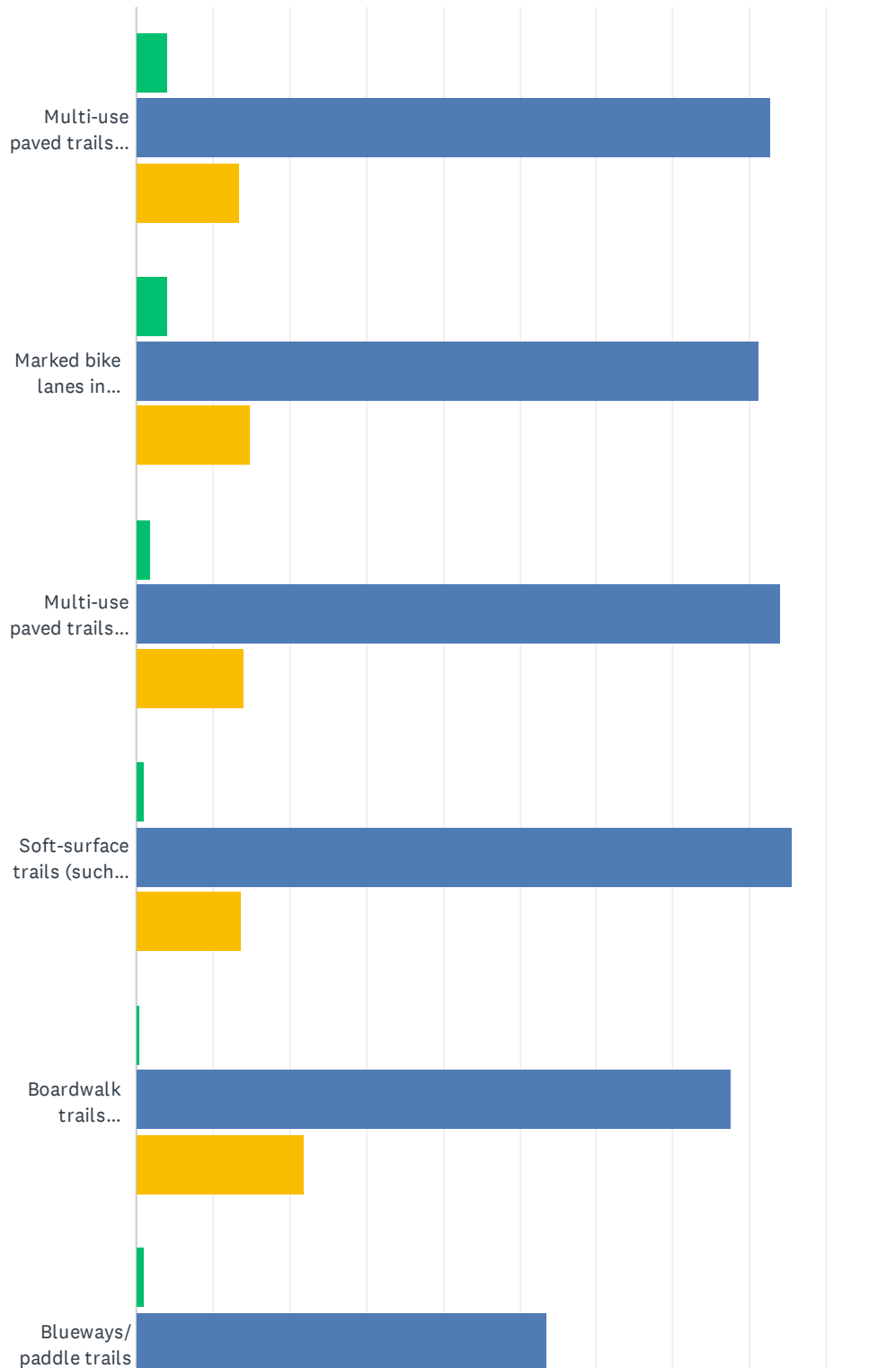
ANSWER CHOICES	RESPONSES	
Nature/ scenic views	82.04%	169
Downtown Conway	77.67%	160
Other neighborhoods	18.93%	39
Parks	58.74%	121
Schools (K-12)	9.22%	19
Campuses (such as Coastal Carolina University or Horry Georgetown Technical College)	31.55%	65
My place of work	10.68%	22
Historic or cultural sites	52.91%	109
Other trails or trail connections	68.93%	142
Blueways/ paddle trails	32.04%	66
Waterfronts	69.42%	143
Fitness, recreation or athletic facilities	24.76%	51
Commercial developments with shopping, restaurants, etc.	30.10%	62
Myrtle Beach	29.13%	60
Other towns in Horry County	26.70%	55
Other trails in Horry County	39.32%	81
Other (please specify)	3.40%	7
Total Respondents: 206		

#	OTHER (PLEASE SPECIFY)	DATE
1	Of greatest interest is CCU campus which is also my place of work. I commute by bicycle from North Conway, and it is dangerous.	2/22/2022 4:09 PM
2	I would enjoy continuous trails, that you could spend the entire day biking!	2/20/2022 10:02 PM
3	I enjoy learning about my hometown.	2/18/2022 8:57 AM
4	N/a	2/18/2022 12:50 AM
5	Some jurisdictions around the country have worked with railroads to place bike paths along their right-of-way.	2/16/2022 8:06 AM
6	Would love a bike trail along the railway leading from CCU to Carolina Forest Blvd (they are nearing completion of a bike path on CF Blvd, should connect to it!) Could potentially reduce traffic on 501, and I could bike to school and/or other commercial developments near 501/Postal Way	2/15/2022 3:10 PM
7	Walking trail extension along Crabtree Canal and more trail connections from North Conway to downtown Conway	2/14/2022 2:54 PM

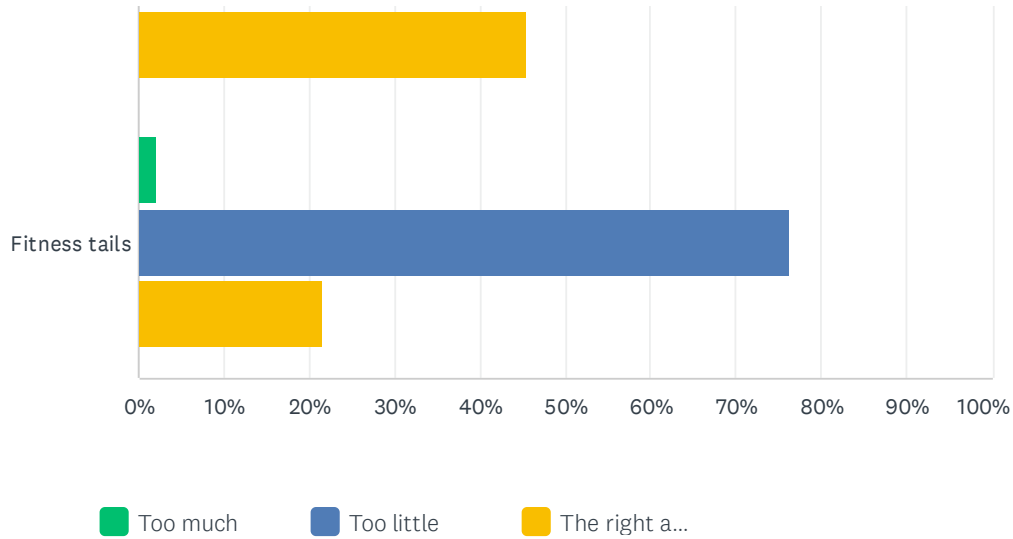


Q7 Pathways and trails can serve various purposes, some of which are listed below. For each item, please rate whether you think there is too much, too little or about the right amount in Conway to serve the needs of your household.

Answered: 205 Skipped: 5



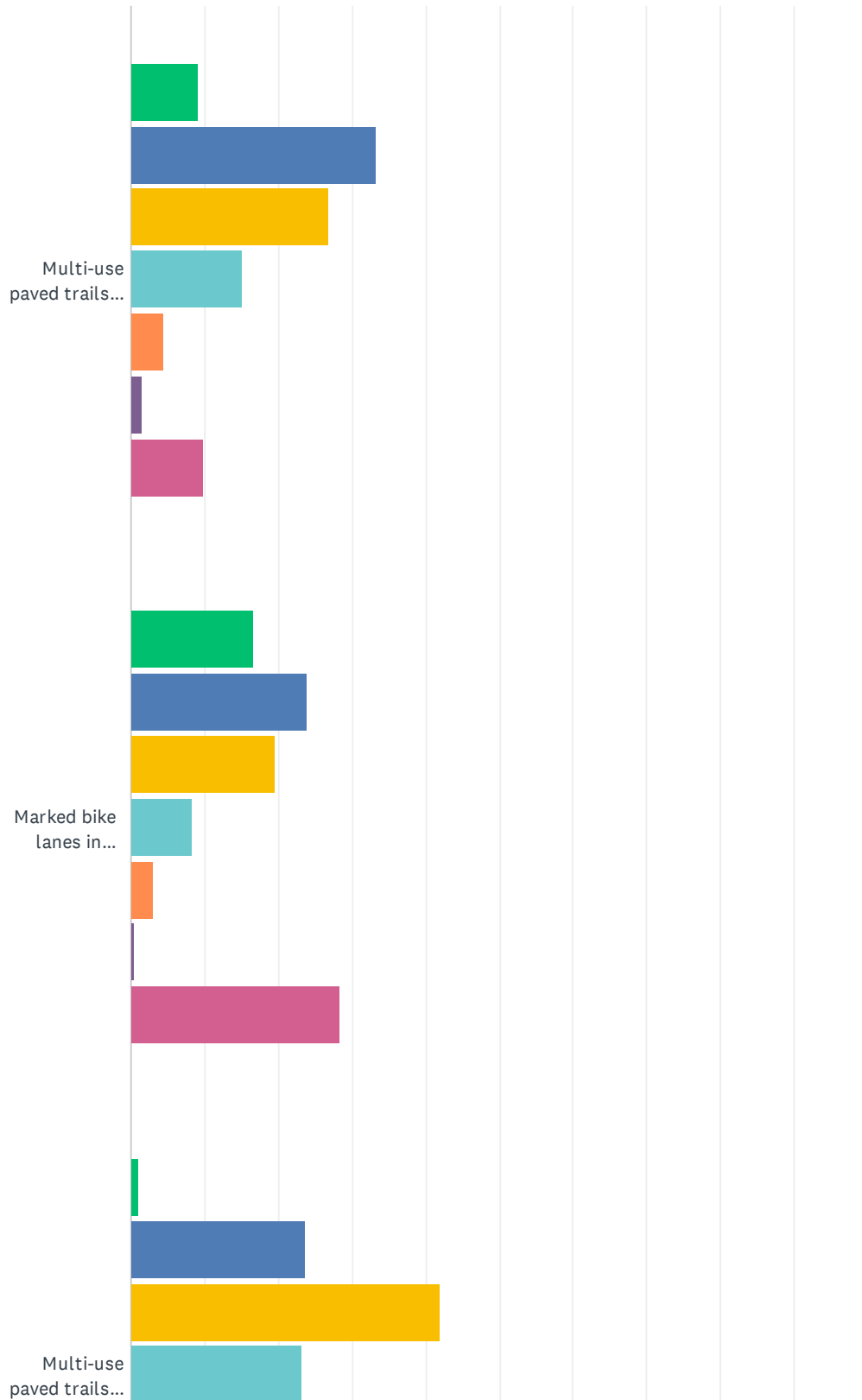
## Conway Pathways and Trails Plan - Survey No. 1

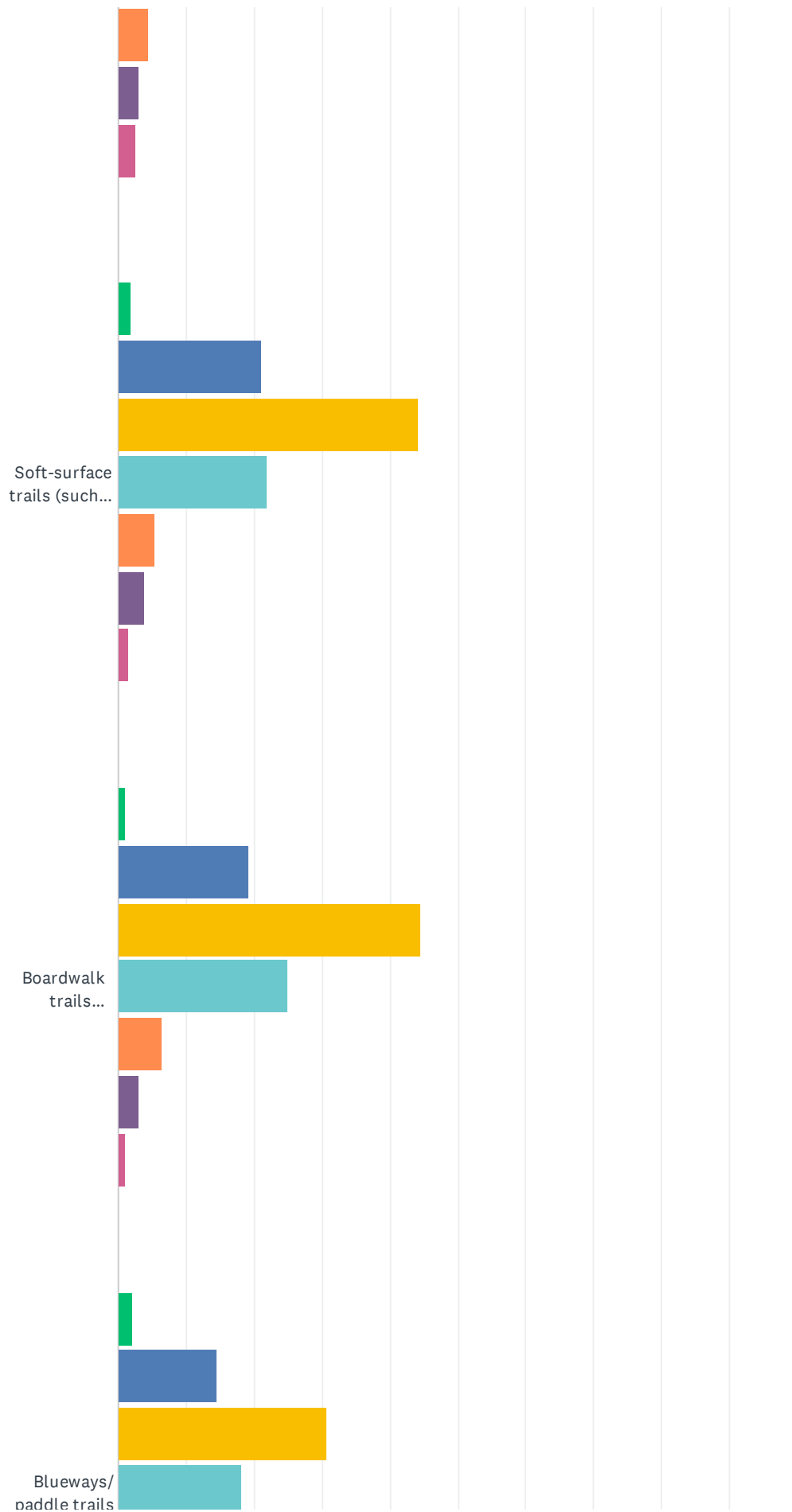


	TOO MUCH	TOO LITTLE	THE RIGHT AMOUNT	TOTAL	WEIGHTED AVERAGE
Multi-use paved trails next to roadways	3.96% 8	82.67% 167	13.37% 27	202	2.09
Marked bike lanes in roadways	3.96% 8	81.19% 164	14.85% 30	202	2.11
Multi-use paved trails through natural areas	2.00% 4	84.00% 168	14.00% 28	200	2.12
Soft-surface trails (such as gravel or dirt) through natural areas	1.01% 2	85.43% 170	13.57% 27	199	2.13
Boardwalk trails alongside streams or wetlands	0.50% 1	77.61% 156	21.89% 44	201	2.21
Blueways/ paddle trails	1.07% 2	53.48% 100	45.45% 85	187	2.44
Fitness trails	2.11% 4	76.32% 145	21.58% 41	190	2.19

## Q8 How far would you and your household be willing to travel from your home to access these types of trails?

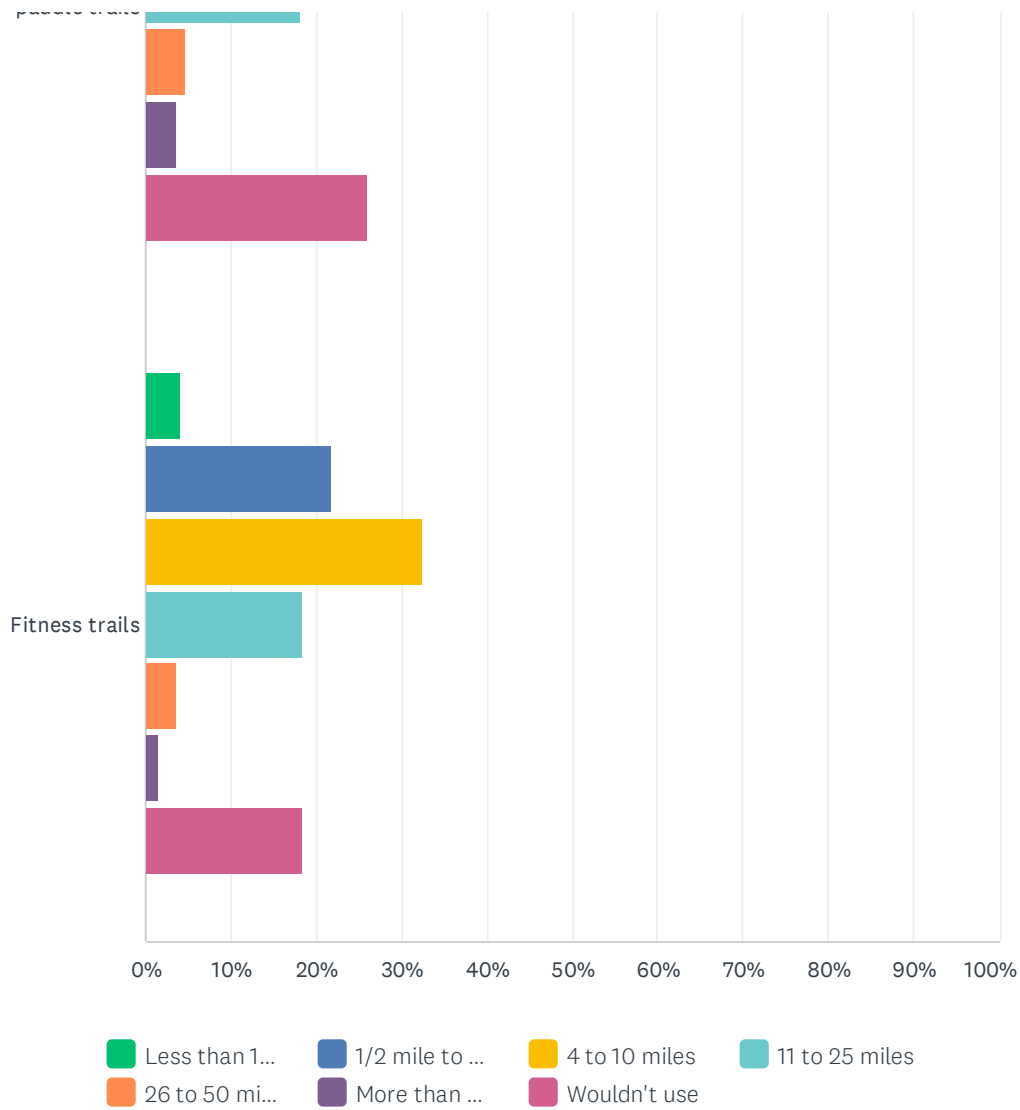
Answered: 207 Skipped: 3







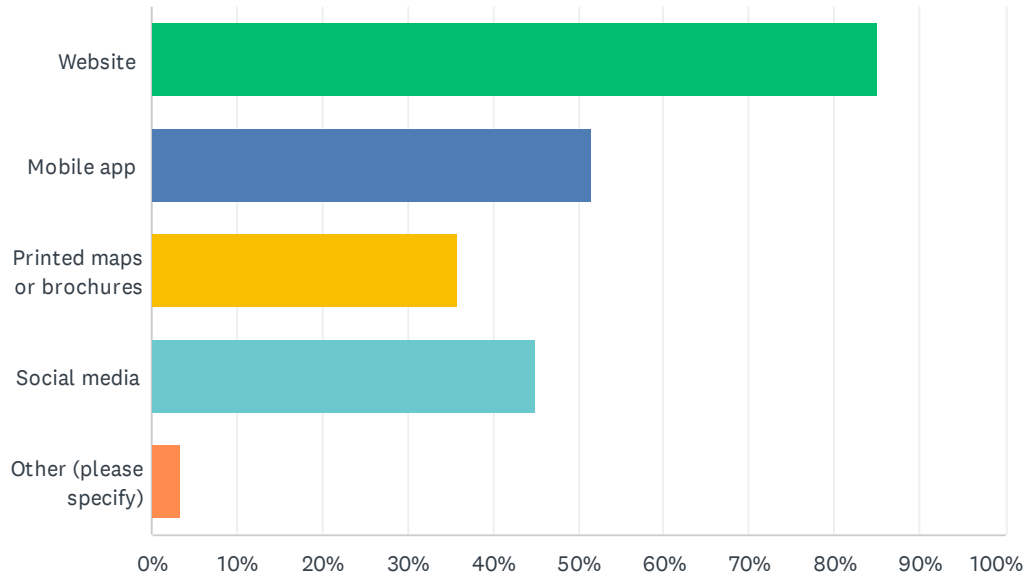
# Conway Pathways and Trails Plan - Survey No. 1



	LESS THAN 1/2 MILE	1/2 MILE TO 3 MILES	4 TO 10 MILES	11 TO 25 MILES	26 TO 50 MILES	MORE THAN 50 MILES	WOULDN'T USE	TOTAL
Multi-use paved trails next to roadways	9.27% 19	33.17% 68	26.83% 55	15.12% 31	4.39% 9	1.46% 3	9.76% 20	205
Marked bike lanes in roadways	16.59% 34	23.90% 49	19.51% 40	8.29% 17	2.93% 6	0.49% 1	28.29% 58	205
Multi-use paved trails through natural areas	0.99% 2	23.76% 48	42.08% 85	23.27% 47	4.46% 9	2.97% 6	2.48% 5	202
Soft-surface trails (such as gravel or dirt)	1.96% 4	21.08% 43	44.12% 90	22.06% 45	5.39% 11	3.92% 8	1.47% 3	204
Boardwalk trails alongside streams or wetlands	0.98% 2	19.12% 39	44.61% 91	25.00% 51	6.37% 13	2.94% 6	0.98% 2	204
Blueways/ paddle trails	2.08% 4	14.58% 28	30.73% 59	18.23% 35	4.69% 9	3.65% 7	26.04% 50	192
Fitness trails	4.06% 8	21.83% 43	32.49% 64	18.27% 36	3.55% 7	1.52% 3	18.27% 36	197

## Q9 What are the top two ways you would like to be able to find information about pathways and trails in Conway?

Answered: 207 Skipped: 3

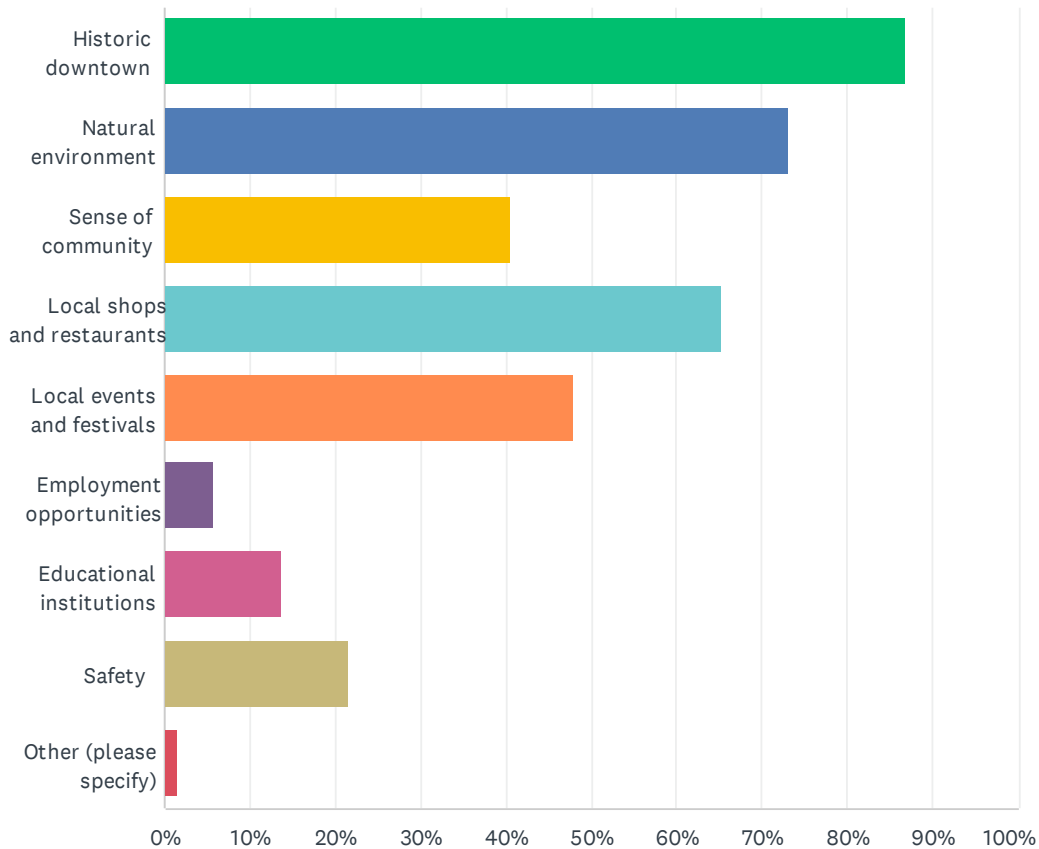


ANSWER CHOICES	RESPONSES	
Website	85.02%	176
Mobile app	51.69%	107
Printed maps or brochures	35.75%	74
Social media	44.93%	93
Other (please specify)	3.38%	7
Total Respondents: 207		

#	OTHER (PLEASE SPECIFY)	DATE
1	signage	2/24/2022 2:10 PM
2	Newspaper	2/23/2022 4:17 PM
3	signs	2/23/2022 12:25 PM
4	Alltrails web app	2/23/2022 9:32 AM
5	Printed maps at trail head	2/20/2022 7:20 PM
6	Public signs and maps	2/17/2022 11:28 PM
7	Email	2/17/2022 7:06 PM

## Q10 Conway's future system of pathways and trails should reflect the values of the Conway community. What are your favorite things about living in Conway? (Choose your top three.)

Answered: 190 Skipped: 20



ANSWER CHOICES	RESPONSES	
Historic downtown	86.84%	165
Natural environment	73.16%	139
Sense of community	40.53%	77
Local shops and restaurants	65.26%	124
Local events and festivals	47.89%	91
Employment opportunities	5.79%	11
Educational institutions	13.68%	26
Safety	21.58%	41
Other (please specify)	1.58%	3
Total Respondents: 190		

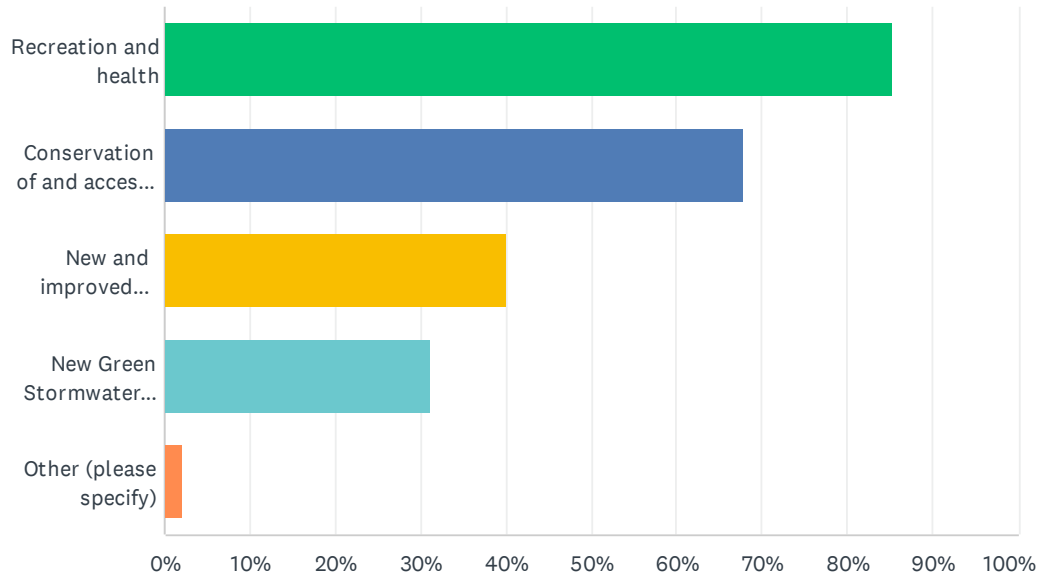
# Conway Pathways and Trails Plan - Survey No. 1

#	OTHER (PLEASE SPECIFY)	DATE
1	Diversity and making sure opportunities are available to all citizens of Conway	2/27/2022 10:20 AM
2	I don't live there	2/23/2022 4:18 PM
3	River access	2/17/2022 11:48 PM



## Q11 Which quality-of-life benefits of a trail system are most important to you?

Answered: 190 Skipped: 20

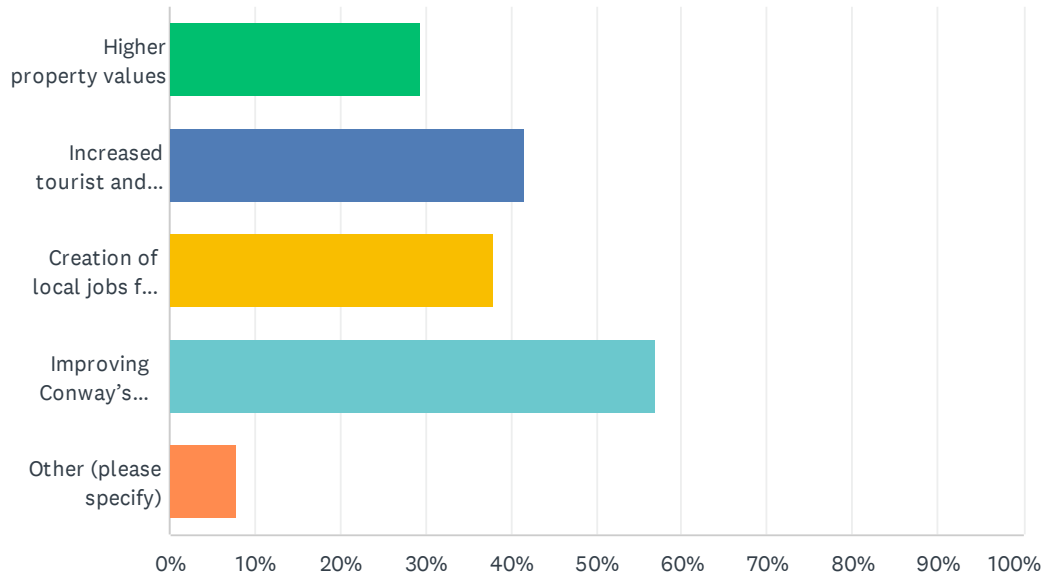


ANSWER CHOICES	RESPONSES	
Recreation and health	85.26%	162
Conservation of and access to nature	67.89%	129
New and improved options for reaching local destinations without a car	40.00%	76
New Green Stormwater Infrastructure (GSI) opportunities, including protection of land within the floodplain	31.05%	59
Other (please specify)	2.11%	4
Total Respondents: 190		

#	OTHER (PLEASE SPECIFY)	DATE
1	Educational/historic preservation opportunities	4/20/2022 3:26 PM
2	Will offer less wildlife hunting	2/18/2022 12:54 AM
3	Escape from noise, crowds and traffic	2/17/2022 11:48 PM
4	Promoting health and fitness to future generations, our youth as well as old need a safe place to enjoy nature and ride bikes or walk trails.	2/16/2022 2:41 PM

## Q12 Which economic development benefits of a trail system are most important to you?

Answered: 190 Skipped: 20



ANSWER CHOICES	RESPONSES	
Higher property values	29.47%	56
Increased tourist and resident traffic to local businesses	41.58%	79
Creation of local jobs for the design, engineering, construction and ongoing maintenance of trails	37.89%	72
Improving Conway's competitive edge to attract employers and retailers	56.84%	108
Other (please specify)	7.89%	15
Total Respondents: 190		

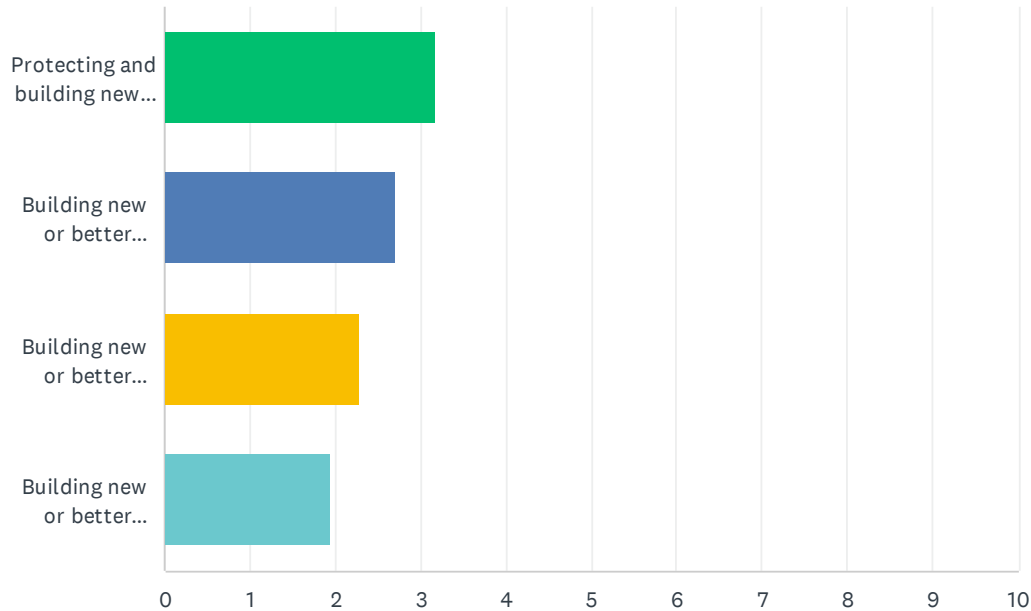
#	OTHER (PLEASE SPECIFY)	DATE
1	Safe places to exercise	4/21/2022 10:20 PM
2	The economic benefits of a healthier population	3/9/2022 4:28 PM
3	healthier people, reduction of fuel use and carbon emissions	3/1/2022 12:20 AM
4	connecting the area and preserving nature while educating	2/24/2022 5:29 PM
5	The opportunity to enjoy nature in historical setting in a safe environment.	2/24/2022 2:20 PM
6	I really don't want Conway to grow anymore	2/23/2022 2:41 PM
7	Better access to Downtown to visit stores and restaurants	2/23/2022 10:28 AM
8	N/a	2/23/2022 8:47 AM
9	I have not considered the economic impact.	2/22/2022 3:00 PM
10	Preserve the small town feel of historic Conway	2/21/2022 4:34 PM

## Conway Pathways and Trails Plan - Survey No. 1

11	Increasing alternative transportation options (alternatives to driving)	2/21/2022 8:03 AM
12	a place by roads for safety to walk or bike to destinations especially 701 north to town	2/18/2022 9:48 AM
13	Will bring Yankees	2/18/2022 12:54 AM
14	Increased tax revenue from increased tourism	2/17/2022 11:48 PM
15	I do not link economic development to any kind of pathway system	2/15/2022 7:30 PM

### Q13 What do you think should be the biggest focus of the future Conway trail system? Please rank from highest (1) to lowest (4).

Answered: 192 Skipped: 18

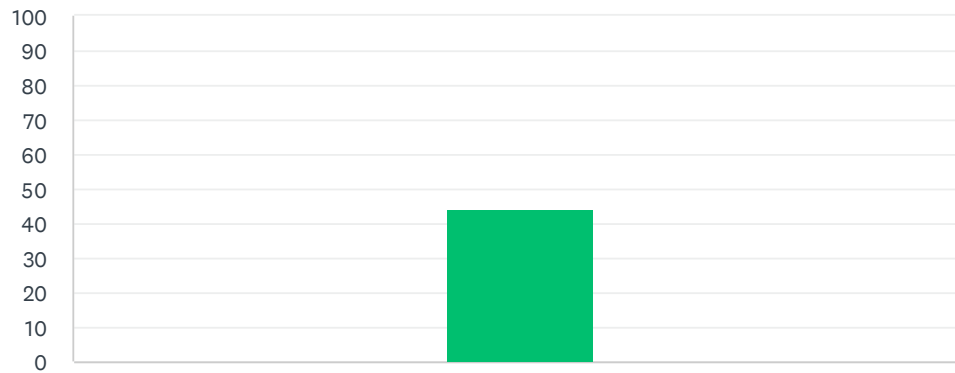


	1	2	3	4	TOTAL	SCORE
Protecting and building new connections to Conway's natural resources	53.48% 100	21.39% 40	14.44% 27	10.70% 20	187	3.18
Building new or better connections to local destinations – such as parks, schools, campuses and Downtown	24.06% 45	38.50% 72	21.39% 40	16.04% 30	187	2.71
Building new or better connections between Conway communities – including those separated by the river	12.23% 23	25.53% 48	40.43% 76	21.81% 41	188	2.28
Building new or better connections to destinations across Horry County – such as other towns, state parks, other trail systems and the beach	12.70% 24	15.87% 30	22.75% 43	48.68% 92	189	1.93



## Q14 As the City prioritizes trail system improvements, which is more important in the short term: Building high-quality standalone trail experiences, or building out communitywide connectivity?

Answered: 189 Skipped: 21



ANSWER CHOICES	AVERAGE NUMBER	TOTAL NUMBER	RESPONSES
	44	8,401	189
Total Respondents: 189			

BASIC STATISTICS					
	MINIMUM	MAXIMUM	MEDIAN	MEAN	STANDARD DEVIATION
	0.00	100.00	49.00	44.45	28.35

#		DATE
1	57	4/25/2022 12:49 PM
2	65	4/24/2022 10:06 PM
3	54	4/22/2022 9:46 AM
4	14	4/21/2022 11:40 PM
5	50	4/21/2022 8:05 PM
6	91	4/21/2022 4:28 PM
7	15	4/21/2022 4:27 PM
8	50	4/21/2022 2:16 PM
9	13	4/21/2022 2:08 PM
10	12	4/21/2022 12:57 PM
11	50	4/21/2022 12:04 PM
12	33	4/21/2022 11:49 AM
13	56	4/21/2022 10:11 AM
14	71	4/21/2022 9:12 AM

# Conway Pathways and Trails Plan - Survey No. 1

15	55	4/21/2022 9:09 AM
16	0	4/21/2022 8:54 AM
17	38	4/21/2022 8:54 AM
18	6	4/21/2022 8:52 AM
19	50	4/21/2022 8:31 AM
20	49	4/20/2022 4:06 PM
21	75	4/20/2022 3:26 PM
22	17	4/20/2022 2:56 PM
23	24	4/20/2022 2:44 PM
24	67	3/9/2022 4:28 PM
25	80	3/9/2022 4:22 PM
26	0	3/9/2022 10:10 AM
27	33	3/9/2022 9:18 AM
28	51	3/9/2022 6:41 AM
29	21	3/8/2022 2:20 PM
30	51	3/8/2022 10:12 AM
31	5	3/7/2022 10:35 PM
32	50	3/7/2022 8:49 PM
33	49	3/7/2022 6:52 PM
34	48	3/7/2022 2:24 PM
35	67	3/7/2022 1:17 PM
36	51	3/7/2022 1:05 PM
37	46	3/7/2022 12:34 PM
38	77	3/7/2022 12:33 PM
39	3	3/7/2022 12:19 PM
40	36	3/7/2022 12:17 PM
41	3	3/7/2022 11:59 AM
42	50	3/7/2022 11:39 AM
43	7	3/7/2022 10:47 AM
44	66	3/2/2022 1:29 PM
45	100	3/1/2022 7:16 PM
46	40	3/1/2022 12:20 AM
47	50	2/27/2022 10:20 AM
48	22	2/26/2022 9:16 AM
49	31	2/25/2022 3:50 PM
50	32	2/24/2022 9:36 PM
51	40	2/24/2022 8:06 PM
52	10	2/24/2022 7:03 PM

# Conway Pathways and Trails Plan - Survey No. 1

53	51	2/24/2022 5:36 PM
54	66	2/24/2022 5:29 PM
55	75	2/24/2022 5:12 PM
56	54	2/24/2022 4:18 PM
57	17	2/24/2022 2:51 PM
58	31	2/24/2022 2:33 PM
59	50	2/24/2022 2:20 PM
60	0	2/24/2022 2:05 PM
61	17	2/24/2022 1:23 PM
62	80	2/24/2022 12:16 PM
63	14	2/24/2022 10:40 AM
64	14	2/24/2022 9:02 AM
65	78	2/24/2022 7:55 AM
66	49	2/24/2022 4:52 AM
67	77	2/24/2022 12:49 AM
68	75	2/23/2022 10:16 PM
69	67	2/23/2022 9:31 PM
70	87	2/23/2022 5:19 PM
71	48	2/23/2022 4:18 PM
72	27	2/23/2022 4:14 PM
73	50	2/23/2022 2:41 PM
74	70	2/23/2022 1:35 PM
75	86	2/23/2022 12:30 PM
76	50	2/23/2022 12:16 PM
77	51	2/23/2022 11:26 AM
78	77	2/23/2022 11:20 AM
79	2	2/23/2022 11:14 AM
80	73	2/23/2022 10:48 AM
81	54	2/23/2022 10:28 AM
82	100	2/23/2022 10:27 AM
83	29	2/23/2022 9:47 AM
84	53	2/23/2022 9:36 AM
85	5	2/23/2022 9:35 AM
86	52	2/23/2022 9:17 AM
87	52	2/23/2022 8:47 AM
88	9	2/23/2022 8:44 AM
89	59	2/23/2022 8:08 AM
90	49	2/23/2022 7:38 AM

# Conway Pathways and Trails Plan - Survey No. 1

91	78	2/23/2022 7:25 AM
92	100	2/23/2022 5:58 AM
93	50	2/22/2022 10:37 PM
94	25	2/22/2022 10:22 PM
95	93	2/22/2022 10:06 PM
96	0	2/22/2022 9:33 PM
97	50	2/22/2022 8:38 PM
98	0	2/22/2022 8:14 PM
99	0	2/22/2022 7:56 PM
100	40	2/22/2022 5:35 PM
101	10	2/22/2022 5:12 PM
102	31	2/22/2022 5:08 PM
103	100	2/22/2022 4:58 PM
104	48	2/22/2022 4:32 PM
105	50	2/22/2022 4:27 PM
106	20	2/22/2022 4:18 PM
107	100	2/22/2022 4:12 PM
108	5	2/22/2022 3:00 PM
109	4	2/22/2022 11:17 AM
110	50	2/22/2022 10:13 AM
111	35	2/22/2022 9:49 AM
112	59	2/22/2022 9:35 AM
113	61	2/22/2022 9:27 AM
114	53	2/21/2022 7:26 PM
115	6	2/21/2022 5:48 PM
116	100	2/21/2022 4:34 PM
117	10	2/21/2022 10:02 AM
118	80	2/21/2022 9:44 AM
119	52	2/21/2022 8:35 AM
120	100	2/21/2022 8:03 AM
121	75	2/20/2022 10:05 PM
122	50	2/20/2022 8:27 PM
123	48	2/20/2022 7:24 PM
124	42	2/20/2022 5:40 PM
125	18	2/20/2022 11:25 AM
126	0	2/19/2022 7:14 PM
127	49	2/19/2022 4:40 PM
128	61	2/19/2022 3:34 PM



# Conway Pathways and Trails Plan - Survey No. 1

129	75	2/19/2022 9:09 AM
130	4	2/19/2022 7:57 AM
131	13	2/19/2022 7:51 AM
132	18	2/19/2022 7:22 AM
133	50	2/18/2022 10:51 PM
134	67	2/18/2022 10:47 PM
135	65	2/18/2022 5:11 PM
136	92	2/18/2022 4:37 PM
137	93	2/18/2022 9:48 AM
138	100	2/18/2022 9:04 AM
139	35	2/18/2022 7:04 AM
140	27	2/18/2022 5:41 AM
141	31	2/17/2022 11:48 PM
142	100	2/17/2022 9:36 PM
143	50	2/17/2022 8:55 PM
144	0	2/17/2022 8:25 PM
145	43	2/17/2022 8:07 PM
146	32	2/17/2022 7:16 PM
147	0	2/17/2022 7:10 PM
148	14	2/17/2022 6:55 PM
149	33	2/17/2022 6:01 PM
150	1	2/17/2022 5:36 PM
151	3	2/17/2022 5:11 PM
152	53	2/17/2022 5:00 PM
153	16	2/17/2022 3:40 PM
154	71	2/17/2022 3:01 PM
155	31	2/17/2022 2:37 PM
156	23	2/17/2022 8:17 AM
157	50	2/16/2022 11:24 PM
158	46	2/16/2022 7:40 PM
159	45	2/16/2022 7:21 PM
160	91	2/16/2022 6:19 PM
161	50	2/16/2022 5:48 PM
162	13	2/16/2022 3:57 PM
163	50	2/16/2022 3:03 PM
164	49	2/16/2022 2:41 PM
165	98	2/16/2022 2:30 PM
166	17	2/16/2022 2:28 PM

# Conway Pathways and Trails Plan - Survey No. 1

167	49	2/16/2022 2:15 PM
168	50	2/16/2022 11:10 AM
169	53	2/16/2022 11:00 AM
170	52	2/16/2022 10:40 AM
171	48	2/16/2022 9:04 AM
172	27	2/16/2022 9:00 AM
173	5	2/16/2022 8:42 AM
174	22	2/16/2022 8:12 AM
175	100	2/16/2022 8:11 AM
176	1	2/16/2022 6:23 AM
177	20	2/16/2022 6:22 AM
178	0	2/16/2022 6:14 AM
179	28	2/15/2022 7:30 PM
180	52	2/15/2022 7:22 PM
181	40	2/15/2022 5:27 PM
182	77	2/15/2022 3:17 PM
183	12	2/14/2022 6:26 PM
184	20	2/14/2022 6:25 PM
185	72	2/14/2022 3:21 PM
186	100	2/14/2022 3:00 PM
187	13	2/14/2022 2:57 PM
188	84	2/14/2022 2:34 PM
189	45	2/14/2022 1:42 PM

## Q15 What do you think will define success for the Conway Pathways and Trails Plan?

Answered: 161 Skipped: 49

#	RESPONSES	DATE
1	Safety of the rails, education on the trail features, lighting for trail use at dawn and dusk.	4/25/2022 12:49 PM
2	Constant use by locals	4/24/2022 10:06 PM
3	The quality of the new infrastructure	4/22/2022 9:46 AM
4	Use	4/21/2022 11:40 PM
5	People using them	4/21/2022 10:20 PM
6	Increased health through exercise	4/21/2022 8:05 PM
7	Implementation and marketing	4/21/2022 4:28 PM
8	Quality and practicality of trails that are built	4/21/2022 4:27 PM
9	accessibility, promotion/marketing and safety	4/21/2022 2:16 PM
10	Usage. The key is constructing trails that check as many boxes as possible so people actually use them.	4/21/2022 2:08 PM
11	Accessibility and parking availability for those using them.	4/21/2022 12:57 PM
12	Locations and accessibility	4/21/2022 12:04 PM
13	People using them	4/21/2022 10:11 AM
14	Long term, year round use and Safety options along the trail	4/21/2022 9:09 AM
15	Parking availability at trailheads	4/21/2022 8:54 AM
16	Useage of the trails	4/21/2022 8:54 AM
17	Trails need to be at least 10 miles long to enable extended fitness activity	4/21/2022 8:52 AM
18	People knowing about them	4/21/2022 8:31 AM
19	user enjoyment. . .utilizing the pathways/trails and seeing others using it as well. for example, the riverwalk and park is usually crowded on a beautiful day because it's appreciated and well done.	4/20/2022 3:26 PM
20	I highly used system of trails that provide alternatives to driving that also assist in adding to the livability of the town.	4/20/2022 2:56 PM
21	If any new trials get built without raising taxes	4/20/2022 2:44 PM
22	A trail system that would be safe and could actually be used by enough people to make this area less car dependent. People might even have improvements in their health if they could get to some of the places they need to go without having to drive everywhere.	3/9/2022 4:28 PM
23	Accessibility and multipurpose use	3/9/2022 10:10 AM
24	Conway will become more of what it already is - more people will come to Conway to enjoy its natural beauty.	3/9/2022 6:41 AM
25	How much they are used.	3/8/2022 2:20 PM
26	They need to be close to neighborhoods with families or directly connected to them AND/OR with parking lots (if it's a nature trail type of thing). Neighborhoods build by large builders are secluded and can't leave their neighborhoods to go on a bike ride with the family because there aren't bike lanes/paths on main roads like 501, Cultra, Oak St. Main St/Church St, etc.	3/8/2022 10:12 AM

## Conway Pathways and Trails Plan - Survey No. 1

27	They need to feel safer. Too many creepy people on them, not enough families.	3/7/2022 10:35 PM
28	Everyone wanting it and using it. Finding help on building it. Help with costs.	3/7/2022 8:49 PM
29	More trails and in new areas. Building a walking culture in Conway.	3/7/2022 6:52 PM
30	Intermittent with nature	3/7/2022 2:24 PM
31	Safe connections to Myrtle beach. It's not a friendly area to those without cars.	3/7/2022 12:34 PM
32	Creating a large amount of trails that interconnect throughout Conway and Horry County and making sure the public knows about them.	3/7/2022 12:33 PM
33	Building quality, thoughtful trails with nature in mind. Quality of Quantity.	3/7/2022 12:19 PM
34	Safety of trails	3/7/2022 11:59 AM
35	Using it	3/7/2022 10:47 AM
36	Separate designated pathways from motor vehicle roads. Clearly defined pathway and directions. Well lit and safe.	3/2/2022 1:29 PM
37	The ability of community members to travel without use of a car.	3/1/2022 7:16 PM
38	Citizens that know more about their natural environment. Usage by citizens.	3/1/2022 12:20 AM
39	More people from all parts of the city having access.	2/27/2022 10:20 AM
40	Scenic and well planned and maintained trails	2/26/2022 9:16 AM
41	Usage	2/25/2022 3:50 PM
42	Everyone in the community, inside and outside Conway, will know we have great trails.	2/24/2022 9:36 PM
43	Active use by the community	2/24/2022 8:06 PM
44	Having a solid plan and support from the community	2/24/2022 7:03 PM
45	Continual community feedback	2/24/2022 5:29 PM
46	Significant increase in regular use	2/24/2022 5:12 PM
47	An experience that lures tourists from Myrtle Beach to Conway.	2/24/2022 4:18 PM
48	Quality	2/24/2022 2:33 PM
49	Safe areas, easily accessed, in a beautiful, natural setting.	2/24/2022 2:20 PM
50	Build and they will come. Build it and they will come.	2/24/2022 2:05 PM
51	More community members using and sharing the trails	2/24/2022 1:23 PM
52	Connection throughout the county	2/24/2022 12:16 PM
53	When all of Conway can be reached safely on a bike. Without the worry of being hit by a car or truck..	2/24/2022 9:02 AM
54	Being able to use Conway's natural resources safely on foot or bike while traveling to and from downtown	2/24/2022 7:55 AM
55	Not making it commercialized keeping it Conway	2/24/2022 4:52 AM
56	Less pedestrian deaths and more ease of access for pedestrian going to their destinations	2/24/2022 12:49 AM
57	I currently bike a 3 mile loop around my neighborhood to make 20 miles. If I could ride twenty miles on a trail, dirt or paved, without going in circles, I would consider that a success. Even if I have to drive across town to access the trailhead.	2/23/2022 10:16 PM
58	Focus on equity in who it serves among the various parts of town and communities	2/23/2022 9:31 PM
59	Advertising. Well marked, multi-use and easily accessible with multiple entry points. A 10 year vision that ultimately crosses the Waccamaw and connects to Myrtle Beach. Think that Swamp Rabbit Trail in Greenville County, SC.	2/23/2022 5:19 PM



## Conway Pathways and Trails Plan - Survey No. 1

60	A well thought out plan that takes into consideration our past flooding issues, as well as what the future could bring. Also, a plan that takes into consideration the long term maintenance issues and a guarantee that those long term maintenance needs will addressed and fully funded.	2/23/2022 4:14 PM
61	More miles of trails and higher usage rate	2/23/2022 2:41 PM
62	Balancing needs and careful planning	2/23/2022 12:30 PM
63	Trails that are a good distance for getting miles in for exercise while being well-maintained and offering scenic views.	2/23/2022 12:16 PM
64	Handicapped accessible trails that connect to parks and have trails for joggers walkers and bikers.	2/23/2022 11:26 AM
65	Community use of trails	2/23/2022 11:14 AM
66	funding and interest in outdoor activities	2/23/2022 10:48 AM
67	Becoming a nationally ranked bike friendly community.	2/23/2022 10:28 AM
68	Look at Ohio as it developed a great bike system and trails throughout it's community's and it continues even now.	2/23/2022 10:27 AM
69	Community buy in	2/23/2022 9:35 AM
70	More people getting out to use the trails	2/23/2022 9:17 AM
71	getting the word out there i have been to waccamaw wildlife refuge and there are few people there parking llooks shut off	2/23/2022 8:47 AM
72	This is a growing area full of local families with children. Introducing them with more opportunities to explore the natural world on safe trails away from traffic is crucial. We often have to drive 45 minutes to visit quality nature trails like the state parks, Vereen Gardens, etc. From the exception of the Riverwalk. The nicest hiking trail closest to our home is Cox Ferry and that is still a 25min drive coming from 378. As a homeschool family that loves hiking and nature we are thrilled this project is underway and can't wait to see the beautiful ideas you come up with. I am concerned about the safety of college students if you plan to extend from CCU to Conway. Please have adequate lighting and safety boxes available for these young students.	2/23/2022 8:44 AM
73	Availability and safety.	2/23/2022 8:08 AM
74	Safe and accessible.	2/22/2022 10:37 PM
75	A place for restoring mental and physical focus while enjoying nature and seeking a community feel.	2/22/2022 10:22 PM
76	Longer, safe, maintained trails that attracts traffic which can help keep trails maintained.	2/22/2022 10:06 PM
77	It would be amazing to have well maintained trails to choose from. I walked on several recently and they were either overgrown or didn't feel safe	2/22/2022 9:33 PM
78	Higher usage across the board	2/22/2022 8:38 PM
79	solid foundation of pathways, bike lanes, maintenance of pathways	2/22/2022 7:56 PM
80	Creating safer and more paved trails throughout Conway's natural habitats and creating safer access to walk around Downtown	2/22/2022 5:35 PM
81	Common sense planning and implementation	2/22/2022 5:12 PM
82	Marking trails properly. Adding information about flora, birds, and distance. Making Swamp busby safe! Miss the lake. And afraid of walking there alone because of gators.	2/22/2022 5:08 PM
83	Connectivity of HGTC and CCU to downtown Conway, preservation of nature and healthy and eco-friendly ways to connect.	2/22/2022 4:58 PM
84	Involvement by both city leaders and residents	2/22/2022 4:32 PM
85	Actually having something come from this. All to often these studies are completed and nothing is ever done.	2/22/2022 4:27 PM

## Conway Pathways and Trails Plan - Survey No. 1

86	Connecting communities	2/22/2022 4:18 PM
87	Connectivity, safety, functionality	2/22/2022 4:12 PM
88	condition and availability of trails. hate to see them closed.	2/22/2022 3:00 PM
89	Location of the trails as well as usability	2/22/2022 10:13 AM
90	I think Conway will be most successful in attracting residents and visitors to its trail system if it can adequately give people the experience of being deep into the woods or nature, and give people that feeling of serenity and awe that only comes with the feeling of being immersed in the untouched world.	2/22/2022 9:49 AM
91	Accessibility	2/22/2022 9:35 AM
92	Use and dedication by city to finish	2/22/2022 9:27 AM
93	Increased use by a wide demographic	2/21/2022 7:26 PM
94	Keeping them safe. Well lit and keeping the leisurely feel of Conway. I would love to see fishing spots. Also places to sit for those elderly that still like to be out in nature. Keep the small town feel	2/21/2022 4:34 PM
95	High quality, easy to access trails that are beautiful and nice to use.	2/21/2022 10:02 AM
96	Ease of use for many people, sustainability, embracing change	2/21/2022 9:44 AM
97	Proper use of funds, and making smart decisions on designing.	2/21/2022 8:35 AM
98	Number of users	2/21/2022 8:03 AM
99	Continuity, and accessibility!	2/20/2022 10:05 PM
100	As a resident of City of Conway (near Collins Park), I'd consider the trails a success when I could safely run 6-8 miles from my house on sidewalks and trails, through some nature, without needing to run next to major roads. Also, when my husband can safely commute to CCU via bike or running. Conway should be a premier destination for young, fitness conscious families and individuals. Many in their 20s-40s want a town where they can safely commute by biking, running, or walking. If you can attract these communities to downtown Conway, they will spend money downtown (especially at good bars and restaurants), and might choose to live in Conway. Conway should be looking at cities like Knoxville, TN which has 80+ miles of greenway (and a river like Conway!). Many of their trails follow rail paths and connect communities and businesses.	2/20/2022 8:27 PM
101	Once built, must be maintained	2/20/2022 7:24 PM
102	Trails that respect our natural environment and feel safe to use.	2/20/2022 5:40 PM
103	Better trail systems and up keep.	2/20/2022 11:25 AM
104	Longer trails. There are several short trails but nothing with substantial distance.	2/19/2022 7:14 PM
105	advertising throughout the community. Signage.	2/19/2022 4:40 PM
106	Getting a connection across the Waccamaw	2/19/2022 3:34 PM
107	Safe way to bike and walk around the Conway area, and access to good quality trail systems for biking and hiking.	2/19/2022 9:09 AM
108	Extending them	2/19/2022 7:57 AM
109	creating a destination for vacationers	2/19/2022 7:51 AM
110	Safe walking trails with a scenic route	2/19/2022 7:22 AM
111	Lots of connectivity...connection of downtown Conway to CCU...lots of trails in/along natural areas.	2/18/2022 10:51 PM
112	More trails. There is hardly anywhere to walk here. More connectivity.	2/18/2022 10:47 PM
113	Ensuring the areas are safe for all ages. Protecting the environment and wetlands. Connecting Downtown Conway to other areas within the area/county.	2/18/2022 4:37 PM

## Conway Pathways and Trails Plan - Survey No. 1

114	safety	2/18/2022 9:48 AM
115	Conservation of natural areas	2/18/2022 7:04 AM
116	Design and security	2/18/2022 5:41 AM
117	Smoother paths	2/18/2022 12:54 AM
118	Limiting environmental impact during building. Mature trees kept in place, natural features preserved. Nobody likes or wants suburban style sidewalk trails with traffic & noise.	2/17/2022 11:48 PM
119	Less cars on road/more bikes on road/trails/paths	2/17/2022 9:36 PM
120	Thinking of everyone, not just the downtown folks	2/17/2022 8:55 PM
121	Use of trails	2/17/2022 8:25 PM
122	Usage measures	2/17/2022 8:07 PM
123	Need to feel safe.	2/17/2022 7:16 PM
124	Listening to what the locals would like to see.	2/17/2022 7:10 PM
125	Incentives for citizens. Don't make it a tourist destination. There's enough of that in Horry county	2/17/2022 6:55 PM
126	Buy-in...support by leader, community and not being dictated by developers. There needs to be balance or all the work, progress and beauty of the area will be underwater, ruined and wasted	2/17/2022 6:01 PM
127	Easy access to a variety of trails. I'd like to see lake busbee connected to the Riverwalk via a bridge or underpass . Downtown should also be easily accessed	2/17/2022 5:11 PM
128	Bike friendly roads and traffic lights Add kayak racks along the River. Cheap to build and generate income	2/17/2022 5:00 PM
129	Developing something similar to Horry County bike and run park that accommodates walkers runners and bikers thru a preservation area. It can connect other features but there is nothing close in Conway besides main roads. Anything current is very small or low mileage. Area needs something desperately	2/17/2022 3:40 PM
130	Complex original paths. Reference Bentonville, AR	2/17/2022 2:37 PM
131	Using community feedback instead of the decisions of the same old leadership that does whatever they want	2/17/2022 8:17 AM
132	Get more trails connected	2/16/2022 11:24 PM
133	Quality of the trails, benches for resting	2/16/2022 7:40 PM
134	Advertising	2/16/2022 7:21 PM
135	Connection to CCU and HGTC to downtown Conway	2/16/2022 6:19 PM
136	Protect areas and connect the trails so you can ride your bike or walk for more than 1 mile. Parking is a must too if you can't ride your bike to the destination.	2/16/2022 5:48 PM
137	The quality of the trails and the connectivity around town will define its success.	2/16/2022 3:57 PM
138	Keep adding more. I moved here from Raleigh, NC which had an amazing Greenway system. I hate that I left that behind. It is my biggest disappointment in living here.	2/16/2022 3:03 PM
139	safety, a trail for all ages to use.	2/16/2022 2:41 PM
140	Access to entry points and long enough distance/good enough maintenance to bring in repeat users	2/16/2022 2:30 PM
141	Year round, use and access to trials	2/16/2022 2:28 PM
142	Local support and financial backing	2/16/2022 2:15 PM
143	Long-term planning and investment, taking into account weather patterns and flooding, to create high quality infra-structure and amenities that people will want to and be able to use for years.	2/16/2022 11:10 AM

# Conway Pathways and Trails Plan - Survey No. 1

144	Ride from downtown conway to the beach.	2/16/2022 11:00 AM
145	Able to access all areas in Conway by trails	2/16/2022 10:40 AM
146	1. For the community to buy-in on such a plan. 2. For the community to use such a plan 3. For money to be spent on keeping those paths well-lit and patrolled at night so that the area is safe. 4. To not let the immediate cost discourage building these pathways the RIGHT way. Too often, I believe the county and municipalities half-do a project because of a budget shortfall...budgets are important, absolutely, but something that could provide so much opportunity such as this, should be done the correct way!	2/16/2022 9:00 AM
147	Connecting to the beach	2/16/2022 8:42 AM
148	reaching areas in and Conway and Horry County	2/16/2022 8:12 AM
149	Building them soon	2/16/2022 8:11 AM
150	Ease of access, trail maintenance and variety	2/16/2022 6:22 AM
151	N/a	2/16/2022 6:14 AM
152	this survey is extremely well put together and so is the website with the pathways. continued involvement of the community is very important and should continue.	2/15/2022 7:30 PM
153	Ability to connect to Conway with paved loops or trails and widening roadways to include shoulders and designated bike lanes. Roads are way to dangerous to ride a bike to reach Conway.	2/15/2022 7:22 PM
154	Use	2/15/2022 5:27 PM
155	Conservation and Connection. Conserve land from overdevelopment and connecting Conway to the grand strand area by giving us an option that doesn't require sitting in our cars on 501.	2/15/2022 3:17 PM
156	Well maintained trails with varying distance options.	2/14/2022 6:26 PM
157	Access to nature with places to picnic and maybe enjoy some music. Connection to all that makes Conway great!	2/14/2022 3:21 PM
158	Extending Crabtree Canal and expanding Crabtree Canal walkway and providing floodplain protection. Also, would love to see more native, wild gardens and natural areas along trails. Providing a nature trail along tributaries like Crabtree that connect North Conway to downtown Conway.	2/14/2022 3:00 PM
159	Making Conway bike friendly	2/14/2022 2:57 PM
160	Increased connectivity along the river, And linking a joining communities	2/14/2022 2:34 PM
161	Protection of the natural beauty of conway and being able to connect around Conway in these trails	2/14/2022 1:42 PM



## Q16 Please share any other thoughts or comments about the planning process or Conway's future system of pathways and trails.

Answered: 105 Skipped: 105

#	RESPONSES	DATE
1	To promote access and ease of use, please have educational classes for kids, teens, and adults. This will get people involved and promote usage of the trails and support eco-tourism. I drive long distances to take part in the offerings of the state parks at the beach (nature walks, horseback riding, etc.) Let's bring this programming to Conway.	4/25/2022 12:49 PM
2	Would love to have loop pathways/trails. Do not personally care for out-and-back trails. Would like to see a bike/pedestrian trail connecting downtown to HGTC and CCU.	4/24/2022 10:06 PM
3	Thank you for considering the public's opinion	4/22/2022 9:46 AM
4	Thank you for reaching out to the community	4/21/2022 8:05 PM
5	Very excited about the direction that downtown Conway is going in.	4/21/2022 4:28 PM
6	Connected trails to offer longer running trails would be great!	4/21/2022 12:57 PM
7	Conway does not have enough trails and many of these trails are underwater for a good part of the year. Paved bike trails or higher boardwalk trails would prevent this issue and allow for full time use	4/21/2022 9:09 AM
8	Connecting to other towns is not as important as accessibility from greater Conway	4/21/2022 8:52 AM
9	is there an opportunity to incorporate trails and/or sidewalks within more zoning regs? i think the community needs to be more informed so they can get excited about this. . . broadcast it everywhere and have lots of images. i honestly wasn't even aware the city had as many trail opportunities as we do until this survey.	4/20/2022 3:26 PM
10	I've seen lots of trails mapped/planned on paper but very few actually get developed in the 15 years I've lived here	4/20/2022 2:44 PM
11	Of course the dangerous driving habits of local motorists need to be taken into account.	3/9/2022 4:28 PM
12	An ideal trail would be something similar to Johnson City, TN's Tweetsie Trail. Perfect for walking, running, biking, nature, and connecting communities. Includes fitness areas, wildlife viewing, and local businesses on route.	3/9/2022 10:10 AM
13	I hope Conway will appreciate Lake Busbee for what it currently is. I call myself a birder, and Busbee is a birding HotSpot according to eBird.org (site maintained by Cornell U). Birders from across the 2 Carolinas, as well as from afar, have visited this site recently to observe the wide variety of birds that frequent these marshy wetlands. Birders have been generally recognized as being affluent and educated - people who would travel to observe and find downtown Conway a nice place for dining and shopping.	3/9/2022 6:41 AM
14	We need way more nature parks for the community to be able to gather in. Sherwood Park is a perfect example. If it had a trail leading to another play ground and there were bike paths on the roads from neighborhoods or other bike paths on main roads these spaces would be used so much more. Look at Portland, Oregon as an example of how we could utilize our natural resources and not just cut down all of our natural tree and green space. We love Elm St. with all of the shade from the old growth trees but we live in Rivertown Row with sidewalks but we can't leave our neighborhood without a car to reach anything else in the city.	3/8/2022 10:12 AM
15	It would be great to have designated paths from each trail so you do not have to get back in a vehicle to get from one trail to the other.	3/7/2022 12:33 PM
16	Would love to see more trails and playgrounds near Wild Wing and CCU.	3/7/2022 12:19 PM
17	Additional and improved Music and Arts "stages" that would bring more people in would support local business. The "stage" seems to only be used once a year for the "4th". Music and nature	3/7/2022 12:17 PM

## Conway Pathways and Trails Plan - Survey No. 1

	have always done well together.	
18	Enjoying the beautiful river	3/7/2022 10:47 AM
19	I would love to be able to travel to a stores up 701 on a bike trail / sidewalk. I also would love to be able ride my bike to Coastal Carolina without having to fear for my life on 501 Business! Bike lane would still be frightening on that road, a separate bike path separate from the road would be awesome.	3/1/2022 7:16 PM
20	Connections will be the best benefit. Remove Sherwood road fill and create a recreational bridge instead- connecting Country club (need sidewalks/bike lanes there)- obtain the old country club property and create a nature park/recreation park that can be accessed via bike or on foot. - connect to the future park/wetland feature going behind Trinity Methodist church. extend Crabtree trail, restore the Crabtree swamp (remove berm fill, build walking bridges. use trails for education and to provide wildlife resources (native plants)	3/1/2022 12:20 AM
21	I didn't even know there was a Kayak path.	2/24/2022 9:36 PM
22	Trails away from motor vehicles for safer family use	2/24/2022 8:06 PM
23	NA	2/24/2022 7:03 PM
24	Need to have safe pathways and trails connecting the rest of Horry County with Conway including CCU & HGTC.	2/24/2022 5:29 PM
25	Why does Myrtle Beach get every road improvement it want and Conway gets no improvements? At some point, something has to be done about traffic	2/24/2022 4:18 PM
26	Will pathways & trails be available 24 hours a day OR will they close at Dark and open at daylight.	2/24/2022 2:33 PM
27	Well-maintained paved or dirt trails would be an improvement. Something needs to be within the City limits where youth out riding their bikes could access easily. Children don't have a lot of places to go out and ride their bikes with friends.	2/24/2022 2:20 PM
28	Look at the hulk in Myrtle Beach. It is visited by many people outside of the area.	2/24/2022 2:05 PM
29	Trail to myrtle beach, east coast greenway	2/24/2022 12:16 PM
30	We need a short path created between Pecan Grove / Sedgefield and the sidewalk in front of the Foodlion on 701 s...	2/24/2022 9:02 AM
31	I live just a short 3 miles outside of town but riding a bike or even walking is very dangerous on the roads as they are today. I would so enjoy being able to ride into town for lunch or to meet friends, even just exercise, but only if safety was first. I realize this means addressing the storm ditch issue and lack of shoulders on the roads but adding a bike path would not only benefit riders, but it gives drivers room for error that they don't have today. Our roads are very dangerous, for walkers, runners, bikers AND drivers.	2/24/2022 7:55 AM
32	I would love to see more sidewalk on roads and make Conway more bicycle and pedestrian friendly. Also, I would love to see trails that connect HGTC or CCU with the rest of Conway. I feel like this will allow college students more access to the rest of Conway and create more economic benefits for local businesses.	2/24/2022 12:49 AM
33	I hope this is just the beginning of building something really wonderful in town	2/23/2022 9:31 PM
34	We are avid hikers and bicyclist and have travelled across the country to utilize trail systems. All are unique because off where they are. Conway has a distinct history as does Myrtle Beach and the whole of Horry County. This history and the landscape should be taken into account when designing this trails system. Also, collaboration with Aynor, Loris, NMB, Myrtle Beach, and even Surfside will ensure that the Conway trail system is just the beginning of a county wide system that sets an example for the state and country. A county wide system could bring a whole new type of tourism to Horry County, there are actually many people that travel with the purpose of riding and hiking trails. Horry County could have some of those trails that people travel to.	2/23/2022 5:19 PM
35	We need a few things that will increase use of existing walking areas. A- fix many curb cuts that hold water for 4 days after a rain B- connect sidewalks to avoid walking in the street on high traffic routes ex: kingston lake drive bottom, west side of Elm street near 9th, etc C- fix water draining across road Kingston Lake Drive bottom D- add public parking at Coastal for	2/23/2022 2:41 PM

## Conway Pathways and Trails Plan - Survey No. 1

walking the campus E- add caution lights or 4 way stops as necessary on busy streets ex 9th Ave F- take over railroad trestle at Kingston Lake for pedestrian walking

36	If Conway wants to link to CCU, there needs to be recreational and easy access plans between the city and Hwy. 544	2/23/2022 12:30 PM
37	Please build a park and trails down 701 South near Bucksport.	2/23/2022 11:26 AM
38	Conway must maintain relevancy, and access to trails and access to downtown will help boost local commerce and keep more businesses open.	2/23/2022 10:28 AM
39	Shared roadways with bikes is a disaster in this area, if you are going to consider bike paths then make them stand alone paths that are not part of the roadway system.	2/23/2022 10:27 AM
40	Thanks for making trails a priority	2/23/2022 9:17 AM
41	I would like to see a path along the railroad tracks and or one off Rt. 501. It would be great to go from Conway to Carolina Forest via a trail or path.	2/23/2022 8:08 AM
42	Nature trails give a feeling of decreased stress while promoting good health, happiness and may lead to less crime.	2/22/2022 10:22 PM
43	My wife and I went on the trail with disc golf. It was so bad, I hoped we didn't run into anyone in there while we were walking. Very unsafe.	2/22/2022 10:06 PM
44	I'm so thankful you are considering this. Getting outside is so important and this would help people to exercise as well as make it easier to get from place to place. Having longer trails that went in a loop would be great. Some have great potential, but are poorly maintained and people don't know about them	2/22/2022 9:33 PM
45	The most important trail will be a walking/biking path that connects downtown Conway with downtown Myrtle Beach. Ideally following the railroad.	2/22/2022 8:38 PM
46	More trails along the river	2/22/2022 7:56 PM
47	Sidewalks where there are none would be more important than trails or bike paths.	2/22/2022 5:12 PM
48	Send out flyers to all residents where these trails are. More people would use them. With today's inflation, no one says no to free exercise and fun!	2/22/2022 5:08 PM
49	A bike/multi-purpose path to connect Conway would be invaluable to our community.	2/22/2022 4:58 PM
50	We could use two more swimming pools in cobwsy	2/22/2022 4:32 PM
51	Question 19 is crap. There is no such thing as "nonbinary" or "other". If you don't think you are your birth sex you are simply mentally ill. Sorry.	2/22/2022 4:27 PM
52	More Bike Lanes if possible	2/22/2022 4:18 PM
53	Establishing safe alternatives to driving is a win for everyone and everything in Conway!	2/22/2022 4:12 PM
54	Have a safe path down hwy 90 would connect to international which would connect to anywhere in Myrtle Beach.	2/22/2022 3:00 PM
55	we need to share this information to all areas of our community and in multiple outlets.	2/22/2022 10:13 AM
56	I live in Myrtle Beach, but Conway is blessed with the Waccamaw River and plenty of large, woodsy natural areas. Myrtle Beach has none of that and it sucks. I would come to Conway more often just to experience good trails.	2/22/2022 9:49 AM
57	Parking must be available or use will not be optimized	2/21/2022 7:26 PM
58	Remember were our community came from. We started as a farming and lumber community based on family values	2/21/2022 4:34 PM
59	Knoxville TN has a great, easy to access city-wide green trails system. You should check it out!	2/21/2022 10:02 AM
60	I am excited! Conway has such potential. Thank you.	2/21/2022 9:44 AM
61	Thanks for asking them community!	2/20/2022 10:05 PM
62	We are so glad to see you prioritizing this. Please continue to do so. We love living in Conway,	2/20/2022 8:27 PM

## Conway Pathways and Trails Plan - Survey No. 1

but one of the biggest drawbacks to us is the lack of safely bikeable paths and the lack of a running community, largely due to the lack of trails. We've enjoyed running on the trails we do have, but they aren't enough. Crabtree is nice but very short for a run. The two Waccamaws are often flooded and the terrain can be tricky for running. It's nice to run through the neighborhood downtown, and the Riverwalk, but Conway needs much more than this!

63	Use city employees to maintain trail system	2/20/2022 7:24 PM
64	Having used the kayak launch at Crabtree off of North Main St., I feel that the launch needs much improvement. The area was steep and covered in clay. I slipped trying to launch, and then that area of the canal was so shallow that we had to walk it. It may be necessary to dredge or move the launch to another location. It would be great to have a dock for launching kayaks. I would also love to see the Riverwalk extended to the Kingston Lake area.	2/20/2022 5:40 PM
65	The waccamaw River trails need wooden pathways to keep from flooding and to where it can be used after a rain	2/20/2022 11:25 AM
66	Need longer and more public trails in nature.	2/19/2022 7:14 PM
67	Putting sidewalks or bike paths immediately next to highways (as in road, curb, sidewalk) is a bad idea, there should be a separation of a few feet. I ride and walk down 4th Ave. and when a large truck passes it is quite scary and startling. Maintenance has to factored into the planning process - for instance, keeping the sidewalks/trails maintained and clear overhead obstructions.	2/19/2022 9:09 AM
68	road infrastructure would also have to be improved in addition due to increased traffic that this endeavor would bring	2/19/2022 7:51 AM
69	I think there are plans for bike lanes in all the Ride III build out, but it would be smart to have dedicated trails, and not just bike lanes, through all those new transportation corridors.	2/18/2022 10:51 PM
70	Anything will be better than what we have now which is so incredibly limited.	2/18/2022 10:47 PM
71	Please look at other cities' successful use of pathways and trails. We are so far behind in this area.	2/18/2022 5:11 PM
72	I'm excited for the growth and development that's happening in the downtown area and around Conway. In my time here (12 years) I've seen so many changes and they're all for the better.	2/18/2022 4:37 PM
73	paths along roads for walking / biking safely	2/18/2022 9:48 AM
74	Thank you	2/18/2022 7:04 AM
75	Thank you for taking the initiative to address this much needed project	2/18/2022 5:41 AM
76	Just don't worry about it. Start to improve and budget for all other departments first. Lower our taxes first instead of being money hungry. Stop building houses in Conway. Keep it a small town.	2/18/2022 12:54 AM
77	More attention to kayak/canoe needs and soft launch access to a Nationally recognized blue trail & ADA kayak launch. Benches & seating are critical need. We just spent over \$700 and traveled 300 miles to paddle a blue trail that has riverside camping and restrooms with potable water. Water bottle refilling stations & a bike repair area with air pump are great additions.	2/17/2022 11:48 PM
78	Look at good examples from other areas. Example- the Rail Trail in Orange County NY	2/17/2022 8:55 PM
79	Keep this as a high priority, but road infrastructure is a basis for getting to trails for most people so it can't be sacrificed as part of a trails plan.	2/17/2022 8:07 PM
80	I have been looking forward to this for years. I think it's a great idea!	2/17/2022 7:10 PM
81	Perhaps plan trails around historic properties or ancient Indian Trails. Place educational information along the trail.	2/17/2022 6:55 PM
82	incorporate the natural beauty of the area, don't bulldoze it to "start over"	2/17/2022 6:01 PM
83	Get started	2/17/2022 5:00 PM
84	I've been hoping Conway would be able to utilize some green space to create a trail system somewhere in the area similar to the bike and run park that accommodates so many.	2/17/2022 3:40 PM
85	Soft trails with bike features, integrate travel/commute, into regular fitness and tourism	2/17/2022 2:37 PM

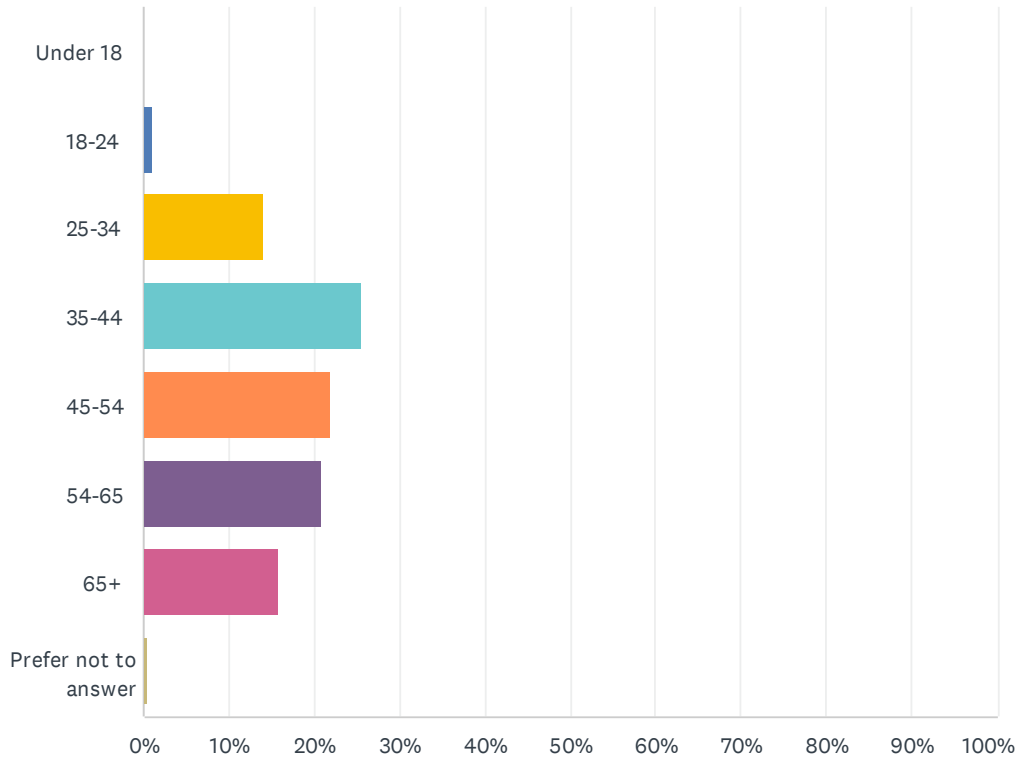


# Conway Pathways and Trails Plan - Survey No. 1

	opportunities	
86	Huge concerns about the safety or the perception of safety on these trails to Downtown Conway. Homeless problems and the folks walking the train tracks come to mind immediately. No one will use these connecting trails if they don't feel safe.	2/17/2022 8:17 AM
87	Love the idea of connecting Coastal to downtown Conway	2/16/2022 11:24 PM
88	No other thoughts	2/16/2022 7:21 PM
89	This could bridge the gap in the growing the connection with the college campus and Conway.	2/16/2022 6:19 PM
90	I am super excited to see more trails along scenic routes to enjoy nature by walking or biking to destinations.	2/16/2022 5:48 PM
91	I own two vehicles, but would love to ride my bike to work. There are no bike lanes in Conway. There are some sidewalks, but they do not connect. I really enjoy kayaking in Conway and appreciate immensely the launch by the marina. The River Walk is gorgeous also and a family favorite. The Waccamaw River Park is nice, but floods too much. I enjoy the Crabtree trail, but the trail is in disrepair. The concrete is busted up. It would be better off with some leveled soft/dirt surface. The same is true for Lake Busbee. I would love more opportunities to experience more trails through nature in Conway. Time in nature is therapeutic and healing. I believe it would improve the quality of life in our community.	2/16/2022 3:57 PM
92	This is the first I've heard about this initiative. I am on social media and live in downtown Conway. Spread the word and more people will get involved.	2/16/2022 3:03 PM
93	Conway has a great road bike route currently in place. However safety on these roads needs to be considered for cyclist such as a side roads through busy areas such as Long Ave Ext. There should also be off road trails for bikes of all types and all levels of bikers.	2/16/2022 2:41 PM
94	Safety and infrastructure to support increased use of trails and natural resources, while protecting them, should be emphasized. Local needs and wants should be prioritized over those of tourists.	2/16/2022 11:10 AM
95	Require bike lanes or larger shoulders on all new road work.	2/16/2022 11:00 AM
96	I've always thought Conway/Horry County should apply for the Rails to Trails Grant the Federal Government offers. I know the railroad has been purchased and that would take some buy-in from the owner, but that could make a HUGE difference in the County! Take a look at the Swamp Rabbit Trail and Falls Park in Greenville, SC. It took a lot of time and even more money, but locals and visitors alike use those trails and parks! They are as beautiful as they are safe!	2/16/2022 9:00 AM
97	Make everything connect	2/16/2022 8:42 AM
98	You might consider contacting the East Coast Greenway organization for information on bike paths.	2/16/2022 8:12 AM
99	Please do this before we leave the area	2/16/2022 8:11 AM
100	N/a	2/16/2022 6:14 AM
101	before coming to beautiful Horry County, i lived in Virginia where there is a huge focus on trails, pathways, and safe sidewalks where people can walk run bike and commute to work. please incorporate sidewalks into your pathway plan. there are many roads in Conway like Singleton Ridge Road which needs a sidewalk so that people can walk to the store for the many medical offices on that road. Using this is an example if you incorporate sidewalks you will find you can connect many of the beautiful natural pathways together and people will become accustomed to walking along the roadways as long as it is a safe experience.	2/15/2022 7:30 PM
102	Long overdue. We value tourism here but don't focus enough on connections to areas with trail and paved connections, non motorized.	2/15/2022 7:22 PM
103	Easy access. Good parking.	2/15/2022 5:27 PM
104	Would love a bike trail along the railway leading from CCU to Carolina Forest Blvd (they are nearing completion of a bike path on CF Blvd, should connect to it!) Could potentially reduce traffic on 501, and I could bike to school and/or other commercial developments near 501/Postal Way	2/15/2022 3:17 PM
105	It is a terrific idea	2/14/2022 2:34 PM

## Q17 What is your age?

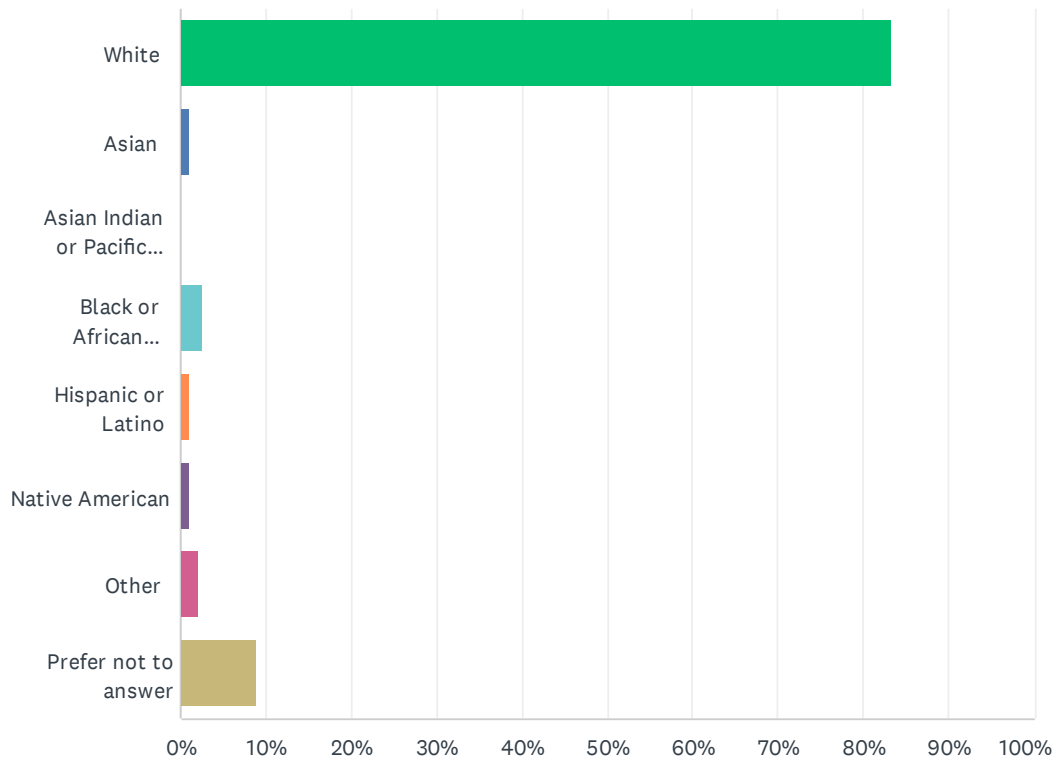
Answered: 191   Skipped: 19



ANSWER CHOICES	RESPONSES	
Under 18	0.00%	0
18-24	1.05%	2
25-34	14.14%	27
35-44	25.65%	49
45-54	21.99%	42
54-65	20.94%	40
65+	15.71%	30
Prefer not to answer	0.52%	1
<b>TOTAL</b>		<b>191</b>

## Q18 What race/ethnicity do you consider yourself to be?

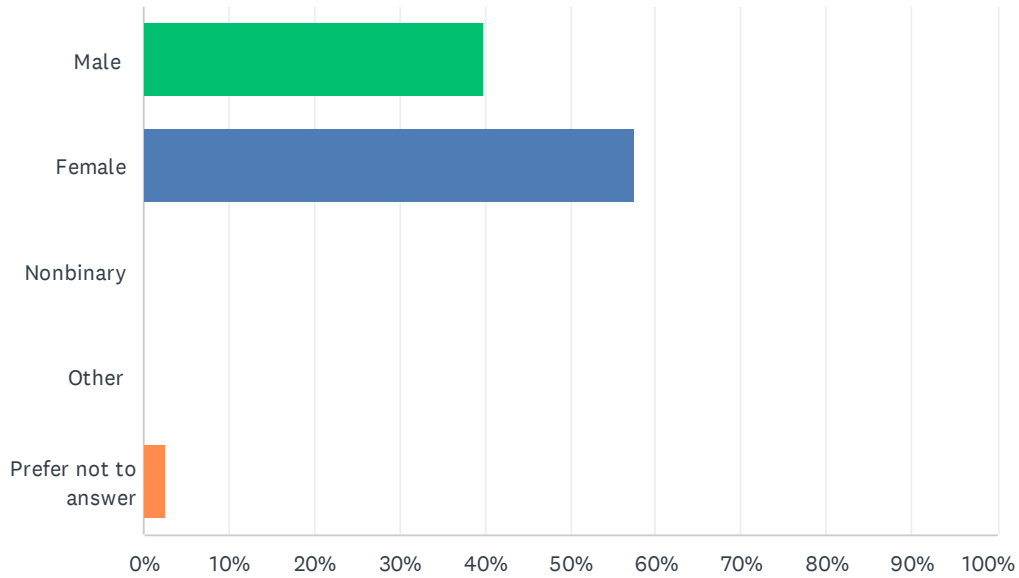
Answered: 192    Skipped: 18



ANSWER CHOICES	RESPONSES	
White	83.33%	160
Asian	1.04%	2
Asian Indian or Pacific Islander	0.00%	0
Black or African American	2.60%	5
Hispanic or Latino	1.04%	2
Native American	1.04%	2
Other	2.08%	4
Prefer not to answer	8.85%	17
<b>TOTAL</b>		<b>192</b>

## Q19 What gender do you consider yourself to be?

Answered: 191 Skipped: 19



ANSWER CHOICES	RESPONSES	
Male	39.79%	76
Female	57.59%	110
Nonbinary	0.00%	0
Other	0.00%	0
Prefer not to answer	2.62%	5
TOTAL		191



## Q20 What is your home ZIP Code? (example: 29526)

Answered: 191 Skipped: 19

#	RESPONSES	DATE
1	29526	4/25/2022 12:49 PM
2	29526	4/24/2022 10:07 PM
3	29577	4/22/2022 9:47 AM
4	29526	4/21/2022 11:41 PM
5	29577	4/21/2022 10:20 PM
6	29588	4/21/2022 8:05 PM
7	29575	4/21/2022 7:26 PM
8	29526	4/21/2022 4:28 PM
9	29591	4/21/2022 4:27 PM
10	29575	4/21/2022 2:17 PM
11	29579	4/21/2022 2:08 PM
12	29579	4/21/2022 12:58 PM
13	29526	4/21/2022 12:05 PM
14	29526	4/21/2022 11:49 AM
15	29576	4/21/2022 10:11 AM
16	29526	4/21/2022 9:13 AM
17	29526	4/21/2022 9:09 AM
18	29588	4/21/2022 8:54 AM
19	29588	4/21/2022 8:54 AM
20	29526	4/21/2022 8:52 AM
21	29526	4/21/2022 8:32 AM
22	29527	4/20/2022 4:07 PM
23	29526	4/20/2022 3:27 PM
24	29526	4/20/2022 2:57 PM
25	29527	4/20/2022 2:45 PM
26	29527	3/9/2022 4:28 PM
27	29588	3/9/2022 4:23 PM
28	29526	3/9/2022 10:11 AM
29	29526	3/9/2022 9:19 AM
30	29526	3/9/2022 6:42 AM
31	29576	3/8/2022 2:21 PM
32	29526	3/8/2022 10:12 AM
33	29526	3/7/2022 10:36 PM

# Conway Pathways and Trails Plan - Survey No. 1

34	29526	3/7/2022 8:50 PM
35	29526	3/7/2022 6:53 PM
36	29526	3/7/2022 2:24 PM
37	29526	3/7/2022 1:17 PM
38	29526	3/7/2022 1:06 PM
39	29526	3/7/2022 12:34 PM
40	29526	3/7/2022 12:33 PM
41	29526	3/7/2022 12:19 PM
42	29526	3/7/2022 12:17 PM
43	29526	3/7/2022 11:59 AM
44	29526	3/7/2022 11:40 AM
45	29526	3/7/2022 10:48 AM
46	29568	3/2/2022 1:30 PM
47	29526	3/1/2022 7:17 PM
48	29526	3/1/2022 12:20 AM
49	29527	2/27/2022 10:21 AM
50	29577	2/26/2022 9:16 AM
51	29526	2/25/2022 3:50 PM
52	29526	2/24/2022 9:37 PM
53	29579	2/24/2022 8:06 PM
54	29526	2/24/2022 7:04 PM
55	29526	2/24/2022 5:37 PM
56	29579	2/24/2022 5:30 PM
57	29526	2/24/2022 5:13 PM
58	29527	2/24/2022 4:19 PM
59	29526	2/24/2022 2:52 PM
60	29527	2/24/2022 2:33 PM
61	29526	2/24/2022 2:20 PM
62	29577	2/24/2022 2:05 PM
63	29511	2/24/2022 1:24 PM
64	29577	2/24/2022 12:16 PM
65	29544	2/24/2022 10:40 AM
66	29527	2/24/2022 9:03 AM
67	29526	2/24/2022 7:55 AM
68	29526	2/24/2022 4:52 AM
69	29526	2/24/2022 12:50 AM
70	29526	2/23/2022 10:17 PM
71	29526	2/23/2022 9:31 PM

# Conway Pathways and Trails Plan - Survey No. 1

72	29527	2/23/2022 5:19 PM
73	28109	2/23/2022 4:18 PM
74	29526	2/23/2022 4:15 PM
75	29526	2/23/2022 2:41 PM
76	29527	2/23/2022 1:35 PM
77	29527	2/23/2022 12:30 PM
78	29526	2/23/2022 12:17 PM
79	29527	2/23/2022 11:26 AM
80	29526	2/23/2022 11:21 AM
81	29527	2/23/2022 11:14 AM
82	29566	2/23/2022 10:49 AM
83	29526	2/23/2022 10:28 AM
84	29527	2/23/2022 10:28 AM
85	29526	2/23/2022 9:47 AM
86	29526	2/23/2022 9:36 AM
87	29626	2/23/2022 9:35 AM
88	29526	2/23/2022 9:17 AM
89	29526	2/23/2022 8:47 AM
90	29527	2/23/2022 8:45 AM
91	29526	2/23/2022 8:08 AM
92	29526	2/23/2022 7:38 AM
93	29527	2/23/2022 7:25 AM
94	29526	2/23/2022 5:58 AM
95	29577	2/22/2022 10:37 PM
96	29566	2/22/2022 10:22 PM
97	29526	2/22/2022 10:06 PM
98	29526	2/22/2022 9:33 PM
99	29526	2/22/2022 8:38 PM
100	29526	2/22/2022 8:15 PM
101	29526	2/22/2022 7:56 PM
102	29526	2/22/2022 5:36 PM
103	29526	2/22/2022 5:12 PM
104	29527	2/22/2022 5:08 PM
105	29526	2/22/2022 4:58 PM
106	29526	2/22/2022 4:32 PM
107	29526	2/22/2022 4:27 PM
108	29526	2/22/2022 4:18 PM
109	29526	2/22/2022 4:12 PM

# Conway Pathways and Trails Plan - Survey No. 1

110	29526	2/22/2022 3:01 PM
111	29526	2/22/2022 11:17 AM
112	29526	2/22/2022 10:18 AM
113	29572	2/22/2022 9:49 AM
114	29526	2/22/2022 9:36 AM
115	29526	2/22/2022 9:27 AM
116	29526	2/21/2022 7:26 PM
117	29579	2/21/2022 5:48 PM
118	29527	2/21/2022 4:35 PM
119	29526	2/21/2022 10:02 AM
120	29526	2/21/2022 9:45 AM
121	29526	2/21/2022 8:05 AM
122	29526	2/20/2022 10:05 PM
123	29526	2/20/2022 8:27 PM
124	29571	2/20/2022 7:24 PM
125	29526	2/20/2022 5:41 PM
126	29527	2/20/2022 11:25 AM
127	29526	2/19/2022 7:15 PM
128	29526	2/19/2022 4:40 PM
129	29526	2/19/2022 3:34 PM
130	29527	2/19/2022 9:10 AM
131	29440	2/19/2022 7:57 AM
132	29526	2/19/2022 7:52 AM
133	29527	2/19/2022 7:23 AM
134	29527	2/18/2022 10:52 PM
135	29527	2/18/2022 10:48 PM
136	29526	2/18/2022 5:12 PM
137	29526	2/18/2022 4:37 PM
138	29526	2/18/2022 9:49 AM
139	29526	2/18/2022 9:04 AM
140	29527	2/18/2022 7:04 AM
141	29526	2/18/2022 5:41 AM
142	29526	2/18/2022 12:54 AM
143	29588	2/17/2022 11:48 PM
144	29526	2/17/2022 9:37 PM
145	29527	2/17/2022 8:56 PM
146	29526	2/17/2022 8:25 PM
147	29527	2/17/2022 8:07 PM



# Conway Pathways and Trails Plan - Survey No. 1

148	29526	2/17/2022 7:16 PM
149	29526	2/17/2022 7:10 PM
150	29526	2/17/2022 6:55 PM
151	29544	2/17/2022 6:02 PM
152	29511	2/17/2022 5:36 PM
153	29527	2/17/2022 5:11 PM
154	29527	2/17/2022 5:00 PM
155	29526	2/17/2022 3:40 PM
156	29526	2/17/2022 3:02 PM
157	29544	2/17/2022 2:37 PM
158	29579	2/17/2022 8:18 AM
159	29526	2/16/2022 11:25 PM
160	29526	2/16/2022 7:40 PM
161	29526	2/16/2022 7:21 PM
162	29526	2/16/2022 6:19 PM
163	29526	2/16/2022 5:49 PM
164	29526	2/16/2022 3:57 PM
165	29526	2/16/2022 3:03 PM
166	29526	2/16/2022 2:41 PM
167	29526	2/16/2022 2:31 PM
168	29526	2/16/2022 2:28 PM
169	29526	2/16/2022 2:16 PM
170	29526	2/16/2022 11:11 AM
171	29526	2/16/2022 11:00 AM
172	29526	2/16/2022 10:40 AM
173	29566	2/16/2022 9:04 AM
174	29579	2/16/2022 9:00 AM
175	29579	2/16/2022 8:43 AM
176	29579	2/16/2022 8:12 AM
177	29579	2/16/2022 8:11 AM
178	29576	2/16/2022 6:23 AM
179	29526	2/16/2022 6:23 AM
180	29526	2/16/2022 6:15 AM
181	29526	2/15/2022 7:30 PM
182	29526	2/15/2022 7:22 PM
183	29526	2/15/2022 5:27 PM
184	29579	2/15/2022 3:17 PM
185	29527	2/14/2022 6:27 PM

# Conway Pathways and Trails Plan - Survey No. 1

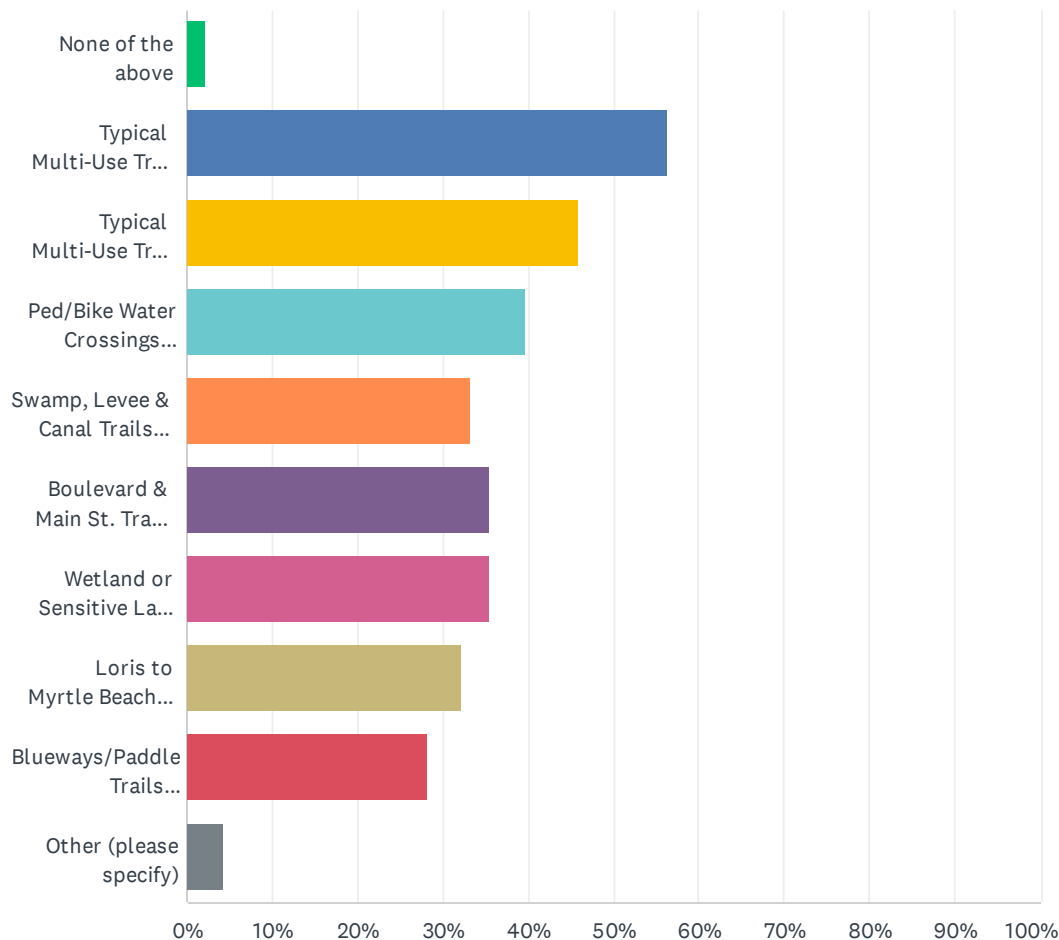
186	29526	2/14/2022 6:25 PM
187	29526	2/14/2022 3:22 PM
188	29526	2/14/2022 3:00 PM
189	29527	2/14/2022 2:58 PM
190	29526	2/14/2022 2:34 PM
191	29526	2/14/2022 1:42 PM

# SURVEY 2 RESULTS

## Conway Pathways and Trails Plan - Survey No. 2

Q1 Which are your highest priority trail types? (Check all that apply.)

Answered: 96    Skipped: 0



## Conway Pathways and Trails Plan - Survey No. 2

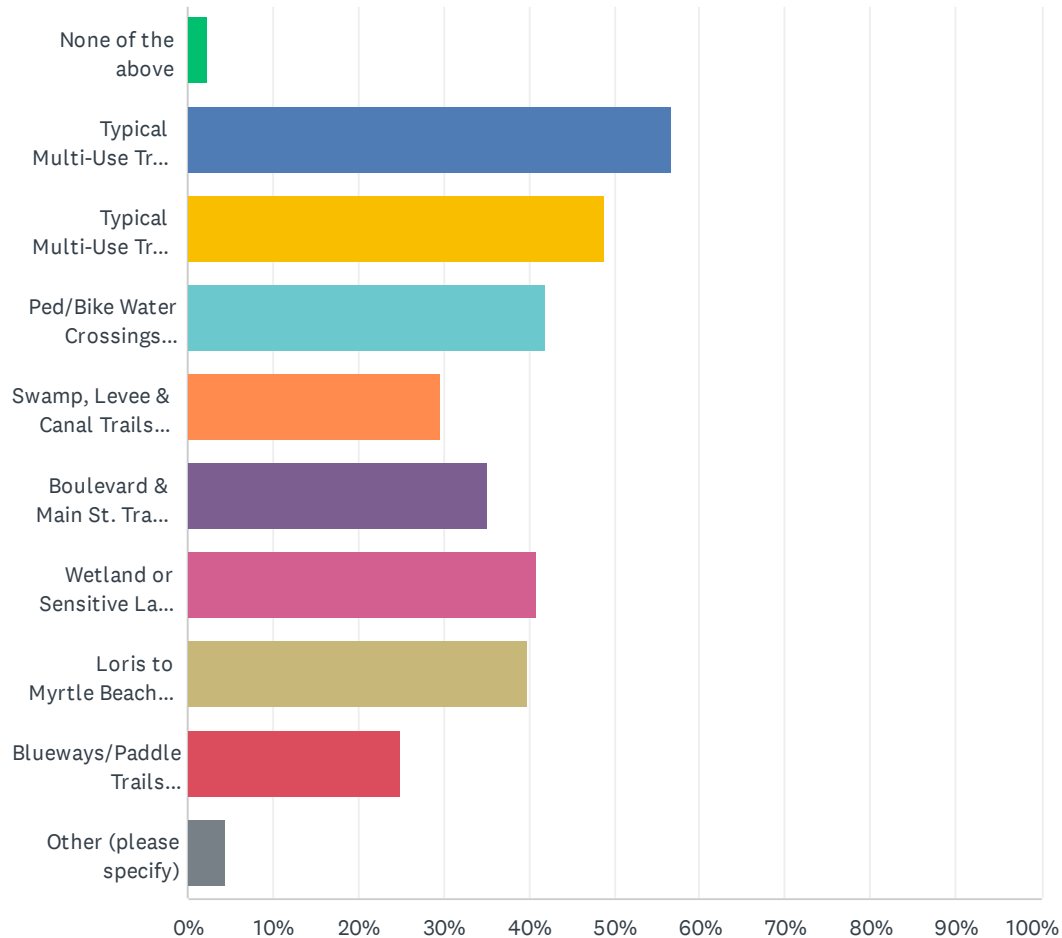
ANSWER CHOICES	RESPONSES	
None of the above	2.08%	2
Typical Multi-Use Trail (Urban/Suburban) [Orange]	56.25%	54
Typical Multi-Use Trail (Rural) [Dark Green]	45.83%	44
Ped/Bike Water Crossings [Black]	39.58%	38
Swamp, Levee & Canal Trails [Yellow]	33.33%	32
Boulevard & Main St. Trails [Brown]	35.42%	34
Wetland or Sensitive Lands Trails [Purple]	35.42%	34
Loris to Myrtle Beach Rail with Trail [Light Green]	32.29%	31
Blueways/Paddle Trails [Dark Blue]	28.13%	27
Other (please specify)	4.17%	4
Total Respondents: 96		

#	OTHER (PLEASE SPECIFY)	DATE
1	Handicapped accessible	5/30/2022 7:07 AM
2	Vote out the current City Council	5/28/2022 10:10 AM
3	Walking trails, access should be a priority, keeping within natural surfaces.	5/17/2022 10:43 AM
4	We need trails of all kind	5/14/2022 12:45 PM



## Q2 Which trail types would you prefer to use both in Conway and to other destinations in the county? (Check all that apply.)

Answered: 88 Skipped: 8



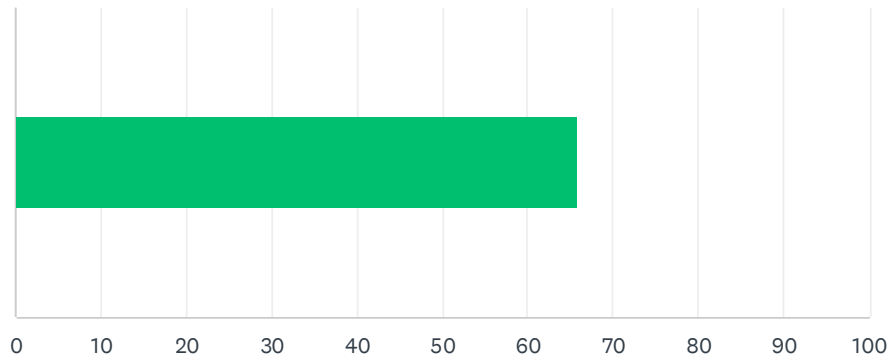
## Conway Pathways and Trails Plan - Survey No. 2

ANSWER CHOICES	RESPONSES	
None of the above	2.27%	2
Typical Multi-Use Trail (Urban/Suburban) [Orange]	56.82%	50
Typical Multi-Use Trail (Rural) [Dark Green]	48.86%	43
Ped/Bike Water Crossings [Black]	42.05%	37
Swamp, Levee & Canal Trails [Yellow]	29.55%	26
Boulevard & Main St. Trails [Brown]	35.23%	31
Wetland or Sensitive Lands Trails [Purple]	40.91%	36
Loris to Myrtle Beach Rail with Trail [Light Green]	39.77%	35
Blueways/Paddle Trails [Dark Blue]	25.00%	22
Other (please specify)	4.55%	4
Total Respondents: 88		

#	OTHER (PLEASE SPECIFY)	DATE
1	I'd love more green spaces where you can sit and enjoy nature	5/28/2022 9:36 PM
2	None, there are too many tourists in Conway now	5/28/2022 10:10 AM
3	Continue river walk down to old water entrance of santee cooper steam plant	5/28/2022 8:52 AM
4	Keeping it natural is vital.	5/17/2022 10:45 AM

**Q3 The Conway Pathways and Trails Plan will recommend priority projects based on community input. Use the slider on the scale to indicate how important a potential pedestrian/bicycle trail crossing over the Waccamaw River would be to you.**

Answered: 82 Skipped: 14



ANSWER CHOICES	AVERAGE NUMBER	TOTAL NUMBER	RESPONSES
	66	5,409	82
Total Respondents: 82			

#		DATE
1	9	6/30/2022 1:50 PM
2	75	6/12/2022 10:05 PM
3	75	6/10/2022 2:23 PM
4	100	6/4/2022 8:27 PM
5	80	6/1/2022 10:48 AM
6	40	5/31/2022 11:13 AM
7	67	5/31/2022 10:39 AM
8	30	5/31/2022 6:36 AM
9	100	5/30/2022 7:38 PM
10	59	5/30/2022 6:59 PM
11	100	5/30/2022 6:07 PM
12	23	5/30/2022 5:19 PM
13	61	5/30/2022 5:10 PM
14	100	5/30/2022 11:51 AM
15	52	5/30/2022 10:56 AM
16	98	5/30/2022 10:06 AM
17	50	5/30/2022 9:23 AM

# Conway Pathways and Trails Plan - Survey No. 2

18	91	5/30/2022 8:09 AM
19	17	5/30/2022 7:09 AM
20	65	5/30/2022 7:04 AM
21	2	5/30/2022 6:35 AM
22	75	5/29/2022 6:58 PM
23	100	5/29/2022 4:23 PM
24	20	5/29/2022 2:42 PM
25	30	5/29/2022 9:06 AM
26	100	5/29/2022 8:15 AM
27	70	5/29/2022 7:00 AM
28	70	5/29/2022 6:59 AM
29	53	5/29/2022 12:47 AM
30	100	5/28/2022 10:43 PM
31	100	5/28/2022 9:57 PM
32	72	5/28/2022 9:37 PM
33	75	5/28/2022 8:59 PM
34	100	5/28/2022 8:36 PM
35	80	5/28/2022 8:29 PM
36	99	5/28/2022 8:20 PM
37	52	5/28/2022 8:16 PM
38	57	5/28/2022 8:13 PM
39	62	5/28/2022 7:22 PM
40	100	5/28/2022 7:06 PM
41	0	5/28/2022 4:31 PM
42	39	5/28/2022 3:30 PM
43	95	5/28/2022 2:56 PM
44	100	5/28/2022 2:54 PM
45	62	5/28/2022 1:13 PM
46	39	5/28/2022 1:10 PM
47	42	5/28/2022 1:01 PM
48	29	5/28/2022 12:36 PM
49	100	5/28/2022 12:34 PM
50	52	5/28/2022 12:10 PM
51	58	5/28/2022 12:09 PM
52	80	5/28/2022 12:09 PM
53	100	5/28/2022 12:08 PM
54	10	5/28/2022 12:04 PM
55	77	5/28/2022 11:52 AM

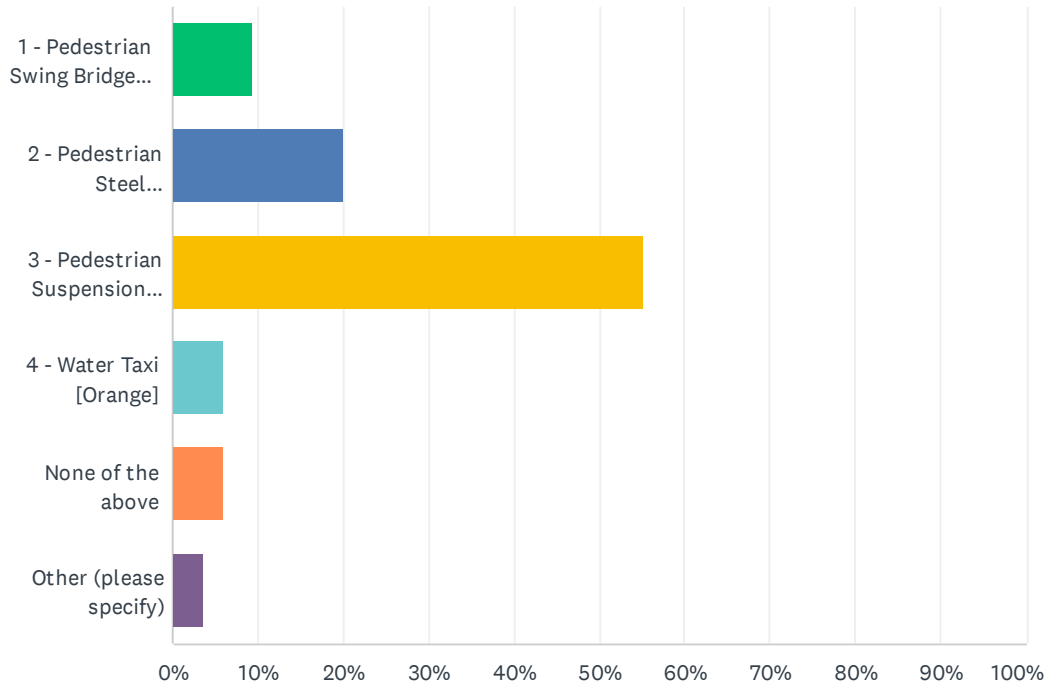


# Conway Pathways and Trails Plan - Survey No. 2

56	100	5/28/2022 11:46 AM
57	99	5/28/2022 11:24 AM
58	100	5/28/2022 10:34 AM
59	100	5/28/2022 10:13 AM
60	0	5/28/2022 10:11 AM
61	100	5/28/2022 10:09 AM
62	51	5/28/2022 10:06 AM
63	44	5/28/2022 9:48 AM
64	42	5/28/2022 9:32 AM
65	70	5/28/2022 9:04 AM
66	71	5/28/2022 8:59 AM
67	100	5/28/2022 8:56 AM
68	27	5/28/2022 8:56 AM
69	97	5/28/2022 8:52 AM
70	0	5/28/2022 8:50 AM
71	100	5/28/2022 8:28 AM
72	84	5/28/2022 8:19 AM
73	100	5/17/2022 11:28 AM
74	100	5/17/2022 10:50 AM
75	71	5/17/2022 8:58 AM
76	100	5/15/2022 6:32 AM
77	100	5/14/2022 1:23 PM
78	100	5/14/2022 12:39 PM
79	1	5/11/2022 2:42 PM
80	10	5/9/2022 10:20 AM
81	50	5/4/2022 11:42 AM
82	0	5/2/2022 11:15 AM

## Q4 Which type of river crossing would you most like to see in Conway? (Select one.)

Answered: 85   Skipped: 11



ANSWER CHOICES	RESPONSES	
1 - Pedestrian Swing Bridge [Dark Blue]	9.41%	8
2 - Pedestrian Steel Arch/Stressed Ribbon with Artistic Lighting [Light Blue]	20.00%	17
3 - Pedestrian Suspension Bridge [Beige]	55.29%	47
4 - Water Taxi [Orange]	5.88%	5
None of the above	5.88%	5
Other (please specify)	3.53%	3
<b>TOTAL</b>		<b>85</b>

#	OTHER (PLEASE SPECIFY)	DATE
1	Any that is best use of budget!	5/31/2022 11:13 AM
2	Turn the existing unused rail bridge over the entrance to Kingston Lake into a pedestrian bridge	5/28/2022 8:16 PM
3	None, there are too many tourists in Conway now	5/28/2022 10:11 AM

## Q5 Do you have any other comments or thoughts for the draft corridors?

Answered: 34   Skipped: 62

#	RESPONSES	DATE
1	No	6/10/2022 2:24 PM
2	Connecting CCU and Downtown, and connecting existing nature preserves and outdoor environments together would be my highest priorities	5/31/2022 11:14 AM
3	none	5/31/2022 6:36 AM
4	No	5/30/2022 10:06 AM
5	Connecting busbee lake area with waccamaw wildlife refuge	5/30/2022 7:06 AM
6	No	5/29/2022 7:00 AM
7	This is very exciting! I'd love to see this happen!	5/28/2022 10:44 PM
8	I would love to see green spaces where you can sit and enjoy nature. Maybe some large boulders that you can sit on or gazebo/picnic tables	5/28/2022 9:38 PM
9	I use a mobility scooter will I have access?	5/28/2022 9:00 PM
10	No	5/28/2022 8:36 PM
11	I love this!!	5/28/2022 8:29 PM
12	No motorcycle please	5/28/2022 8:14 PM
13	Would love more trails. The family use the existing trails but would love to have more!	5/28/2022 7:23 PM
14	No, they are a waste of my tax money	5/28/2022 4:32 PM
15	N	5/28/2022 3:30 PM
16	2nd choice for the previous question would be pedestrian suspension bridge.	5/28/2022 2:57 PM
17	Making roadways with a fatter shoulder and painting a bike symbol there does not make me feel comfortable/safe enough to actually use it.	5/28/2022 1:24 PM
18	I believe cost is very important. You must take into consideration the number of retired and seniors who can not withstand a Tax Increase. We are on fixed incomes and do not get raises as many (government).	5/28/2022 1:15 PM
19	Na	5/28/2022 1:13 PM
20	Connecting or extending the downtown river walk to lake busbee would be great	5/28/2022 12:35 PM
21	No	5/28/2022 12:11 PM
22	I'm just excited this conversation is happening!	5/28/2022 12:11 PM
23	We need something similar to what Greenville has with the Swamp Rabbit Trail, for commuting AND recreation. The fewer cars on the road, the better (and healthier!).	5/28/2022 11:47 AM
24	This is a great idea that will benefit our area in so many ways!	5/28/2022 11:25 AM
25	Nope. Sounds like y'all have really thought this through and are being sensitive to existing resources and not making flooding worse. Thank you for all you do!	5/28/2022 10:35 AM
26	Other than not move forward? No, let's take care of the citizens first	5/28/2022 10:12 AM
27	This is an essential Project for the health and well-being of the community.	5/28/2022 10:10 AM
28	We used to enjoy the busbee trail when it was manicured, but now seems a little unsafe. It's a dead end and with the growth, no one can see to the back from the road. It would be nice to	5/28/2022 9:35 AM

## Conway Pathways and Trails Plan - Survey No. 2

have some emergency poles that contact police along the trail and for the end on the trail to continue over the river to the boat landing, so that there's access from both sides.

29	Conway needs safe pedestrian walkways and trails. Cars often are very aggressive to pedestrians at street crossings in the downtown area	5/28/2022 9:05 AM
30	No	5/28/2022 8:58 AM
31	No	5/15/2022 6:32 AM
32	I believe these projects are vital to the growth and betterment of Horry County, especially Conway.	5/14/2022 1:24 PM
33	Road barrier	5/14/2022 12:40 PM
34	We do not need to add another river crossing. Rather use existing infrastructure (501 business and bypass) to cross versus creating new wetland impacts. Utilize permeable surfaces where possible. Link to CCU, Loris, Myrtle B using rail line (off to side have bike path). An effort to get more cars off our roads should be prioritized this helps with traffic and offsetting for carbon footprint in a changing climate. I think we can prioritize bike-ped flow/connectivity between Conway and adjacent areas while still connecting to natural areas/downtown. Don't see why these need to be separate goals/privatizations.	5/2/2022 11:22 AM

## Q6 Do you have any comments or thoughts for the Plan regarding the Pathway and Trail Design Guidelines?

Answered: 40    Skipped: 56

#	RESPONSES	DATE
1	waste of money find out if anyone would really use it	6/30/2022 1:52 PM
2	No	6/10/2022 2:25 PM
3	no	6/1/2022 10:49 AM
4	Any of these look excellent. The least valuable for cyclists is the painted on the roadway type (as on International Drive in the County). Separate from traffic is safer.	5/31/2022 11:16 AM
5	none	5/31/2022 6:36 AM
6	no	5/30/2022 6:11 PM
7	No	5/30/2022 10:57 AM
8	No	5/30/2022 10:07 AM
9	Make it wheelchair and stroller accessible	5/30/2022 7:11 AM
10	I personally prefer anything in a wooded area with access to see various wildlife and native plants and trees	5/30/2022 7:09 AM
11	With as much traffic we now have in Conway paths for walking or running in a safe place is very important. Just walking my dog is hazardous due to traffic.	5/29/2022 7:09 AM
12	Bicycle trails around Lake Busbee would be great!	5/28/2022 10:48 PM
13	Green spaces for sitting and enjoying nature	5/28/2022 9:39 PM
14	Looks great. Sure to increase home values in the area.	5/28/2022 9:02 PM
15	No	5/28/2022 8:37 PM
16	I like all of these.	5/28/2022 8:32 PM
17	The design looks appropriate.	5/28/2022 8:23 PM
18	I think the city owns the land that was the old JC landing (adjacent to Waccamaw trestle). Turn that land into another park.	5/28/2022 8:19 PM
19	No sorry not good at design	5/28/2022 7:24 PM
20	No	5/28/2022 6:19 PM
21	No	5/28/2022 4:33 PM
22	No	5/28/2022 3:31 PM
23	I would like to see barriers that restrict golf carts on the pedestrian and bike paths but that still allow for wheelchairs.	5/28/2022 3:00 PM
24	Cost. Getting grants,etc. I have noticed that there was to be a bike path along Long Avenue and they added a ride walk in front of several houses when you make a right turn on Long Avenue. It was my understanding that the bike path was to continue on highway 813.	5/28/2022 1:23 PM
25	I would be happy with any or all options in the table. I think that these trails would get a lot of use from our community no matter which one or where it leads.	5/28/2022 12:38 PM
26	The Loris to Myrtle Beach idea blows my mind. This could be an incredible improvement for livability and connectivity for our area.	5/28/2022 12:15 PM



## Conway Pathways and Trails Plan - Survey No. 2

27	No	5/28/2022 12:12 PM
28	No, full support!	5/28/2022 11:48 AM
29	I haven't really researched enough to have comments or suggestions but like the general idea.	5/28/2022 11:27 AM
30	Wide enough that a person walking a dog and a biker can pass without it being scary, please!	5/28/2022 10:36 AM
31	Idiotic waste of time, need to focus on safety, fixing the streets around Conway, and less time on working with CCU and bringing in tourists	5/28/2022 10:13 AM
32	See previous	5/28/2022 9:36 AM
33	High need for safe pedestrian walkways	5/28/2022 9:07 AM
34	Every project should focus on sustaining nature, providing educational opportunities, preserving the areas, we are blessed to enjoy and protect.	5/17/2022 10:54 AM
35	consider having adequate signage and maps on trails. rules of use, speeds	5/17/2022 9:00 AM
36	No	5/15/2022 6:32 AM
37	No.	5/14/2022 1:27 PM
38	No	5/14/2022 12:40 PM
39	None other than avoid interaction with crossing busy roads and plan a good parking area to access the trail.	5/9/2022 10:21 AM
40	Prioritize Loris to Myrtle Rail Trail for reasons mentioned prev question. Connect to downtown/natural areas where it makes sense after the rail trail is prioritized.	5/2/2022 11:26 AM

## Q7 Do you have any comments or thoughts for the Plan regarding a potential River Crossing?

Answered: 36    Skipped: 60

#	RESPONSES	DATE
1	waste put money into making conway town really quaint.....	6/30/2022 1:52 PM
2	No	6/10/2022 2:25 PM
3	no	6/1/2022 10:49 AM
4	If you can add something to the Main Street bridge connecting to Depot road or using the railroad, that would be great (less costly).	5/31/2022 11:16 AM
5	none	5/31/2022 6:36 AM
6	The simplest and least costly design is he best option. A swing bridge is a poor choice as the lifetime cost would be high due to needing to employee someone to operate the bridge and moving things break more easily than non-moving things.	5/30/2022 6:11 PM
7	No	5/30/2022 10:57 AM
8	No	5/30/2022 10:07 AM
9	I think it would b3 awesome	5/30/2022 7:09 AM
10	Walking across any of the existing bridges is taking your life in your hands. So a safe bridge for pedestrians and bicycles is needed. Do not allow golf carts on the bridge. People drive and allow kids to drive in a very unsafe manner.	5/29/2022 7:09 AM
11	This would be amazing!	5/28/2022 10:48 PM
12	Traditional wooden bridges if possible	5/28/2022 9:39 PM
13	That would be fantastic	5/28/2022 9:02 PM
14	We need something for locals and tourist to do and be able to see what we have to offer	5/28/2022 8:37 PM
15	Functional, cost effective, and attractive, would be the criteria for me. I would guess people would want to fish from a pedestrian bridge so provision would need to be made for that.	5/28/2022 8:32 PM
16	The design looks appropriate.	5/28/2022 8:23 PM
17	Environmental concerns	5/28/2022 8:15 PM
18	Would be nice don't want it to interfere with boating.	5/28/2022 7:24 PM
19	No	5/28/2022 6:19 PM
20	All of this is a waste of money just like the Waccamaw River Park that floods any time there is substantial rain	5/28/2022 4:33 PM
21	No	5/28/2022 3:31 PM
22	I would like to see barriers that restrict golf carts but still allow for wheelchairs.	5/28/2022 3:00 PM
23	I think the immediate need is to make a third lane to Business 501. On a new bridge coming into Conway and then use the current Business 501 as a connection to Coastal Carolina.	5/28/2022 1:23 PM
24	As someone who works in Myrtle beach and lives in Conway I would like another vehicle bridge before any river crossing. I do think adding a walkway to the current bridges would be a good idea because so many people seem to be walking over them on a daily basis.	5/28/2022 12:38 PM
25	Very much needed.	5/28/2022 12:12 PM
26	Pedestrian-only, for recreation and commuting to and from work!	5/28/2022 11:48 AM

## Conway Pathways and Trails Plan - Survey No. 2

27	I definitely think the suspension bridge fits better with the feel of our town.	5/28/2022 11:27 AM
28	No	5/28/2022 10:36 AM
29	Please don't build one	5/28/2022 10:13 AM
30	This would expand our ability to connect with nature	5/28/2022 9:07 AM
31	Natural access.	5/17/2022 10:54 AM
32	No	5/15/2022 6:32 AM
33	I believe a crossing would a great way to expand. Potentially connecting Downtown Conway with the small Waccamaw park, and further to the refuge.	5/14/2022 1:27 PM
34	No	5/14/2022 12:40 PM
35	When designing, please take into consideration boat traffic and the need to allow constant boat travel.	5/11/2022 2:43 PM
36	We do not need another river crossing. Utilize 501 business and bypass, these would only require a short segment for crossing. A new crossing would call for new wetland impacts we cannot afford. Have a water taxi or kayak rental service available coupled with bike rentals on either side of river from CCU/refuge. Similar to scooter rental through apps in larger cities.	5/2/2022 11:26 AM

## Q8 Do you have any other comments or thoughts for this plan?

Answered: 33    Skipped: 63

#	RESPONSES	DATE
1	concentrate on making conway a nicer looking town.. paint buildings , tear down ruins, get business to invest .	6/30/2022 1:54 PM
2	No	6/10/2022 2:25 PM
3	no	6/1/2022 10:49 AM
4	Thank you for leading the way in providing an alternative to the evil necessity of the motor vehicle!	5/31/2022 11:16 AM
5	none	5/31/2022 6:37 AM
6	No	5/30/2022 10:57 AM
7	No	5/30/2022 10:07 AM
8	I am super excited to see our city developing pathways and trail!	5/30/2022 7:09 AM
9	Get started, and keep moving forward.	5/29/2022 7:10 AM
10	I'd love to see bicycle trails in Conway!	5/28/2022 10:48 PM
11	No	5/28/2022 9:39 PM
12	No	5/28/2022 9:03 PM
13	No	5/28/2022 8:37 PM
14	This is very, very well thought out. Thank you!!	5/28/2022 8:32 PM
15	A couple years ago, there was a big project to upgrade sidewalks n curbcuts on Main Street. That was nice because it corrected unlevel sidewalks and added curbcuts, but aparrently no thought was given to grading. There are many curbcuts that cannot be used in the days after a rain because they are full of water. Yes, it will evaporate, but what handicapped person wants to walk thru 2 inches of water. I live on Lakewood Ave and where it intersects Main Street there is a puddle that goes half way across Lakewood. .there was a moat around the block where the new Conway Eye Surgery Center was built. That block was corrected because it no longer holds water. Please fix all the other curbcuts. Thanks!	5/28/2022 8:26 PM
16	If possible some paths may need to be lit with lights.	5/28/2022 8:24 PM
17	I love it can't wait to walk it.	5/28/2022 7:24 PM
18	No	5/28/2022 6:19 PM
19	Kill it	5/28/2022 4:34 PM
20	Concerned about "roundabouts "on Main Street.	5/28/2022 1:25 PM
21	I know our community has some other more pressing issues but I love that we have the future in mind. My family uses all of the parks on a very regular basis and would love more parks and trails to navigate.	5/28/2022 12:40 PM
22	Hopefully connectivity for bike usage can be as equally utilitarian as it is recreational.	5/28/2022 12:16 PM
23	The more, the merrier, and healthier!	5/28/2022 11:48 AM
24	Not at this time.	5/28/2022 11:27 AM
25	No	5/28/2022 10:36 AM
26	Fix the internals problems with the City first, or find leadership that will	5/28/2022 10:13 AM

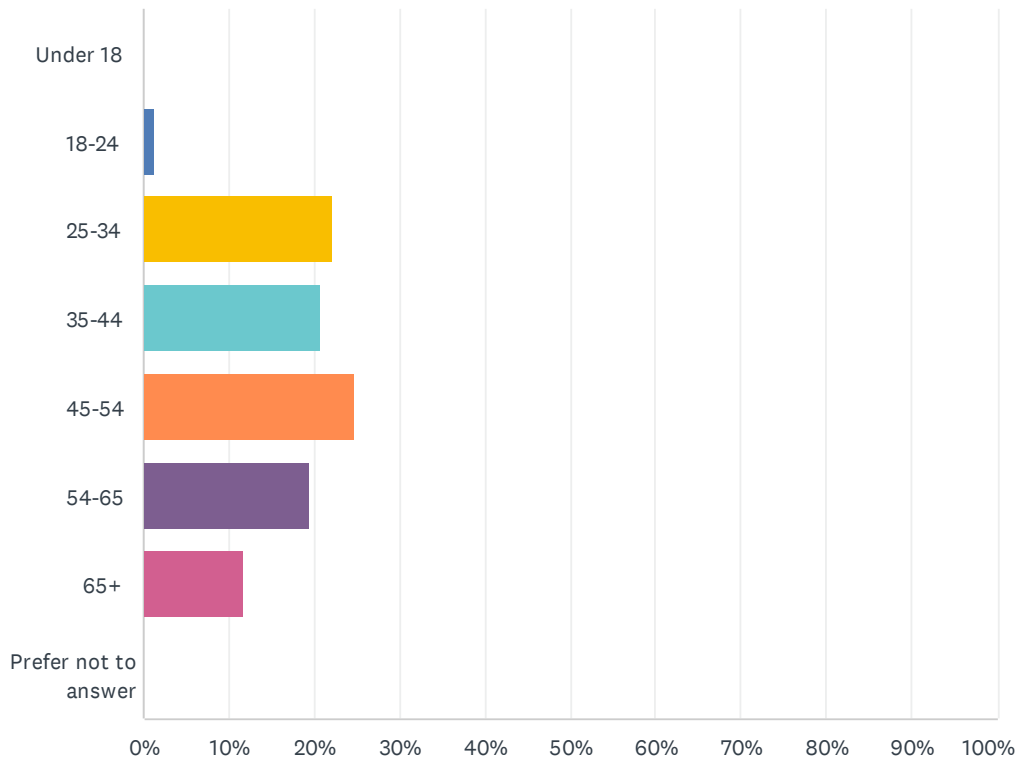
## Conway Pathways and Trails Plan - Survey No. 2

27	Love the idea of more trails.	5/28/2022 9:49 AM
28	No but, I'm excited to see we possibly get a trail where I can ride my bike. I can enjoy the weather and listen to nature	5/28/2022 8:59 AM
29	Conway needs to be different from Myrtle Beach, focus on the beauty of the area, nature' natural habitats, sustaining these. Also providing education and opportunities for those who cherish our earth.	5/17/2022 10:57 AM
30	Like ideas	5/15/2022 6:33 AM
31	Only that it is much needed	5/14/2022 1:28 PM
32	No	5/14/2022 12:40 PM
33	Thank you city of Conway for being so forward thinking and allowing us to be involved. Your staff really is the best, top notch.	5/2/2022 11:27 AM



## Q9 What is your age?

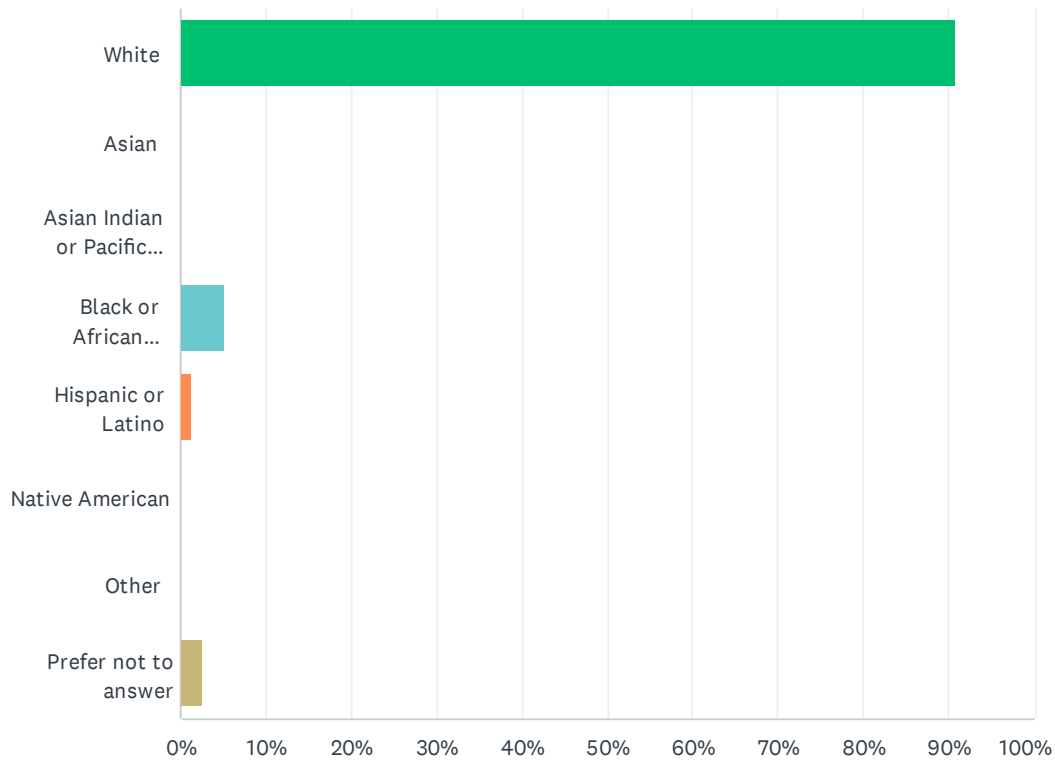
Answered: 77   Skipped: 19



ANSWER CHOICES	RESPONSES	
Under 18	0.00%	0
18-24	1.30%	1
25-34	22.08%	17
35-44	20.78%	16
45-54	24.68%	19
54-65	19.48%	15
65+	11.69%	9
Prefer not to answer	0.00%	0
<b>TOTAL</b>		<b>77</b>

## Q10 What race/ethnicity do you consider yourself?

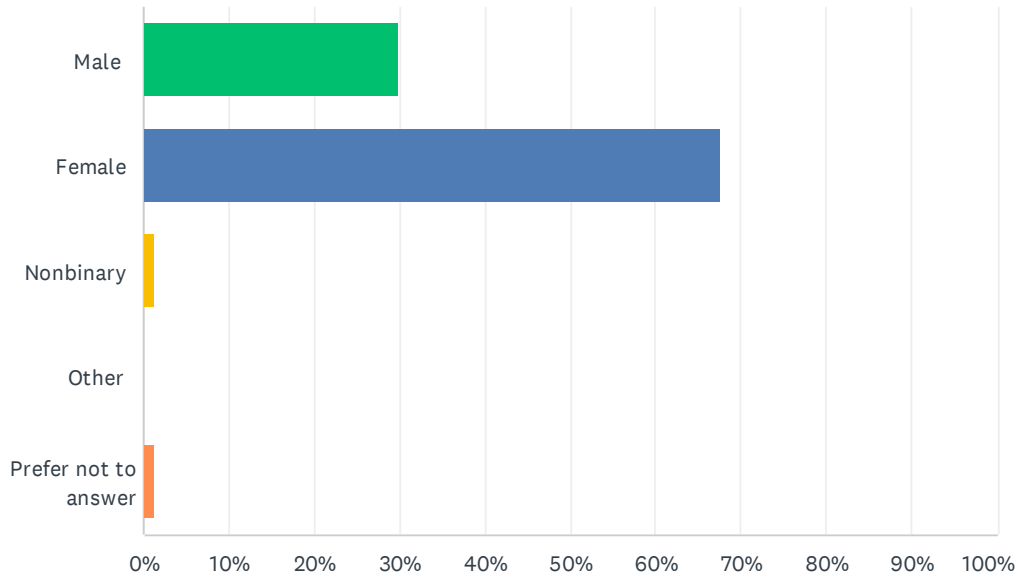
Answered: 77   Skipped: 19



ANSWER CHOICES	RESPONSES	
White	90.91%	70
Asian	0.00%	0
Asian Indian or Pacific Islander	0.00%	0
Black or African American	5.19%	4
Hispanic or Latino	1.30%	1
Native American	0.00%	0
Other	0.00%	0
Prefer not to answer	2.60%	2
<b>TOTAL</b>		<b>77</b>

## Q11 What gender do you consider yourself?

Answered: 77 Skipped: 19



ANSWER CHOICES	RESPONSES	
Male	29.87%	23
Female	67.53%	52
Nonbinary	1.30%	1
Other	0.00%	0
Prefer not to answer	1.30%	1
TOTAL		77

## Q12 What is your home ZIP Code? (example: 29526)

Answered: 78 Skipped: 18

#	RESPONSES	DATE
1	29526	6/30/2022 1:55 PM
2	29526	6/12/2022 10:07 PM
3	29527	6/10/2022 2:26 PM
4	29526	6/4/2022 8:28 PM
5	29526	6/1/2022 10:50 AM
6	29526	5/31/2022 11:17 AM
7	29579	5/31/2022 10:40 AM
8	29527	5/31/2022 6:37 AM
9	29526	5/30/2022 7:39 PM
10	29526	5/30/2022 6:59 PM
11	29527	5/30/2022 6:11 PM
12	29579	5/30/2022 5:20 PM
13	29526	5/30/2022 5:11 PM
14	29527	5/30/2022 11:52 AM
15	29527	5/30/2022 10:57 AM
16	29526	5/30/2022 10:08 AM
17	29527	5/30/2022 9:24 AM
18	29527	5/30/2022 8:10 AM
19	29527	5/30/2022 7:11 AM
20	29526	5/30/2022 7:10 AM
21	29526	5/30/2022 6:36 AM
22	29511	5/29/2022 4:24 PM
23	29577	5/29/2022 2:43 PM
24	29527	5/29/2022 9:07 AM
25	29526	5/29/2022 8:17 AM
26	29526	5/29/2022 7:10 AM
27	29526	5/29/2022 6:59 AM
28	29527	5/29/2022 12:48 AM
29	29526	5/28/2022 10:49 PM
30	29511	5/28/2022 9:58 PM
31	29526	5/28/2022 9:39 PM
32	29526	5/28/2022 9:03 PM
33	29527	5/28/2022 8:37 PM

# Conway Pathways and Trails Plan - Survey No. 2

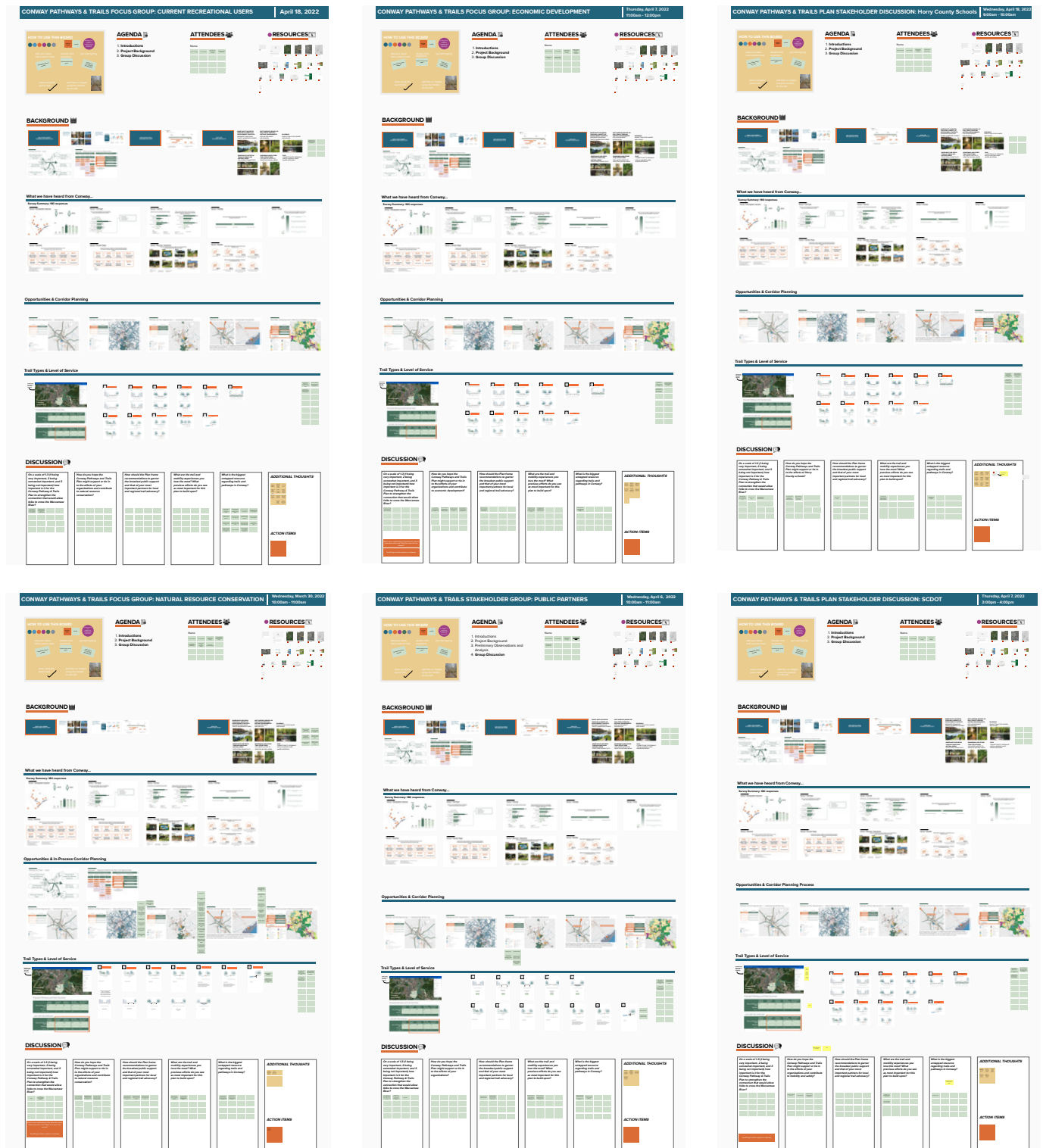
34	29527	5/28/2022 8:32 PM
35	29526	5/28/2022 8:27 PM
36	29526	5/28/2022 8:26 PM
37	29526	5/28/2022 8:15 PM
38	29526	5/28/2022 7:24 PM
39	29526	5/28/2022 7:07 PM
40	29526	5/28/2022 6:20 PM
41	29526	5/28/2022 4:34 PM
42	29527	5/28/2022 3:00 PM
43	29526	5/28/2022 2:55 PM
44	29527	5/28/2022 1:26 PM
45	29526	5/28/2022 1:26 PM
46	29526	5/28/2022 1:14 PM
47	29527	5/28/2022 1:02 PM
48	29527	5/28/2022 12:40 PM
49	29526	5/28/2022 12:37 PM
50	29526	5/28/2022 12:16 PM
51	29511	5/28/2022 12:13 PM
52	29526	5/28/2022 12:10 PM
53	29527	5/28/2022 12:09 PM
54	29585	5/28/2022 11:49 AM
55	29526	5/28/2022 11:28 AM
56	29526	5/28/2022 10:37 AM
57	29526	5/28/2022 10:18 AM
58	29526	5/28/2022 10:14 AM
59	29526	5/28/2022 10:14 AM
60	29527	5/28/2022 10:10 AM
61	29527	5/28/2022 10:07 AM
62	29588	5/28/2022 9:50 AM
63	29526	5/28/2022 9:37 AM
64	29526	5/28/2022 9:07 AM
65	29527	5/28/2022 9:01 AM
66	29526	5/28/2022 8:59 AM
67	29526	5/28/2022 8:57 AM
68	29527	5/28/2022 8:55 AM
69	29526	5/28/2022 8:29 AM
70	29526	5/28/2022 8:20 AM
71	29526	5/17/2022 11:29 AM



## Conway Pathways and Trails Plan - Survey No. 2

72	29526	5/17/2022 10:58 AM
73	29526	5/17/2022 9:00 AM
74	29511	5/15/2022 6:33 AM
75	29527	5/14/2022 1:28 PM
76	29527	5/14/2022 12:41 PM
77	29526	5/11/2022 2:44 PM
78	29526	5/2/2022 11:28 AM

# STAKEHOLDER AND FOCUS GROUP MEETING MINUTES



Each Focus Group and Stakeholder meeting was held via Zoom, using interactive discussion boards pictured above.

## DESIGNWORKSHOP

Landscape Architecture

Planning

Urban Design

Strategic Services

301 N West Street

Suite 109

Raleigh, North Carolina 27603

919-973-6254

designworkshop.com

## MEETING RECORD

To: Mary Catherine Hyman, City of Conway  
From: Emily McCoy, Design Workshop  
Date: April 28, 2022  
Project Name: Conway Pathways and Trails Plan  
Project #: 6772  
Subject: Focus Group: SCDOT  
Meeting Date: April 7, 2022  
Start/End: 3:00 pm – 4:00 pm  
Location: Zoom

☐ Meeting ☐ Telephone ☒ Conference Call

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Following are the minutes of the above referenced meeting.

The following people were present: Adeline (Ellie) Lerner (DW) Corey Dodd (DW), Adam Emrick (City), Anna Barnhill (SCDOT)

### 1. Discussion

- a. **How do you hope the Conway Pathways and Trails Plan might support or tie into the efforts of your organizations and contribute to mobility and safety?**
  - Adequate lighting and signage
  - Access for seniors and safety walking around town
- b. **What are the trail and mobility experiences you love the most? What previous efforts do you see as most important for this plan to build upon?**
  - #1 – mobility, getting downtown on foot and by bike
  - Safe trails for kids, the ability for them to be “off in the woods”
- c. **What is the biggest untapped resource regarding trails and pathways in Conway?**
  - Crossing the river
- d. **Additional Thoughts**
  - LPA projects in Myrtle Beach and North Myrtle Beach, filling in gaps in East Coast Greenway
  - There is strong interest in the project type within DOT
  - Carolina Forest, Market Commons as precedents
  - Feeling extremely excited, in the past always had to leave Conway for entertainment
  - Getting around Conway by car is stressful, the goal is to make the elderly feel safe walking around town

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## MEETING RECORD

To: Mary Catherine Hyman, City of Conway  
From: Emily McCoy, Design Workshop  
Date: April 28, 2022  
Project Name: Conway Pathways and Trails Plan  
Project #: 6772  
Subject: Focus Group: Current Recreational Users  
Meeting Date: April 18, 2022  
Start/End: 1:00 pm – 2:00 pm  
Location: Zoom

☐ Meeting ☐ Telephone ☒ Conference Call

---

Following are the minutes of the above referenced meeting.

The following people were present: Adeline (Ellie) Lerner (DW) Corey Dodd (DW), Mary Catherine Hyman (City), Dale Long (Conway Police Chief)

### 1. Discussion

- a. **On a scale of 1-3 (1 being very important, 2 being somewhat important, and 3 being not important) how important is it for the Conway Pathway & Trails Plan to strengthen the connection that would allow folks to cross the Waccamaw River?**
  - For connections to CCU and other amenities across the river this connection will be important
  - A cart path along the powerline at Lewis Ocean Bay Heritage Preserve and 501 could provide connections to Myrtle Beach
- b. **What is the biggest untapped resource regarding trails and pathways in Conway?**
  - Significant railroad investment throughout the County, from Conway to the state line there are 25+ miles of railway that can be utilized
  - Virginia Creeper Trail and Blue Clay Park in New Hanover County as precedent
  - Property acquired via flood buyouts for incorporation
  - Outfitters for rental services, tours, guides
  - Creation of a boardwalk at Lake Busbee and along 501 for marathons to increase loop size to support longer recreation routes
  - Bike lanes needed for safety downtown
  - Lewis Ocean Bay Heritage Preserve, potential for mountain bike facility and connections via rail/utility lines
  - Simpson Creek, Buck Creek, Crabtree Swamp
  - Laurel Street connector to have the ability to avoid Main Street
- c. **Additional Thoughts**
  - Signage and maps are critical, road markings, signalized crossings, and safe intersections
  - Safety for all levels of cycling, especially on Main Street
  - Large mountain biking population in Myrtle Beach and larger road bike group in Conway
  - Considerations for trail hours – limited access afterhours, lighting
  - Need for off-road opportunities, dirt roads are limited
  - Coquina replacing dirt roads, means losing the “trail” feel
  - Performance riders (cyclists) avoid “in town” trails and need gravel roads/paths
  - Canal system and major road crossings are a challenge

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## MEETING RECORD

To: Mary Catherine Hyman, City of Conway  
From: Emily McCoy, Design Workshop  
Date: April 28, 2022  
Project Name: Conway Pathways and Trails Plan  
Project #: 6772  
Subject: Focus Group: Economic Development  
Meeting Date: April 7, 2022  
Start/End: 11:00 am – 12:00 pm  
Location: Zoom

☐ Meeting ☐ Telephone ☒ Conference Call

---

Following are the minutes of the above referenced meeting.

The following people were present: Adeline (Ellie) Lerner (DW), Corey Dodd (DW), Mary Catherine Hyman (City), Sandy Davis (Myrtle Beach Regional Economic Development Corporation), Lesli McGee (McLeod Hospital), McKenzie Jordan (378 Land Holdings)

### 1. Discussion

- a. **On a scale of 1-3 (1 being very important, 2 being somewhat important, and 3 being not important) how important is it for the Conway Pathway & Trails Plan to strengthen the connection that would allow folks to cross the Waccamaw River?**
  - Concerns raised in other locations about river crossings and fishing, permits are required, crossings may create safety issues
- b. **How do you hope the Conway Pathways and Trails Plan might support or tie into the efforts of your organizations and contribute to economic development?**
  - Excited to bring people to the area to enjoy County resources
  - McLeod Health recognizes that an active trail system links well into [Social Determinants of Health](#), that when improved, also improves health and well-being of Conway residents
  - McLeod Health has discussed how to develop the new location on El Bethel Road to complement the trails system
- c. **How should the Plan frame recommendations to garner the broadest public support and that of your most important partners for local and regional trail advocacy?**
  - McLeod Health will want to connect the trail system development to improving Social Determinants of Health – a public campaign on improving health and wellbeing
- d. **What are the trail and mobility experiences you love the most? What previous efforts do you see as most important for this plan to build upon?**
  - Multi-user trail planning is key
  - Offer options for walking and biking running to encourage high levels of use making the network of pathways and trails feel safe
  - In Greenville there are horse mounted police that make everyone feel safe when using remote areas of the trails
- e. **What is the biggest untapped resource regarding trails and pathways in Conway?**
  - Natural beauty
- f. **Additional Thoughts**
  - The group is happy to forward information and feedback to and from residents who have relocated to Conway from cities that have similar plans
  - Walkability/bike accessibility as an alternative to vehicular traffic, will contribute to less congestion and a healthier and more active population
  - Greenville, Augusta, Charleston, and Hilton Head as precedents for security
  - Use planned expanses of trails and mile markers to encourage physical activity in care plans for seniors
  - Consider what is needed to make seniors feel safe, designated senior safety areas, lighting, guided group tours
  - How will trails connect to destinations, businesses, and services along the trail routes to inform the public?



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## MEETING RECORD

To: Mary Catherine Hyman, City of Conway  
From: Emily McCoy, Design Workshop  
Date: April 28, 2022  
Project Name: Conway Pathways and Trails Plan  
Project #: 6772  
Subject: Focus Group: Horry County Schools  
Meeting Date: April 18, 2022  
Start/End: 9:00 am – 10:00 am  
Location: Zoom

☐ Meeting ☐ Telephone ☒ Conference Call

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Following are the minutes of the above referenced meeting.

The following people were present: Adeline (Ellie) Lerner (DW) Corey Dodd (DW), Mary Catherine Hyman (City), Joe Burch (Horry County Schools)

### 1. Discussion

- a. **On a scale of 1-3 (1 being very important, 2 being somewhat important, and 3 being not important) how important is it for the Conway Pathway & Trails Plan to strengthen the connection that would allow folks to cross the Waccamaw River?**
  - Not important to cross but important to have access to the river
  - Need for a new bridge
  - The two roads crossing Busbee are the lifeline to Conway
- b. **How do you hope the Conway Pathways and Trails Plan might support or tie into the efforts of Horry County schools?**
  - Tie into after school opportunities, libraries, parks, rec center programming
  - Serve as a place for students to recreate and get between spaces safely
- c. **How should the Plan frame recommendations to garner the broadest public support and that of your most important partners for local and regional trail advocacy?**
  - Address safety concerns
- d. **What are the trail and mobility experiences you love the most? What previous efforts do you see as most important for this plan to build upon?**
  - Riverwalk, Lake Busbee, Waccamaw National Wildlife Refuge, downtown
- e. **What is the biggest untapped resource regarding trails and pathways in Conway?**
  - Opportunity for connectivity to schools
- f. **Additional Thoughts**
  - Colleagues concerned about security on multi-use paths along roadways, and trails on or adjacent to school lands
  - Need for monitoring capabilities to avoid having to fence school campuses
  - Need for connectivity by way of sidewalks and bike paths along roadways
  - Concerns for children walking and biking to school while unattended
  - Noted large schools are built Conway to encompass student populations from large geographical areas
  - Recommended a survey for families with students to learn about obstacles
  - Need for traffic calming and security guidelines
  - Myrtle Beach as precedent, walkable schools within a superblock

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## MEETING RECORD

To: Mary Catherine Hyman, City of Conway  
From: Emily McCoy, Design Workshop  
Date: April 28, 2022  
Project Name: Conway Pathways and Trails Plan  
Project #: 6772  
Subject: Focus Group: Natural Resource Conservation  
Meeting Date: March 30, 2022  
Start/End: 10:00 am – 11:00 am  
Location: Zoom

☐ Meeting ☐ Telephone ☒ Conference Call

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Following are the minutes of the above referenced meeting.

The following people were present: Corey Dodd (DW), Urey Zhuang (DW), Mary Catherine Hyman (City), Cara Schildknecht (Winyah Rivers Alliance and Waccamaw River Keeper), Craig Sasser (Waccamaw National Wildlife Refuge), Trapper Fowler (Coastal Conservation League), April O'Leary (Horry County Rising)

### 1. Background

- a. What are pathways and trails? (comments on needs to include/consider)
  - Multi-use trails that include flood mitigation projects
  - More connectivity to downtown and Riverwalk Park (e.g., former Grainger plant)
  - Soft surface permeable trails where possible (hydrology and flooding concerns)
  - Stronger connections to blueways
  - Better access for kayaking with soft launches

### 2. Opportunities & In-Process Corridor Planning

- a. In review of draft corridor opportunity maps (April O'Leary)
  - Majority of trails shown connect to nature rather than providing connections to historical and cultural elements
  - Need for feedback from children, how would they enjoy using pathways and trails?
  - Need for school partnerships and workshops
  - Desire for more connectivity to reduce feelings of division (divided by bypass, creating cohesion between old and new)
  - Although not the biggest priority, connectivity to Aynor and Myrtle beach would be beneficial
  - Connectivity to CCU as a higher priority than connectivity to Aynor or Myrtle Beach
  - Often hears others express desires for quick access to nature
  - Connections to broader destinations via bike trail is desired
  - Future connections via railroad (rails with trails / rails to trails)
- b. In review of draft corridor opportunity maps (Trapper Fowler)
  - Coastal Conservation League as potential partner
  - Priority to tap into rails/trails idea to provide connections to downtown for folks that live outside of downtown
  - Rails system for connections from CCU to downtown, downtown to Myrtle Beach, 701 North
  - Get folks outside of cars and on bikes for daily exercise
  - With widening of 701 North, the addition of bike lanes is seen as an opportunity
  - Connections from downtown to other municipalities and larger destinations
  - Public land and the perception of publicly accessible land is that it is scarce
  - How will the city plan to provide more natural resources to link pathways?
  - What parcels exist within a certain radius of the city for natural resource conservation?
  - Makes sense to cross the bridge and tap into the rail system and connect to the wildlife refuge
  - Pathways and trails will be utilized more once residents can bike to work
  - Having paths in proximity to rails will encourage business development along the trails, trails become destinations
- c. In review of draft corridor opportunity maps (Cara Schildknecht)

- Goal of attracting more people to river towns
- Economic benefits will result from the Plan
- For those outside of Conway it does not make sense to drive into town for the existing short trail experiences
- Connectivity between Wildlife Refuge and downtown
- Signage/wayfinding – educational components centered on trail locations, parking, understanding where people are in context of the city and county while on trails and trailheads
- Will benefit existing business owners

### 3. Trail Types & Level of Service

#### a. Comments on Blueway trail section

- Need for flood resistant materials and maintenance
- Erosion concerns

### 2. Discussion

#### a. **On a scale of 1-3 (1 being very important, 2 being somewhat important, and 3 being not important) how important is it for the Conway Pathway & Trails Plan to strengthen the connection that would allow folks to cross the Waccamaw River?**

- 3
- 2, important but not most important
- The two roads crossing Busbee are the lifeline to Conway

#### b. **How do you hope the Conway Pathways and Trails Plan might support or tie into the efforts of your organizations and contribute to natural resource conservation?**

- More flood storage needed to preserve the city, critical to prioritize over other needs in protection of current assets
- Winayah Rivers Alliance aims to improve access to the river for paddling and recreation, increased access to blueways will help with this
- Winyah Rivers Alliance is dedicated to preservation, the more green space protected while utilizing for education and recreation, the better
- Serve as a place for students to recreate and get between spaces safely

#### c. **How should the Plan frame recommendations to garner the broadest public support and that of your most important partners for local and regional trail advocacy?**

- Signage is important, sponsored benches, trees, gardens, lighting, etc. would be a way to solicit the support of more stakeholders (e.g., sponsored brick pathway for veterans)
- Many partners are focused on land conservation, establishing green space and corridors for connections between conserved areas is important
- The positive feedback loop of folks using conserved lands and giving back and supporting the community is a large part of our work, if we can build appreciation through access, it will benefit the environment and the economy

#### d. **What are the trail and mobility experiences you love the most? What previous efforts do you see as most important for this plan to build upon?**

- Walking paths that also provide benefits including flood mitigation, preservation, connectivity
- Boat landings that have kayak launch area, will decrease competition for the ramp and increases safety for paddlers

#### e. **What is the biggest untapped resource regarding trails and pathways in Conway?**

- The floodplain is disconnected and could be reconnected to provide nature trails, former Grainger plant land could be connected to the Riverwalk or Riverwalk Park along Depot Road
- Connecting the river town identity to the river
- The river offers so much and has meant a lot to Conway since the founding of Conway, it should be celebrated as a centerpiece of the city

#### f. **Additional Thoughts**

- Desire to incorporate native species into the trails to provide critical habitat and plants for local species (e.g., Mayors' Monarch Pledge)
- Use trails to protect the city from flooding and consider hazard mitigation with all future plans

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## MEETING RECORD

To: Mary Catherine Hyman, City of Conway  
From: Emily McCoy, Design Workshop  
Date: April 28, 2022  
Project Name: Conway Pathways and Trails Plan  
Project #: 6772  
Subject: Focus Group: Public Partners  
Meeting Date: April 6, 2022  
Start/End: 10:00 am – 11:00 am  
Location: Zoom

☐ Meeting ☐ Telephone ☒ Conference Call

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Following are the minutes of the above referenced meeting.

The following people were present: Adeline (Ellie) Lerner (DW) Corey Dodd (DW), Mary Catherine Hyman (City), Kelly Mezzapelle (Myrtle Beach)

1. Opportunities & Corridor Planning
  - a. In review of draft corridor opportunity maps (Opportunity 3 – Connected Destinations)
    - Opportunities exist for a rail trail from Conway to Myrtle Beach connecting workforce housing neighborhoods
    - How can the workforce use trails as a means of transportation? Connections are needed to neighborhoods
    - Connections via Carolina Forest (in webmap) are beneficial in providing connections to the East Coast Greenway
    - The crossing at US-17 Bypass will address the need for something to connect and improve the experience
2. Discussion
  - a. **On a scale of 1-3 (1 being very important, 2 being somewhat important, and 3 being not important) how important is it for the Conway Pathway & Trails Plan to strengthen the connection that would allow folks to cross the Waccamaw River?**
    - Goal of committees to use Myrtle Beach as a bicycle tourism destination
    - Farther reach of trail system will be beneficial, if people know they can ride bikes from Myrtle Beach to Conway it will produce economic benefits for both cities
    - Transportation alternatives to traffic congestion that increases during the summer
    - Need for a new bridge
  - b. **How should the Plan frame recommendations to garner the broadest public support and that of your most important partners for local and regional trail advocacy?**
    - Consider that people walk and ride for different reasons
    - Key to know many people ride their bikes to work, need for additional outreach to these folks
    - Funding is DOT money, need to highlight trails as transportation corridors in addition to recreation corridors
  - c. **What are the trail and mobility experiences you love the most? What previous efforts do you see as most important for this plan to build upon?**
    - Infrastructure should feel safe, what is the experience in arriving to destinations?
    - We are working to spruce up the experiential quality of Myrtle Beach's 20-mile loop, food stops along trails, art, etc.
  - d. **What is the biggest untapped resource regarding trails and pathways in Conway?**
    - Those who use trails and pathways as their key mode of transportation, these stakeholders are a huge resource
    - Transportation funding, frame the narrative to the audience
  - e. **Additional Thoughts**
    - Desired connections to Murrells Inlet trails
    - Slow Roll Program, Slow Stroll Program, now wanting to begin a Slow Row Program to connect kayakers to recreational experiences between cities
    - Ecotourism opportunity in paddling from Myrtle Beach to downtown Conway
    - The recreation division is looking at kayaking corridors via public and private stormwater facilities

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## MEETING RECORD

To: Mary Catherine Hyman, City of Conway  
From: Emily McCoy, Design Workshop  
Date: April 28, 2022  
Project Name: Conway Pathways and Trails Plan  
Project #: 6772  
Subject: Focus Group: Santee Cooper  
Meeting Date: April 4, 2022  
Start/End: 1:00 pm – 2:00 pm  
Location: Zoom

☐ Meeting ☐ Telephone ☒ Conference Call

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Following are the minutes of the above referenced meeting.

The following people were present: Adeline (Ellie) Lerner (DW) Corey Dodd (DW), Mary Catherine Hyman (City), Adam Emrick (City), Anna Strickland (Santee Cooper)

1. Opportunities & Corridor Planning
  - a. Conversion of overhead utilities to underground is a long-term goal
  - b. Lighting along roadway trails
  - c. Natural and soft trails accessible dawn to dusk (no lighting)
2. Discussion
  - a. **On a scale of 1-3 (1 being very important, 2 being somewhat important, and 3 being not important) how important is it for the Conway Pathway & Trails Plan to strengthen the connection that would allow folks to cross the Waccamaw River?**
    - Very important
    - How would this access contribute to safety during hurricanes and weather resistance for emergency corridors?
    - Review feasibility of bridge option 2 with electricity corridor
  - b. **What is the biggest untapped resource regarding trails and pathways in Conway?**
    - Santee Cooper is willing to serve as a resource
    - Corridors will need to be reviewed on a case-by-case basis
  - c. **Additional Thoughts**
    - Design guidelines and trail restrictions
    - Are there examples of trails and public access partners Santee Cooper has partnered with?



